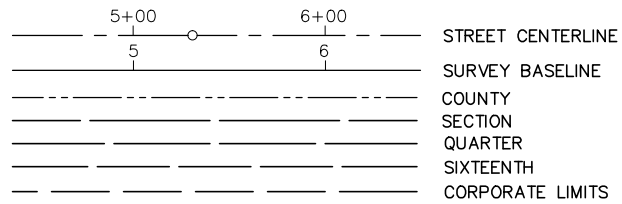
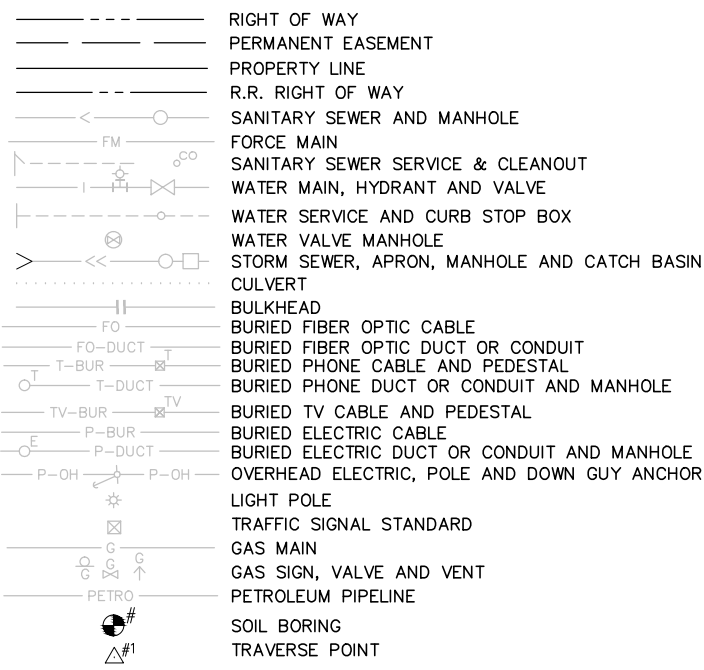


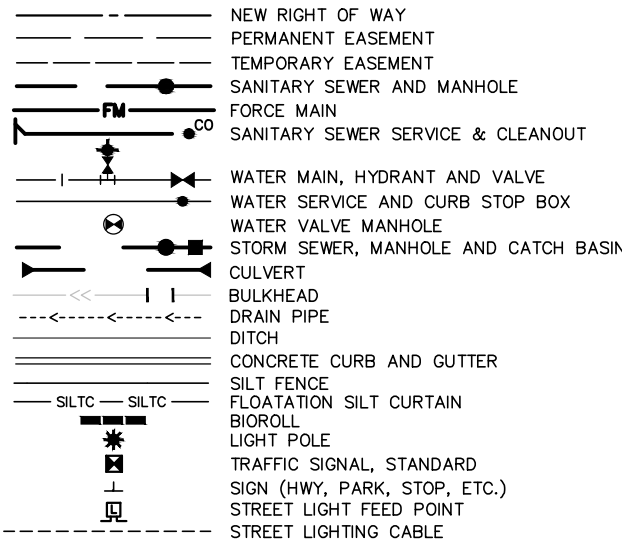
LEGEND



EXISTING



PROPOSED

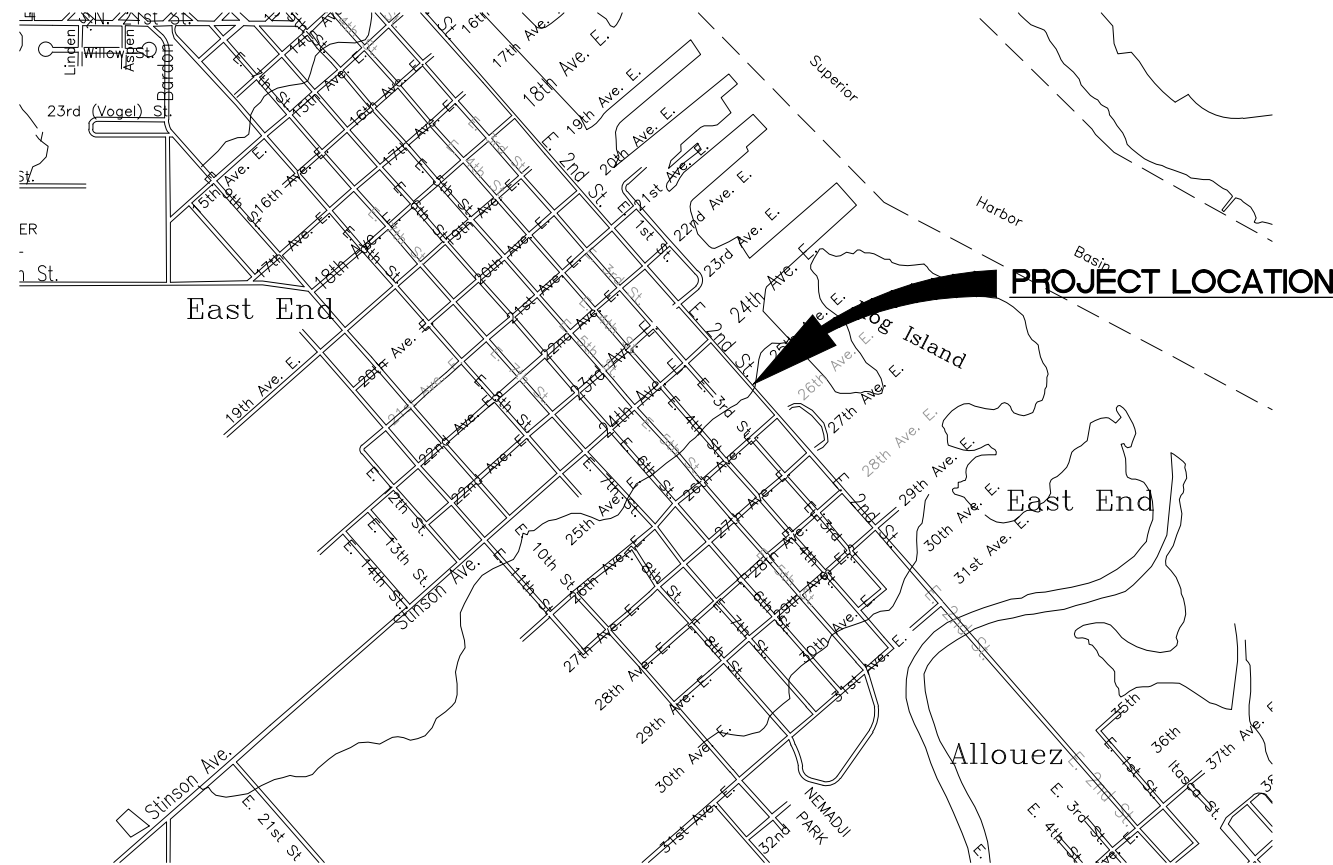


CITY OF SUPERIOR, WISCONSIN

CONSTRUCTION PLANS FOR

REPLACEMENT OF STORM SEWER, SANITARY SEWER

NEWTON CREEK FLOOD REPAIR



GOVERNING SPECIFICATIONS
 THE 2013 EDITION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION" SHALL GOVERN EXCEPT AS MODIFIED BY THE SPECIFICATIONS AND PLANS FOR THIS PROJECT.
 ALL PIPING AND MANHOLE CONNECTIONS SHALL BE INSTALLED ACCORDING TO THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR SEWER AND WATER CONSTRUCTION IN WISCONSIN".
 ALL TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM TO THE MUTCD, INCLUDING "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS", - CURRENT EDITION.

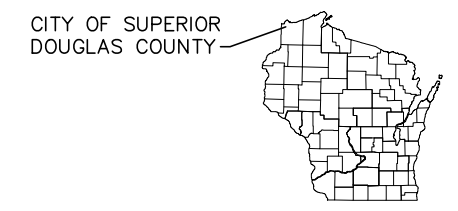
INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	STATEMENT OF ESTIMATED QUANTITIES/DETAILS
3	EXISTING CONDITIONS (COMBINED SHEETS)
4-3	EROSION CONTROL PLAN (COMBINED SHEETS)
5-4	STORM SEWER PLAN AND PROFILE
6-5	SANITARY SEWER PLAN AND PROFILE
7-6	TRAFFIC CONTROL

THIS PLAN CONTAINS 7 SHEETS
6

- | | |
|--|--|
| <p>ENGINEER
 PROJECT ENGINEER
 RESIDENT PROJECT REP.
 SURVEYOR
 BRIDGE MONITORING
 GENERAL CONTRACTOR
 SUBCONTRACTORS
 CONCRETE PIPE SUPPLIER
 CONCRETE MATERIALS
 MH CONNECT/CORE DRILLING
 BITUMINOUS
 EXCAVATING & GRADING
 EROSION CONTROL
 TURF ESTABLISHMENT
 UNDERGROUND UTILITIES (SANITARY, STORM, WATER)
 UNDERGROUND UTILITIES (GAS, ELEC, TELE, CABLE)
 INSTALL HP STEEL PILES
 TESTING-UTILITIES
 FENCE REPAIR
 YEAR OF CONSTRUCTION</p> | <p>SHORT ELLIOTT HENDRICKSON INC.
 DAN HINZMANN
 JEROLD HALDORSON, TYLER YNGSDAL
 SHORT ELLIOTT HENDRICKSON INC.
 BRIAN ALBRECHT-SEH (GEOTECH)
 RJS CONSTRUCTION GROUP
 COUNTY MATERIALS
 LAKEHEAD CONCRETE
 SUPERIOR DIAMOND CONC. CUTTING
 MONARCH PAINTING
 RJS CONSTRUCTION GROUP
 SUPERIOR LANDSCAPING
 SUPERIOR LANDSCAPING
 RJS CONSTRUCTION GROUP
 NOT REQUIRED
 RJS CONSTRUCTION GROUP
 RJS, SEH-SANITARY
 DAIRYLAND FENCE
 2013</p> |
|--|--|

PROJECT LOCATION



SUPERIOR, WISCONSIN



RECORD DRAWINGS
 UPDATED 03/2014

FILE NO.
 122115

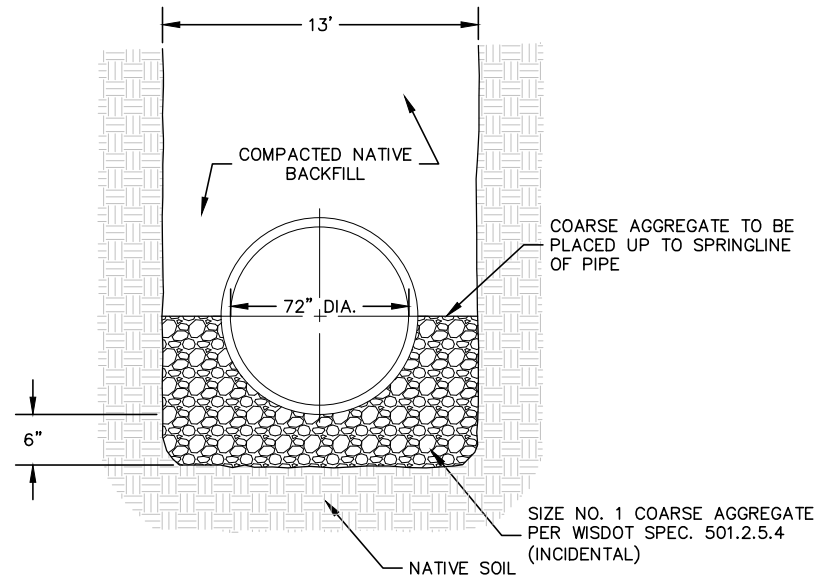
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NOTE:
 THE SUBSURFACE UTILITY QUALITY INFORMATION IN THIS PLAN IS LEVEL D. THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02 ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."

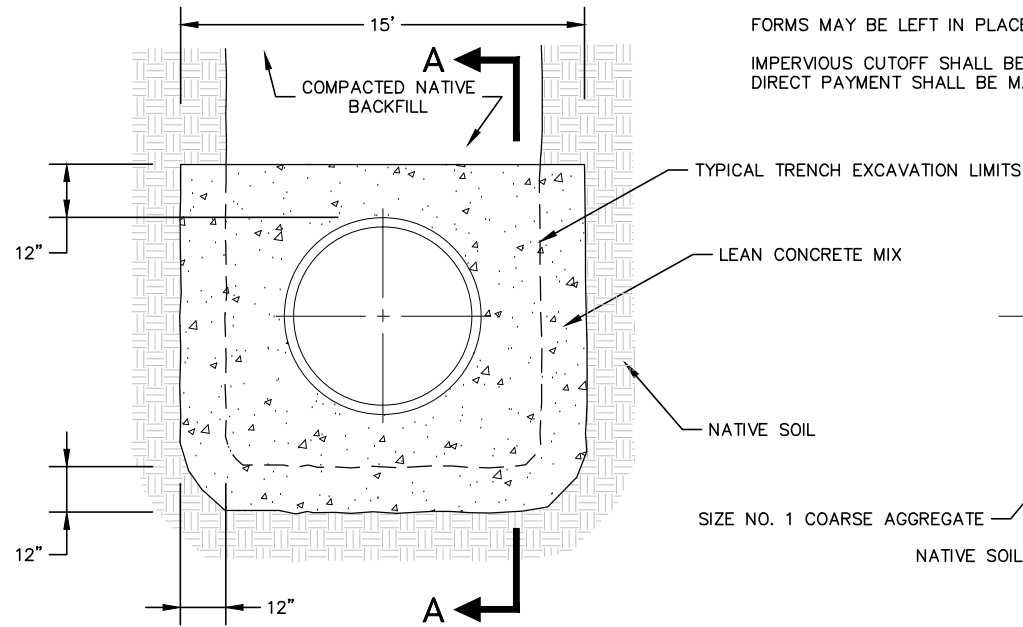
THE CONTRACTOR SHALL CALL DIGGER'S HOTLINE AT 811 BEFORE COMMENCING EXCAVATION.



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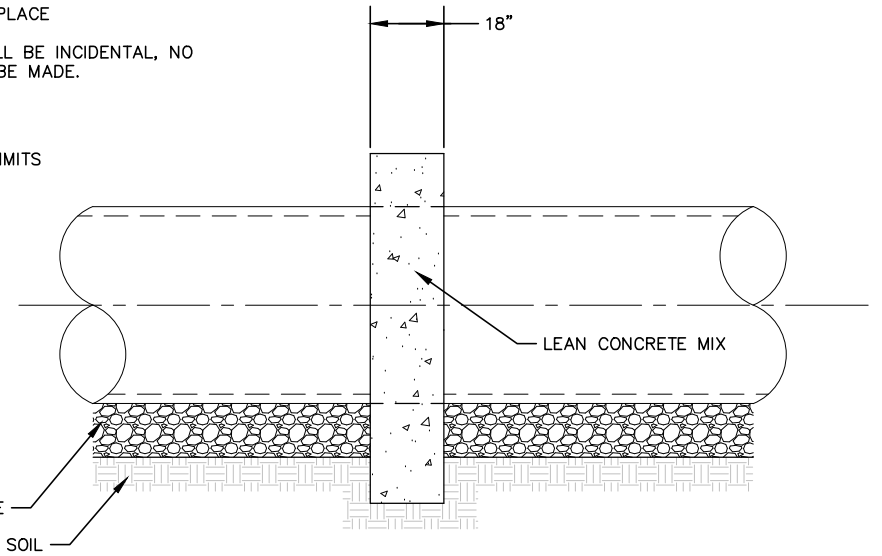


PIPE BEDDING DETAIL / TYPICAL SECTION
N.T.S.



IMPERVIOUS CUTOFF DETAIL
N.T.S.

IMPERVIOUS CUTOFF NOTES:
FORMS MAY BE LEFT IN PLACE
IMPERVIOUS CUTOFF SHALL BE INCIDENTAL, NO DIRECT PAYMENT SHALL BE MADE.



SECTION A-A
N.T.S.

NOTES:	
1	ACTUAL LENGTH OF REMOVAL SHALL BE THE AMOUNT REQUIRED TO INSTALL PROPOSED PIPING. REMAINING PIPES TO BE ABANDONED.
2	PORTIONS OF THE 72" CMP WHICH REMAIN SHALL BE FILLED WITH SAND PRIOR TO BULKHEADING OF PIPE ENDS. ESTIMATED LENGTH OF 72" CMP ABANDONMENT=140 LF. EXISTING CMP IS PARTIALLY COLLAPSED NEAR BRIDGE PIER.
3	ESTIMATED QUANTITY TO BE USED FOR GRAVEL ROADWAY RESTORATION.
4	SPLICES WILL NOT BE MEASURED FOR PAYMENT AND ARE INCIDENTAL TO THE LF COST OF DELIVERED AND DRIVEN SHEET PILING. NO MORE THAN ONE SPLICE PER PILE.
5	ALL WORK RELATED TO THE INSTALLATION OF SHEET PILING SHALL BE COMPLETED 14 DAYS IN ADVANCE OF ANY ADDITIONAL WORK.
6	TRACER WIRE, TESTING, AND BOXES SHALL BE INCIDENTAL.
7	ALL DEWATERING AND STREAM BYPASS ITEMS SHALL BE INCIDENTAL.

STATEMENT OF ESTIMATED QUANTITIES					
NOTE	ITEM NO.	ITEM DESCRIPTION	UNIT	ESTIMATED QUANTITIES	FINAL QUANTITIES
	204.0245	REMOVE SANITARY SEWER (10" RCP)	LIN FT	250	250
1	204.0245	REMOVE SANITARY SEWER (12" HDPE)	LIN FT	20	83
1	204.0245	REMOVE STORM SEWER (72" CMP)	LIN FT	125	135
	204.0270	ABANDONING CULVERT PIPE (12" HDPE)	EACH	1	1
2	204.0270	ABANDONING CULVERT PIPE (72" CMP)	EACH	1	1
3	305.0125	BASE AGGREGATE DENSE 1-1/4"	TON	60	100
	455.0105	ASPHALTIC MATERIAL PG 58-28	TON	0.90	1.64
	460.1100	HMA PAVEMENT TYPE E-0.3	TON	15	29.99
	520.1072	APRON ENDWALLS FOR CULVERT PIPE 72-INCH	EACH	1	1
4, 5	550.1140	PILING STEEL HP 14-INCH X 73 LB	LIN FT	1800	1398
	606.0200	RIPRAP MEDIUM	CU YD	20	92
	608.0472	STORM SEWER PIPE REINFORCED CONCRETE CLASS V 72-INCH	LIN FT	258	248
	608.0472	STORM SEWER PIPE REINFORCED CONCRETE CLASS V 72-INCH 30 DEGREE BEND	EACH	1	1
	608.0472	STORM SEWER PIPE REINFORCED CONCRETE CLASS V 72-INCH 40 DEGREE BEND	EACH	2	2
	611.2504	MANHOLE VARIABLE TEE 4-FT DIAMETER	EACH	1	1
	611.0530	MANHOLE COVERS TYPE J	EACH	1	1
	619.1000	MOBILIZATION	LUMP SUM	1	1
	619.1000	MOBILIZATION (PILING)	LUMP SUM	1	1
	625.0105	TOPSOIL	CU YD	150	166
	628.1504	SILT FENCE	LIN FT	400	350
	628.2008	EROSION MAT URBAN CLASS I TYPE B	SQ YD	1600	1600
	628.7504	TEMPORARY DITCH CHECKS	LIN FT	100	0
	630.1011	SEEDING MIXTURE NO. 10	LB	25	25
	643.0100	TRAFFIC CONTROL (PROJECT)	EACH	1	1
	SPV.0060.01	CONNECT TO EXISTING SANITARY SEWER MANHOLE	EACH	2	2
	SPV.0060.02	CONNECT TO EXISTING STORM SEWER MANHOLE	EACH	1	1
6	SPV.0090.01	10" PVC SANITARY SEWER SDR-35	LIN FT	266	266
	SPV.0105.01	FENCE REMOVAL AND REPAIR	LUMP SUM	1	1

CHANGE ORDER NO. 1 (TIE PIPE JOINTS-72" RCP, REMOVE AND RESTOCK HP STEEL PILING)
CHANGE ORDER NO. 2 (REMOVE EXIST. BRIDGE FOUNDATION, ADDITIONAL SAN. MH, RESTOCK (2) 72" RCP PIPE, MISC. DEBRIS CLEANUP)

RECORD DRAWINGS
UPDATED 03/2014

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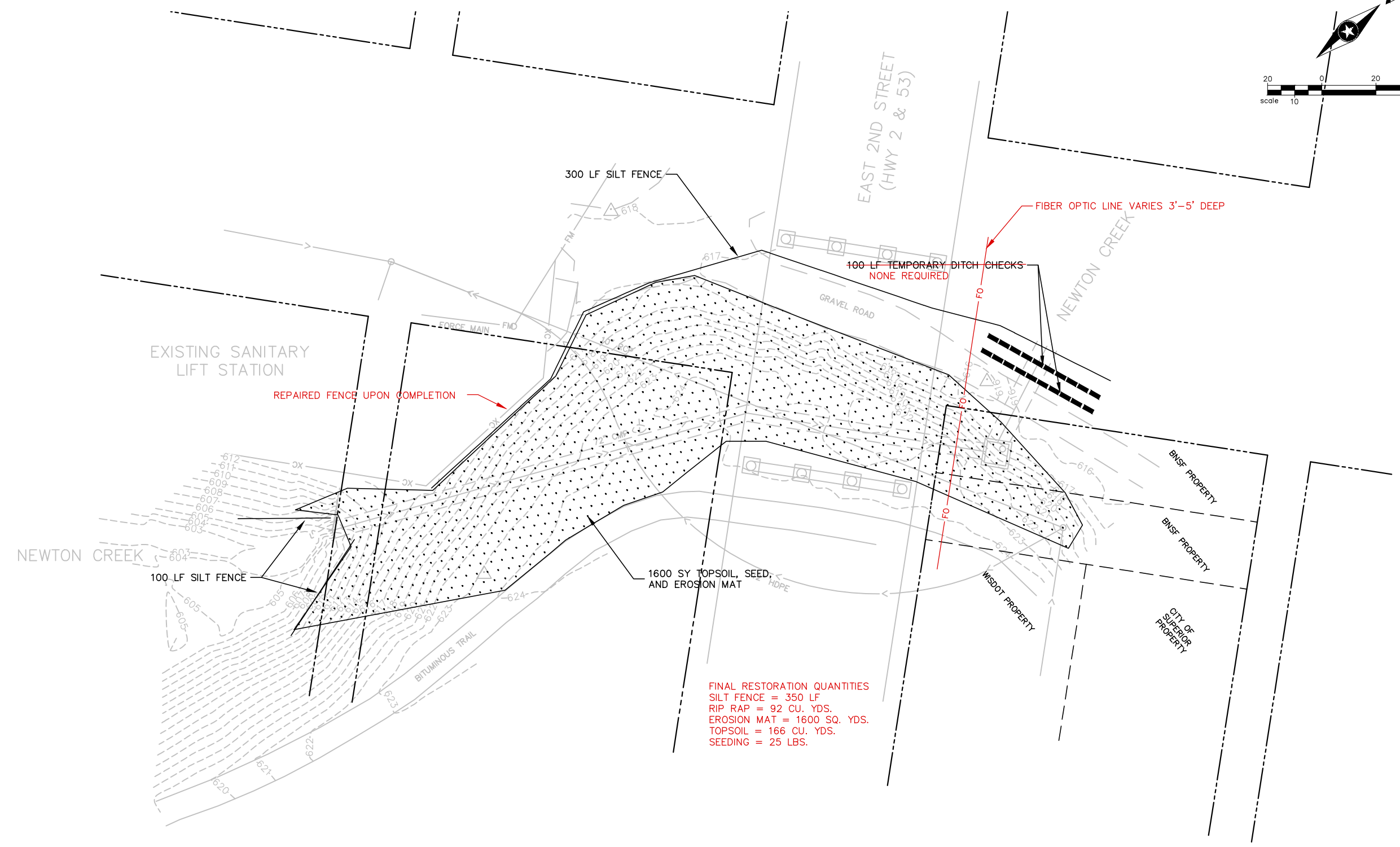
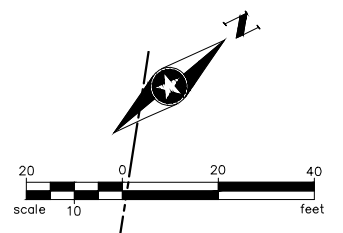
**SUPERIOR,
WISCONSIN**

**STATEMENT OF ESTIMATED QUANTITIES
/ DETAILS
NEWTON CREEK FLOOD REPAIR**

FILE NO.
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FINAL RESTORATION QUANTITIES
 SILT FENCE = 350 LF
 RIP RAP = 92 CU. YDS.
 EROSION MAT = 1600 SQ. YDS.
 TOPSOIL = 166 CU. YDS.
 SEEDING = 25 LBS.

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS LEVEL D. THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02 ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR SHALL CONDUCT A LOCATE OF ALL UTILITIES ON SITE PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH OTHER EXISTING UTILITY OWNERS.

**RECORD DRAWINGS
 UPDATED 03/2014**

NEWTON CREEK FLOOD REPAIR

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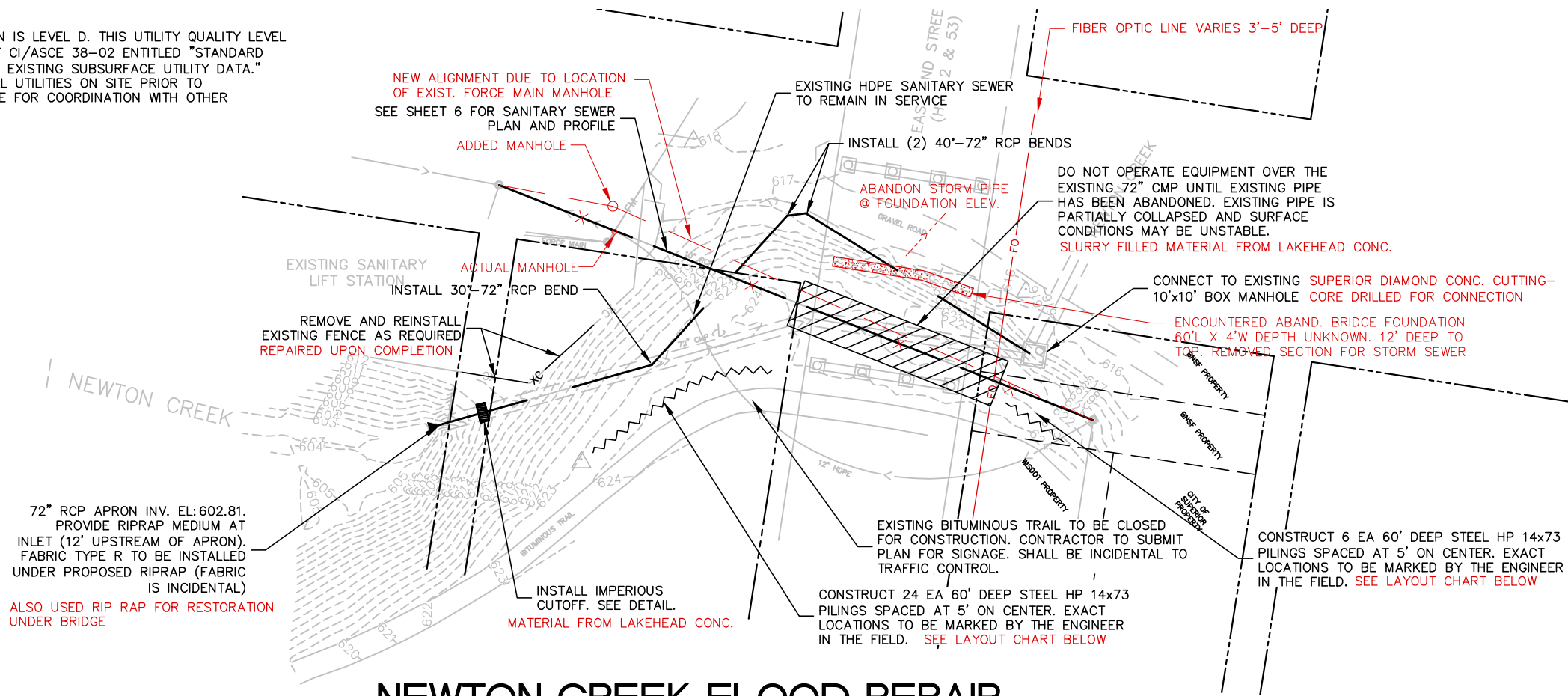
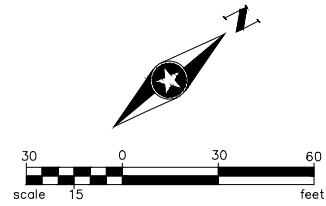
**SUPERIOR,
 WISCONSIN**

**EXISTING CONDITIONS AND
 EROSION CONTROL PLAN
 NEWTON CREEK FLOOD REPAIR**

FILE NO.
 SUPER 122115

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THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS LEVEL D. THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02 ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR SHALL CONDUCT A LOCATE OF ALL UTILITIES ON SITE PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH OTHER EXISTING UTILITY OWNERS.

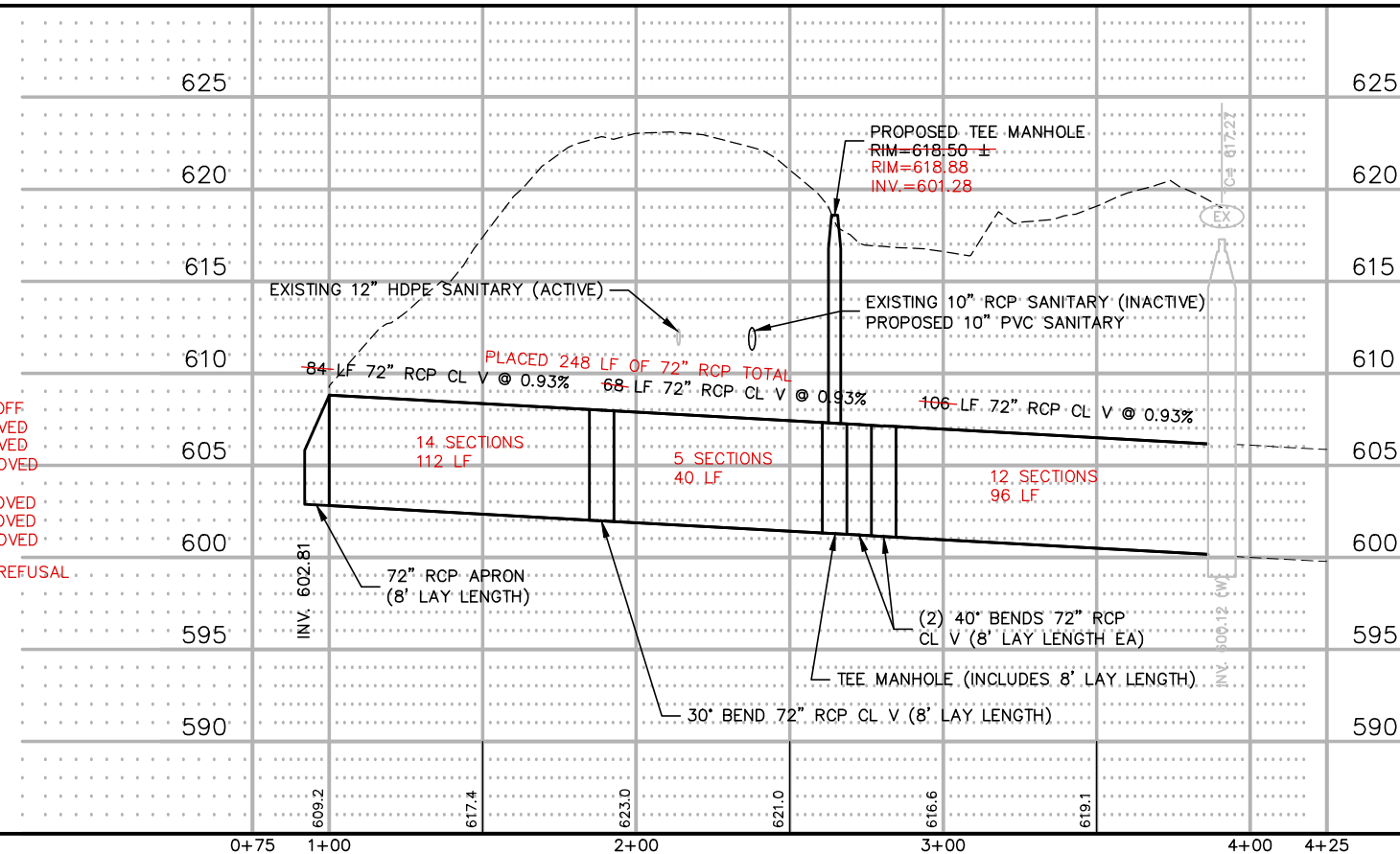


NEWTON CREEK FLOOD REPAIR

STEEL HP PILE LAYOUT TO SE (MEAS. FROM SOUTH BRIDGE LINE)

- No. 1: 4' S./7' W. OF TRAIL EDGE-FULL DEPTH
- No. 2: 12' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 3: 17' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 4: 22' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 5: 27' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 6: 32' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 7: 37' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 8: 42' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 9: 47' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 10: 52' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 11: 57' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 12: 62' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 13: 67' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 14: 72' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 15: 77' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 16: 82' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 17: 87' S./5' W. OF TRAIL EDGE-REFUSAL @ 18' DEEP, CUT OFF
- No. 18: 92' S./5' W. OF TRAIL EDGE-REFUSAL @ 13' DEEP, REMOVED
- No. 19: 97' S./5' W. OF TRAIL EDGE-REFUSAL @ 14' DEEP, REMOVED
- No. 20: 102' S./5' W. OF TRAIL EDGE-REFUSAL @ 14' DEEP, REMOVED
- No. 21: 107' S./5' W. OF TRAIL EDGE-FULL DEPTH
- No. 22: 112' S./5' W. OF TRAIL EDGE-REFUSAL @ 13' DEEP, REMOVED
- No. 23: 117' S./5' W. OF TRAIL EDGE-REFUSAL @ 14' DEEP, REMOVED
- No. 24: 122' S./5' W. OF TRAIL EDGE-REFUSAL @ 13' DEEP, REMOVED

POSSIBLY ENCOUNTERED ABANDON BRIDGE ABUTMENT @ PILES W/REFUSAL. MOVED PILES 1 & 2 DUE TO SAN. LOCATION



STEEL HP PILE LAYOUT TO NE (MEAS. FROM NORTH BRIDGE LINE)

- No. 1: 4' N./5' W. OF TRAIL EDGE-FULL DEPTH
 - No. 2: 11' N./5' W. OF TRAIL EDGE-FULL DEPTH
 - No. 3: 16' N./8' W. OF TRAIL EDGE-FULL DEPTH
 - No. 4: 21' N./9' W. OF TRAIL EDGE-FULL DEPTH
 - No. 5: 26' N./5' W. OF TRAIL EDGE-FULL DEPTH
 - No. 6: 31' N./5' W. OF TRAIL EDGE-FULL DEPTH
- EXIST. BURIED FIBER OPTIC LINE BETWEEN PILES 1 & 2
MOVED PILES 3 & 4 OUT TO W. DUE TO SAN. LOCATION

RECORD DRAWINGS
UPDATED 03/2014

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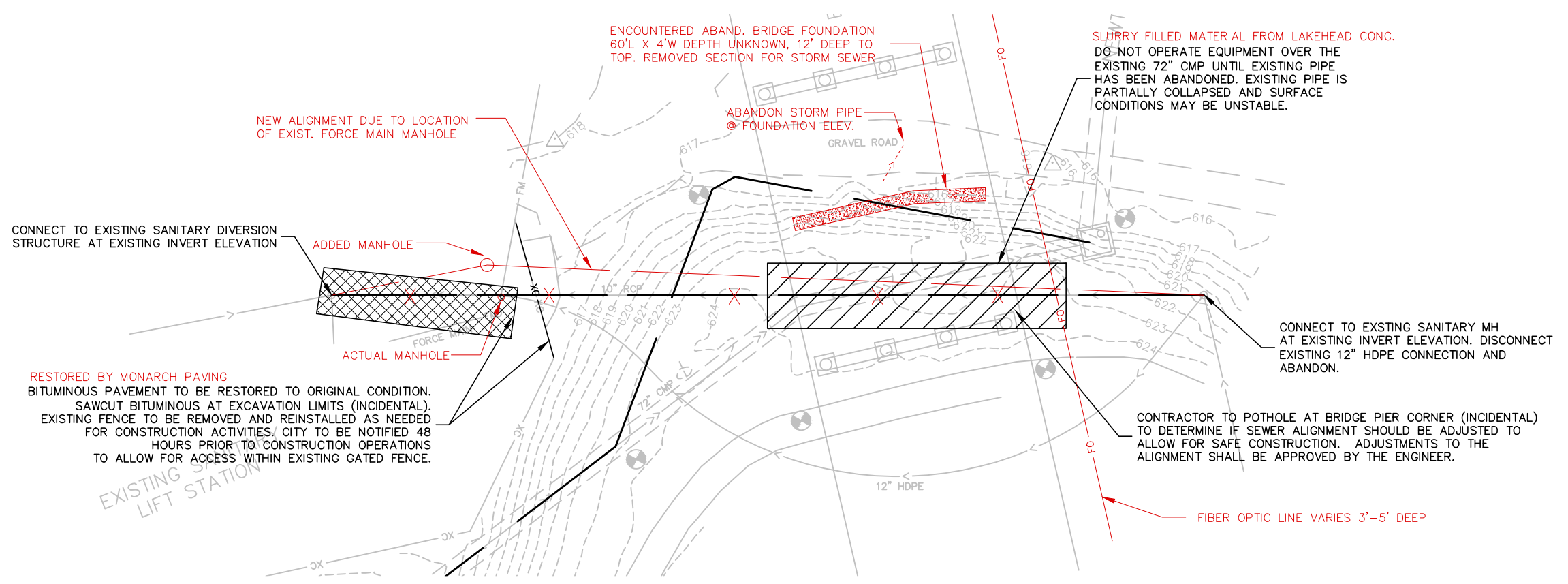
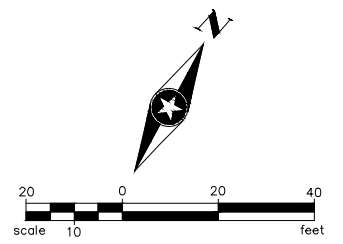
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**SUPERIOR,
WISCONSIN**

**STORM SEWER
PLAN AND PROFILE
NEWTON CREEK FLOOD REPAIR**

FILE NO. **546**
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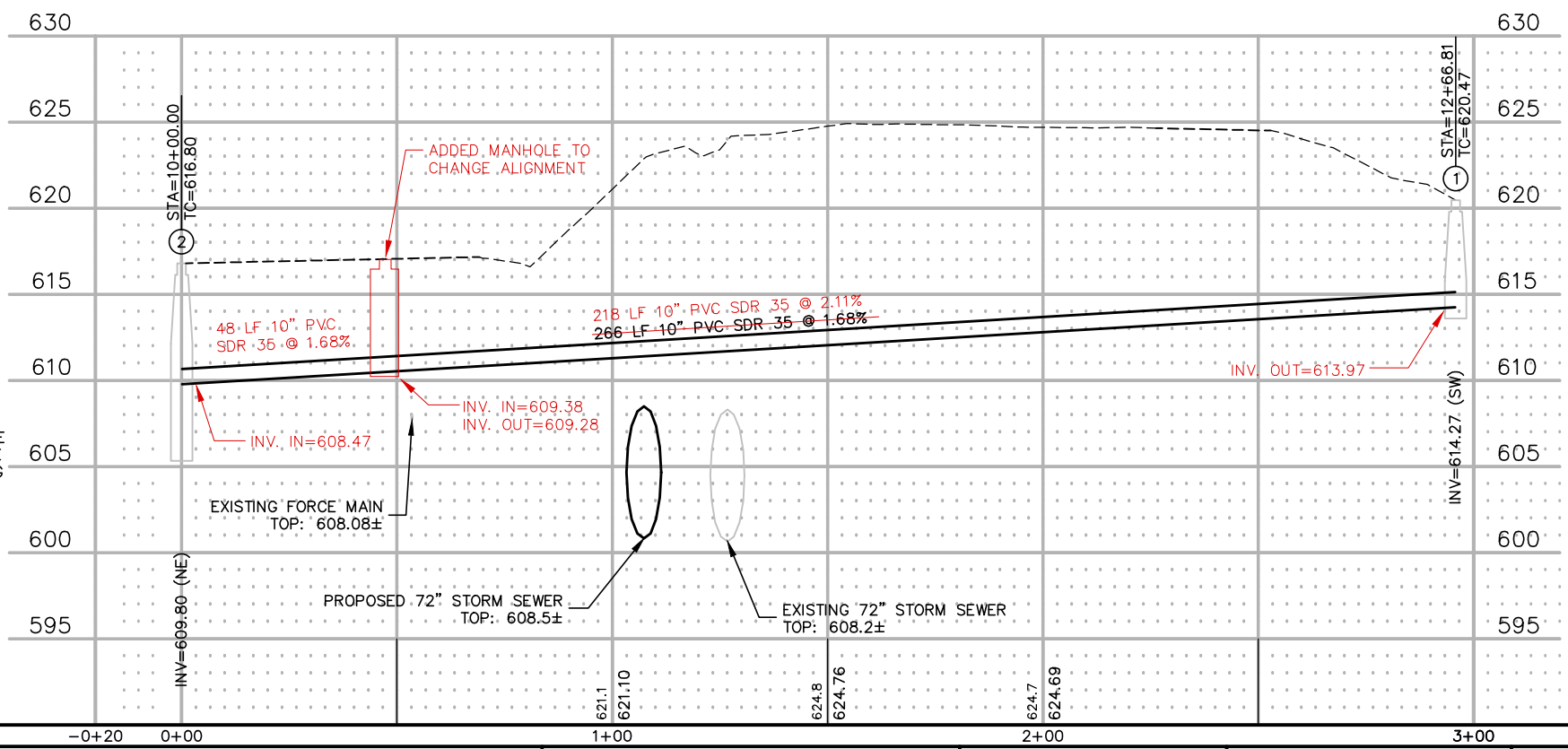
BENCHMARK EL.: 623.22
DESCRIPTION: 60d

NEWTON CREEK FLOOD REPAIR

BENCHMARK EL.: 623.22
DESCRIPTION: 60d

NOTES:

1. THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS LEVEL D. THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-02 ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR SHALL CONDUCT A LOCATE OF ALL UTILITIES ON SITE PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH OTHER EXISTING UTILITY OWNERS.
2. ALL PIPING AND MANHOLE CONNECTIONS SHALL BE INSTALLED ACCORDING TO THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR SEWER AND WATER CONSTRUCTION IN WISCONSIN".
3. THE PROPOSED 10" PVC PIPE SHALL GENERALLY FOLLOW ALONG THE ALIGNMENT OF THE EXISTING 10" RCP. PORTIONS OF THE EXISTING RCP MAY BE OUT OF THE ALIGNMENT SHOWN DUE TO DAMAGE FROM PARTIALLY COLLAPSED STORM PIPE.
4. CONNECTION TO THE EXISTING MANHOLE SHALL BE BY MEANS OF AN APPROVED FLEXIBLE WATERTIGHT PIPE TO MANHOLE SEAL. THIS SEAL SHALL MEET THE PHYSICAL REQUIREMENTS OF ASTM C-443 AND THE PERFORMANCE REQUIREMENTS OF ASTM C-425 AND C-433. PIPE ENTERING A MANHOLE THROUGH THIS SEAL SHALL BE LAID IN ACCORDANCE WITH THE BEDDING SECTION REQUIREMENTS AND SHALL NOT BE RIGIDLY SUPPORTED AS REQUIRED FOR NONFLEXIBLE CONNECTIONS. TO MAINTAIN THE SEAL FLEXIBILITY THAT PORTION OF THE ANNULAR SPACE BETWEEN THE PIPE AND THE MANHOLE WALL SHALL BE PLUGGED WITH BUTYL RUBBER GASKET MATERIAL PRIOR TO THE PLACING OF CONCRETE IN THE MANHOLE.
5. CONNECTION TO THE EXISTING DIVERSION STRUCTURE SHALL BE A DOUBLE LINK SEAL CONNECTION.



NOTES:

6. TRACER WIRE SHALL BE INSTALLED ALONG THE PROPOSED SANITARY SEWER. TRACER WIRE BOX TYPE AND LOCATION SHALL BE APPROVED IN THE FIELD BY THE ENGINEER.
7. THE NEW SANITARY SEWER LINE SHALL BE TESTED ACCORDING TO THE SPECIAL PROVISIONS, INCLUDING TELEVISION, AIR TESTING, AND DEFLECTION TESTING.
8. CONTRACTOR TO FIELD VERIFY EXISTING PIPE INVERTS AND PROPOSED PIPE SLOPE PRIOR TO INSTALLATION OF SANITARY SEWER PIPE.
9. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SANITARY FLOW THROUGHOUT THE COURSE OF CONSTRUCTION. CONTRACTOR SHALL NOTIFY THE CITY A MINIMUM OF 48 HOURS IN ADVANCE OF ANY POSSIBLE DISRUPTION OF FLOW.

**RECORD DRAWINGS
UPDATED 03/2014**

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**SUPERIOR,
WISCONSIN**

**SANITARY SEWER
PLAN AND PROFILE
NEWTON CREEK FLOOD REPAIR**

FILE NO.
SUPER 122115

65
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Hog Island

SECTION OF E 2ND STREET (US HWY 2 & US HWY 53)
TO BE CLOSED AT THE DIRECTION OF THE ENGINEER.
SEE NOTES THIS SHEET.

① SEE S.D.D. 15C 3-1

NOTES:

ALL TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM AND BE PLACED OR CONSTRUCTED IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND PART VI, "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS."

ALL SIGNAGE TO BE PLACED "FACE DOWN" PRIOR TO CONSTRUCTION. SHOULD TRAFFIC NEED TO BE DETOURED AWAY FROM THE EXISTING BRIDGE DUE TO UNSTABLE CONDITIONS, SIGNAGE SHALL IMMEDIATELY BE FLIPPED UP TO START DETOURING TRAFFIC.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS ARE APPROXIMATE AND SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

DURING HOURS OF DARKNESS ALL BARRICADES USED TO SHIELD A HAZARD SHALL BE EQUIPPED WITH WARNING LIGHTS, TYPE A.

UNLESS OTHERWISE NOTED, THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES DURING CONSTRUCTION.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS OR PAVEMENT MARKING, TEMPORARY OR EXISTING, WHICH MAY CONFLICT WITH THE CONSTRUCTION TRAFFIC PATTERN SHALL BE REMOVED OR COVERED.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (mph)	SPACING OF ADVANCE WARNING SIGNS (A) FEET	TAPER LENGTH (L) FEET	SPACING OF CHANNELIZING DEVICES (G) FEET	BUFFER SPACE (B) FEET
0-30	250	200	25	85
35-40	325	350	25	170
45-50	400	600	50	280
55	750	700	50	335
60-65	1000	800	50	485

RECORD DRAWINGS
UPDATED 03/2014

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CITY OF SUPERIOR
 NEWTON CREEK
 CULVERT

TRAFFIC CONTROL
 NEWTON CREEK FLOOD REPAIR

FILE NO.
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