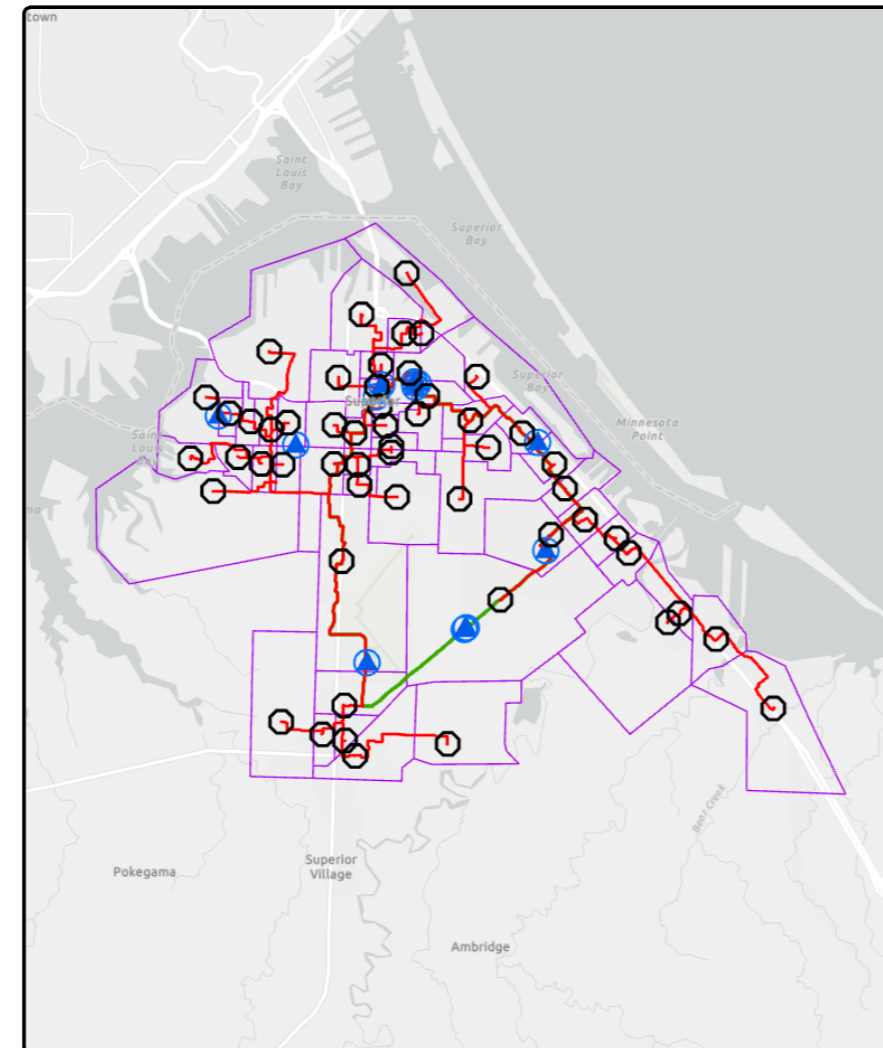


CITY OF SUPERIOR, WI



THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES, WHETHER PUBLIC OR PRIVATE, PRIOR TO EXCAVATION OR BORING. THE INFORMATION AND DATA SHOWN WITH RESPECT TO EXISTING UNDERGROUND FACILITIES AT OR CONTIGUOUS TO THE SITE IS APPROXIMATE AND BASED ON PHYSICAL APPURTENANCES OBSERVED IN THE FIELD. THE OWNER, CITY OF SUPERIOR SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR THE COMPLETENESS OF ANY SUCH INFORMATION AND DATA; AND, THE CONTRACTOR SHALL HAVE FULL RESPONSIBILITY FOR REVIEWING AND CHECKING ALL SUCH INFORMATION AND DATA, FOR LOCATING ALL UNDERGROUND FACILITIES, FOR COORDINATION OF THE WORK WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES DURING CONSTRUCTION, FOR THE SAFETY AND PROTECTION THEREOF, AND FOR REPAIRING ANY DAMAGE THERETO RESULTING FROM THE WORK. THE COST OF ALL OF WHICH WILL BE CONSIDERED AS HAVING BEEN INCLUDED IN THE CONTRACT PRICE. THE CONTRACTOR SHALL NOTIFY ANY AFFECTED UTILITY COMPANIES OR AGENCIES IN WRITING AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION.



LOCATION MAP
SCALE: N.T.S.

APPROVED FOR CONSTRUCTION

DATE CITY ENGINEER

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-01

ENGINEER: SM

DRAWN BY: JW

CHECKED BY: JW

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2024-05-21

WISCONSIN PROFESSIONAL ENGINEER

SCOTT D BOWLES
E-405146
NAPLES, FL

Scott D. Bowles

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NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED



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LIST OF CONTACTS

FACILITY OWNER

CITY OF SUPERIOR
 Contact Name : Stephanie Beckon
 Contact Phone: 715-395-1496
 Contact Email: beckens@superiorwi.gov

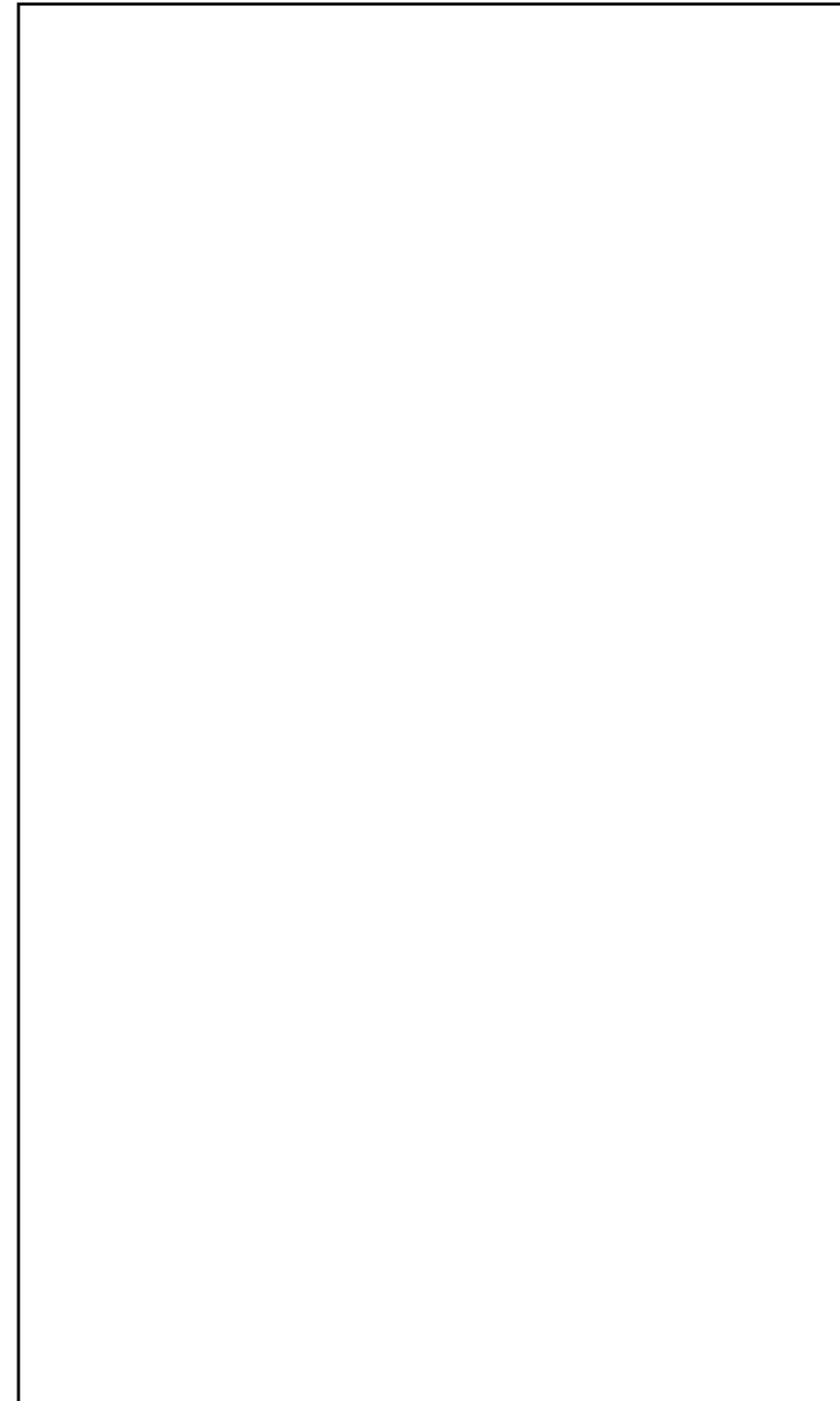
ENGINEERING

EN COMMUNICATIONS
 Contact Name : Tom Cheski
 Contact Phone: 6303534106
 Contact Email: tcheski@ENTRUSTSOL.com

Contact Name : Shawn Morris
 Contact Phone: 7853175887
 Contact Email: smorris@ENTRUSTSOL.com

Contact Name : Austin Gillard
 Contact Phone: 5738373711
 Contact Email: agillard@ENTRUSTSOL.com

UTILITY CONTACTS CONT.



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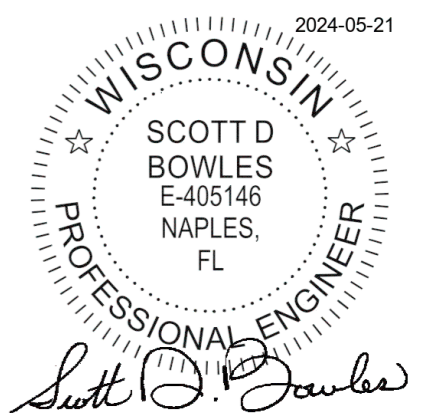
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ABBREVIATIONS

DB	DIRECTIONAL BORE
ED	EXISTING DUCT
EOP	EDGE OF PAVEMENT
FOC	FIBER OPTIC CABLE
HDD	HORIZONTAL DIRECTIONAL DRILL
HDPE	HIGH DENSITY POLY ETHYLENE
HH	HAND HOLE (VAULT)
HUT	LOCATION WITH ACTIVE ELECTRONICS SERVING FIBER
FDH	FIBER DISTRIBUTION HUB (PASSIVE CABINET)
MCA	MID CABLE ACCESS (SPLICE POINT)
MDU	MULTI DWELLING UNIT
NAP	NETWORK ACCESS POINT
POP	POINT OF PRESENCE (HUT OR DATACENTER, CITY HALL)
PUE	PUBLIC UTILITY EASEMENT
ROW	RIGHT OF WAY

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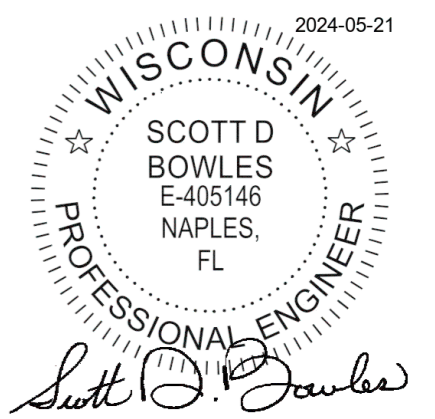
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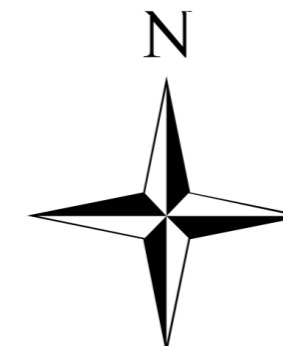
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GENERAL NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES, WHETHER PUBLIC OR PRIVATE, PRIOR TO EXCAVATION OR BORING. THE INFORMATION AND DATA SHOWN WITH RESPECT TO EXISTING UNDERGROUND FACILITIES AT OR CONTIGUOUS TO THE SITE IS APPROXIMATE AND BASED ON PHYSICAL APPURTENANCES OBSERVED IN THE FIELD. THE OWNER, CITY OF SUPERIOR, SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR THE COMPLETENESS OF ANY SUCH INFORMATION AND DATA; AND, THE CONTRACTOR SHALL HAVE FULL RESPONSIBILITY FOR REVIEWING AND CHECKING ALL SUCH INFORMATION AND DATA, FOR LOCATING ALL UNDERGROUND FACILITIES, FOR COORDINATION OF THE WORK WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES DURING CONSTRUCTION, FOR THE SAFETY AND PROTECTION THEREOF, AND FOR REPAIRING ANY DAMAGE THERETO RESULTING FROM THE WORK. THE COST OF ALL OF WHICH WILL BE CONSIDERED AS HAVING BEEN INCLUDED IN THE CONTRACT PRICE. THE CONTRACTOR SHALL NOTIFY ANY AFFECTED UTILITY COMPANIES OR AGENCIES IN WRITING AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION.

2. STATE LAW REQUIRES CONTRACTORS TO CALL FOR LOCATES (811), NO LESS THAN 2 BUSINESS DAYS OR NO MORE THAN 3 BUSINESS DAYS BEFORE BEGINNING ANY EXCAVATION OR DEMOLITION. NOT ALL UTILITY AGENCIES/OWNERS ARE MEMBERS OF THE ONE-CALL SYSTEM.

MAINTAIN AN 18-INCH BUFFER ZONE ON EACH SIDE OF A MARKED FACILITY.

NO MECHANIZED EQUIPMENT CAN BE USED WITHIN THE BUFFER ZONE, ONLY HAND DIGGING IS ALLOWED. EXCAVATION, ACCORDING TO THE LAW, MEANS ANYTHING THAT MOVES, REMOVES, OR DISPLACES EARTH, ROCK, OR OTHER MATERIAL IN OR ON THE GROUND. WISC STATE STATUTE 182.0175

3. EXISTING UTILITY INFORMATION DEPICTED ON THE PLANS IS IN ACCORDANCE WITH THE AMERICAN SOCIETY OF CIVIL ENGINEER'S (ASCE 38-22) STANDARD GUIDELINE FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA QUALITY LEVEL D (ASCE-38-22 QL D).

4. THE WISDOT STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION AND CITY OF SUPERIOR CONSTRUCTION SPECIFICATIONS SHALL BE REFERENCED FOR ALL EROSION AND RESTORATION WORK FOR THE PROJECT. SEE SDD 08E08 - 03, SHEET-COSR-18.

5. THE CONTRACTOR IS RESPONSIBLE FOR DEVELOPING A BEST MANAGEMENT PRACTICE (BMP) FOR THE PROJECT. THE BMP SHALL BE APPROVED BY THE CITY OF SUPERIOR PRIOR TO BEGINNING CONSTRUCTION. THE BMP SHALL BE UPDATED MAINTAINED AND UPDATED AS THE PROJECT PROGRESSES.

6. CONTRACTOR SHALL FIELD LOCATE EXISTING UTILITY LINES TO BE CONNECTED TO PRIOR TO COMMENCING WORK. IF A DISCREPANCY EXISTS BETWEEN THE PLANS AND ACTUAL FIELD CONDITIONS THE CONTRACTOR SHALL NOTIFY THE PROJECT MANAGER IMMEDIATELY.

7. CONTRACTOR SHALL TAKE SPECIAL PRECAUTIONS IN VICINITY OF ANY OVERHEAD ELECTRIC LINES. CONTRACTOR SHALL ABIDE BY NATIONAL ELECTRIC CODE AND ANY REQUIREMENT BY OWNER OF ELECTRIC LINE.

8. CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT MANAGER OF ANY DAMAGE OR CHANGED CONDITION CAUSED BY CONSTRUCTION ACTIVITIES THAT MAY RESULT IN A DISRUPTION OF SANITARY SEWER SERVICE.

9. THE CONTRACTOR SHALL NOT COMMENCE WORK BEFORE 6:00AM OR PER TRAFFIC CONTROL RULES. NO WORK IS ALLOWED AFTER 8:00PM OR WEEKENDS WITHOUT WRITTEN APPROVAL OF SUPERIOR CITY PUBLIC WORKS DEPARTMENT. REFERENCE SHEET-CON-10, JOB CONDITIONS AND PARKING.

10. THE CONTRACTOR SHALL RE-ESTABLISH ANY PROPERTY MARKER, BENCHMARK, ETC. DISTURBED DURING CONSTRUCTION TO ITS ORIGINAL LOCATION AND ELEVATION. WISDOT SECTION 785.1

11. THE CONTRACTOR SHALL TAKE ALL AVAILABLE PRECAUTIONS TO CONTROL DUST. CONTRACTOR SHALL CONTROL DUST BY SPRINKLING WATER, OR AS APPROVED BY OWNER, CITY OF SUPERIOR, WI.

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CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
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SHEET-CON-01

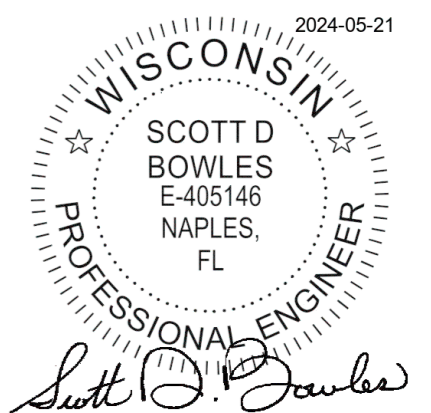
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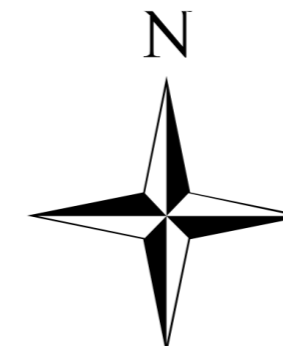
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CITY OF SUPERIOR, WI
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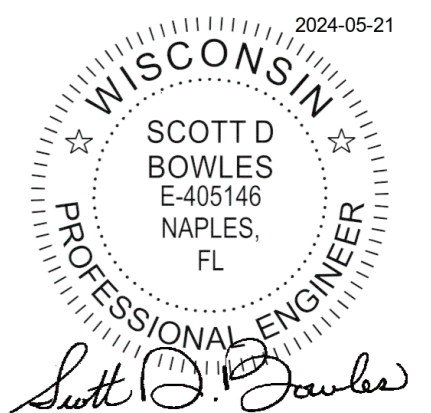
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12. THE CONTRACTOR SHALL NOT PLACE FILL OR WASTE MATERIAL ON ANY PRIVATE PROPERTY WITHOUT PRIOR WRITTEN AGREEMENT WITH PROPERTY OWNER.

13. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE WISDOT STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION AND CITY OF SUPERIOR CONSTRUCTION SPECIFICATIONS EXCEPT WHERE MODIFIED IN THESE PLANS OR THE SPECIFICATIONS.

14. NO MATERIALS MAY BE STORED IN THE ROADWAY PAVEMENT AREA & CURB/GUTTER OR SIDEWALK SO AS TO BLOCK DRAINAGE OR CREATE A HAZARD TO THE TRAVELING PUBLIC. ANY STOCKPILE THAT REMAINS IN THE ROADWAY AT THE END OF THE WORKDAY SHALL HAVE MUTCD COMPLIANT TRAFFIC CONTROL INSTALLED UNTIL SUCH TIME AS THE STOCKPILE IS REMOVED & CLEANUP IS COMPLETE. NO ADDITIONAL PAYMENT WILL BE MADE FOR INSTALLING EROSION CONTROL, ROADWAY & BOULEVARD CLEANUP, & HYDROSEEDING RESULTING FROM MATERIAL STOCK PILES. NO STOCKPILE SHALL BE STORED ON ANY ROADWAY THAT IMPEDES REGULAR TRAFFIC IN ALL DRIVING LANES AT ANY TIME DURING THE PROJECT.

15. THE CONTRACTOR SHALL ABIDE BY ALL APPLICABLE FEDERAL, STATE, AND LOCAL LAWS GOVERNING EXCAVATION. TRENCH SIDE SLOPES SHALL MEET OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) STANDARDS THAT ARE IN EFFECT AT THE TIME OF CONSTRUCTION. SHEETING, SHORING AND BRACING SHALL BE REQUIRED WHEN SIDE SLOPE STANDARDS ARE NOT MET. A PULL BOX, MEETING OSHA STANDARDS, MAY BE ACCEPTABLE, UNLESS NEGATED BY GROUNDWATER CONTROL MEASURES.

16. UNTIL THE WORK IS ACCEPTED BY THE OWNER, THE PROJECT SHALL BE UNDER THE CHARGE AND CUSTODY OF THE CONTRACTOR AND THE CONTRACTOR SHALL TAKE EVERY NECESSARY PRECAUTION AGAINST INJURY AND/OR DAMAGE TO THE WORK.

17. THE CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE ANY DAMAGE TO PRIVATE PROPERTY, INCLUDING, BUT NOT LIMITED TO, FENCES, WALLS, PAVEMENT, GRASS, AND TREES, AT NO ADDITIONAL COST TO THE OWNER. THIS WORK SHALL BE SUBSIDIARY TO THE COST OF THE CONTRACT UNLESS OTHERWISE NOTED.

18. CONTRACTOR SHALL INSTALL TEMPORARY BACK FILL AS REQUIRED FOR OPEN TRENCH IN ESTABLISHED ROADWAYS. NO OPEN TRENCH WILL BE ALLOWED IN EXISTING PAVEMENT EXCEPT DURING DAYLIGHT HOURS AND DURING CONSTRUCTION OPERATIONS. CONTRACTOR SHALL INSTALL TEMPORARY BACKFILL TO FOLLOW WISDOT & CITY STANDARDS UNLESS PERMISSION WITH RESTRICTIONS ARE GRANTED BY PUBLIC WORKS.

19. EXCAVATION BACKFILL IN 12-INCH OR SHALLOWER THOROUGHLY COMPACT LAYERS. TOPSOIL TO BE REPLACED IN 4" LAYER AND MUST CONSIST OF LOAM, SANDY LOAM, SILTY CLAY LOAM OR CLAY LOAM HUMUS-BEARING SOILS ADAPTED TO SUSTAIN PLANT LIFE AND ENSURE PH RANGES FROM 6.0-7.0 TOPSOIL SHALL BE KEPT SEPARATE FROM GENERAL TRENCH EXCAVATED MATERIAL AND SHALL BE PLACED ON TOP OF TRENCH BACK FILL. CONTRACTOR SHALL REMOVE ALL ROCK FROM TOPSOIL IN CULTIVATED AREAS. REFER TO WISDOT 625.2 AND WISDOT 651.3.1.

20. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND SUBMITTING A TRENCH SAFETY PLAN, PREPARED BY A PROFESSIONAL ENGINEER IN THE STATE OF WISCONSIN, TO THE OWNER, CITY OF SUPERIOR, WI PRIOR TO CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TRENCH SAFETY REQUIREMENTS IN ACCORDANCE WITH CITY, STATE, AND FEDERAL REQUIREMENTS, INCLUDING OSHA FOR ALL TRENCHES. NO OPEN TRENCHES SHALL BE ALLOWED OVERNIGHT WITHOUT PRIOR WRITTEN APPROVAL OF THE CITY.

21. IN ORDER TO MINIMIZE IMPACT TO LANDSCAPING MATERIAL, THE CONTRACTOR SHALL EXERCISE CAUTION THROUGH LANDSCAPING LIMITS DURING ALL PHASES OF CONSTRUCTION ACTIVITY. ANY LANDSCAPE MATERIAL DAMAGED DURING THE CONSTRUCTION PROCESS SHALL BE REPLACED IN KIND AT THE CONTRACTORS EXPENSE. REFERENCE SHEET-TREES-01-04 FOR FURTHER STANDARDS AND DETAILS

22. ALL APPLICABLE PROVISIONS OF EXISTING UTILITY EASEMENTS WILL BE ADHERED TO BY THE CONTRACTOR.

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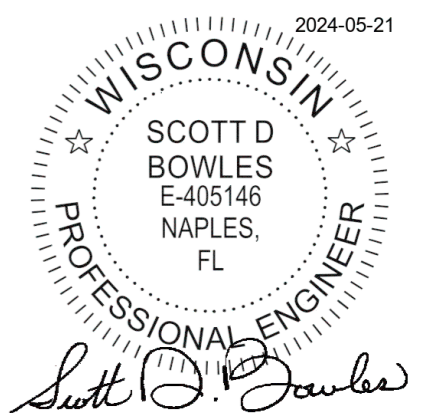
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23. THE CONTRACTOR SHALL AVOID AND/OR PROTECT ALL TREES AND ROOTS BY HAND DIGGING AS NECESSARY. ANY TREES, SHRUBS OR VEGETATION DAMAGED BY THE CONTRACTOR SHALL BE REPLACED IN KIND AT NO COST TO THE CITY. REFER TO SHEET-TREES-01 THROUGH 04 FOR FURTHER STANDARDS AND DETAILS.

24. WORK SHALL NOT START UNTIL THE CONTRACTOR HAS ALL NECESSARY PERMITS FROM THE APPROPRIATE GOVERNING REGULATORY AGENCIES, INCLUDING BUT NOT LIMITED TO THE CITY OF SUPERIOR, APPROPRIATE RAILROAD, WISDOT, AND SUPERIOR WATER LIGHT & POWER

25. THE CONTRACTOR MUST SEED ALL DISTURBED AREAS UPON COMPLETION OF PROJECT. FOLLOW SPECIFICATIONS FOUND IN WISDOT 107 LEGAL RELATIONS AND RESPONSIBILITY TO THE PUBLIC 107.20 EROSION CONTROL PART 4&6, SHEET-TYP-02.

26. ANY APPLICANT OR CONTRACTOR DOING WORK WITHIN THE CITY'S RIGHT OF WAY MUST RESTORE THE AREA TO THE PREVIOUS CONDITION AND LEAVE THE AREA IN A CLEAN AND NEAT CONDITION. ANY FUTURE COMPLAINTS ABOUT CUT OR DAMAGED TILES OR POORLY FINISHED SLOPES OR SURFACES WILL BE DIRECTED BACK TO THE CONTRACTOR TO BE CORRECTED. REFERENCE SDD 08D01 - A THROUGH SDD 08D21 ATTACHED

UTILITIES TO BE PLACED ALONG PAVED ROADS:

1. SHALL BE INSTALLED PER THE APPROVED PERMITS AND DRAWINGS AND IN RESPECT TO EXISTING UTILITIES & LOCATES.
2. SHALL NOT CUT OR DISTURB ANY DRAINAGE TILES. THE CONTRACTOR MUST BORE UNDER TILE LINES A MINIMUM OF THIRTY-SIX (36) INCHES.
3. ALL CONSTRUCTION SITE RESTORATION TO CONFORM TO CITY AND WISDOT STANDARDS. REFERENCE SHEET-TYP-02, RESTORATION.
4. REFERENCE WISDOT HMM 09-15-25 LOCATION REQUIREMENTS

UTILITIES TO BE PLACED ALONG GRAVEL ROADS:

1. SHALL BE INSTALLED PER THE APPROVED PERMITS AND DRAWINGS AND IN RESPECT TO EXISTING UTILITIES & LOCATES.

2. SHALL NOT CUT OR DISTURB ANY DRAINAGE TILES, CULVERTS OR BRIDGES. THE CONTRACTOR MUST BORE UNDER TILE AND CULVERT A MINIMUM OF THIRTY-SIX (36) INCHES. UTILITIES MAY BE INSTALLED AROUND A CULVERT OR BRIDGE WITH A MINIMUM OF 15 TO BE MAINTAINED FROM ALL SIDES OF THE STRUCTURE. REFERENCE SHEET-TYP-04.

3. ALL CONSTRUCTION SITE RESTORATION TO CONFORM TO CITY AND WISDOT STANDARDS. REFERENCE SHEET-TYP-02, RESTORATION.

4. REFERENCE WISDOT HMM 09-15-25 LOCATION REQUIREMENTS

ROADWAY CROSSINGS:

1. ALL UTILITIES SHOULD CROSS THE ROAD AT A 90° ANGLE.
2. ALL CROSS ROAD BORE PITS SHALL BE BACKFILLED IN LAYERS NOT MORE THAN EIGHT (8) INCHES IN LOOSE THICKNESS. BASE LAYER TO BE STRUCTURAL BACKFILL (CMM 325 & WISDOT 209.1) AND EIGHT (8) INCHES OF 1 1/4" BAD (WISDOT 305) COMPACTED. FURTHER BACKFILL LAYERS TO MATCH EXISTING SECTION ABOVE. EACH LAYER SHALL BE THOROUGHLY COMPACTED BEFORE THE NEXT LAYER IS PLACED. 625.2 AND WISDOT 651.3.1.

SAWCUT PAVEMENTS A MINIMUM OF 12" FROM THE AREA TO BE EXCAVATED.

3. DUE TO THE HEAVY CLAY MAKEUP OF THE REGIONAL SOIL, THE TOP LAYER OF THE EXCAVATION BACKFILL IS TO 'CRUSH AND RUN' GRAVEL MATERIAL 1/4" AND SMALLER, NOT TO BE FINISHED WITH OPEN GRADED ROCK LARGER THAN 1/4". TOPSOIL REPLACEMENT IS REQUIRED IN ALL AREAS WHERE TOPSOIL EXISTS. REFERENCE WISDOT 209.1 GRANULAR BACKFILL AND CMM - 325 STRUCTURE EXCAVATION. TOPSOIL REFERENCE SHEET-CON-02, ITEM 18.

3. ALL PAVED ROADS SHALL HAVE THE UTILITIES BORED.
4. ALL CONSTRUCTION SITE RESTORATION TO CONFORM TO CITY AND WISDOT STANDARDS. REFERENCE SHEET-TYP-02, RESTORATION.
5. REFERENCE TYPICAL DRAWINGS ON SHEET-TYP-03 AND WISDOT HMM 09-15-25

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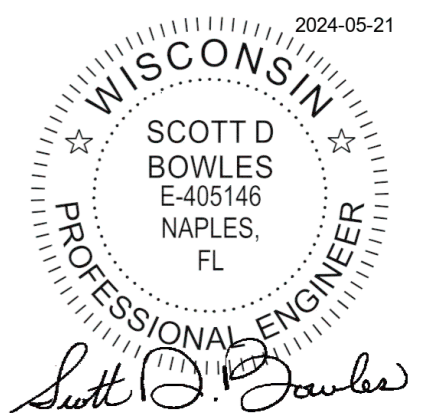
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EROSION CONTROL:

1. AN NPDES PERMIT FROM THE WISCONSIN DNR IS REQUIRED FOR ANY SITE THAT DISTURBS AND EXPOSES ONE ACRE OF LAND OR MORE. A PERMIT IS ALSO REQUIRED FOR PROJECTS THAT WILL DISTURB ONE OR MORE ACRES AS PART OF A COMMON PLAN OF DEVELOPMENT, EVEN IF THERE WILL NOT BE ONE ACRE OF DISTURBED GROUND EXPOSED AT ANY GIVEN TIME. IN ADDITION TO THE WISCONSIN DNR, MANY LOCAL AGENCIES ALSO HAVE A PERMIT PROCESS. IT IS NECESSARY TO CHECK WITH THE JURISDICTIONAL ENGINEER TO DETERMINE WHAT, IF ANY, INFORMATION IS NEEDED FOR THE LOCAL AGENCY PERMIT.
2. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NPDES PERMITS IF REQUIRED.
3. THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL EROSION CONTROL AND WATER QUALITY REQUIREMENTS, LAWS, AND ORDINANCES THAT APPLY TO THE CONSTRUCTION SITE LAND DISTURBANCE.
4. EROSION CONTROL DEVICES SHOWN ON THE EROSION CONTROL PLAN FOR THE PROJECT SHALL BE INSTALLED PRIOR TO THE START OF LAND DISTURBANCE. LAND DISTURBANCE SHOULD NOT PROCEED UNTIL PRE-CONSTRUCTION INSPECTION OF SITE HAS BEEN APPROVED BY CITY REPRESENTATIVE ON OVERSIGHT.
5. CONTRACTOR IS SOLELY RESPONSIBLE FOR INSTALLATION, IMPLEMENTATION, MAINTENANCE, AND EFFECTIVENESS, AND REMOVAL OF ALL EROSION CONTROL DEVICES, BEST MANAGEMENT PRACTICES (BMPS), AND FOR UPDATING THE EROSION CONTROL PLAN DURING CONSTRUCTION AS FIELD CONDITIONS CHANGE.
6. THE EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL THE AREA IT PROTECTS HAS BEEN PERMANENTLY STABILIZED, TO A UNIFORM, GOOD PERENNIAL VEGETATIVE COVER OF 70%(NO INVASIVE OR UNPERMITTED SPECIES).

7. CONTRACTOR SHALL OBSERVE THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES AND MAKE FIELD ADJUSTMENTS AND MODIFICATIONS AS NEEDED TO PREVENT SEDIMENT FROM LEAVING THE SITE. IF THE EROSION CONTROL DEVICES DO NOT EFFECTIVELY CONTROL EROSION AND PREVENT SEDIMENTATION FROM WASHING OFF THE SITE, THEN THE CONTRACTOR SHALL NOTIFY THE OWNER, CITY OF SUPERIOR. THE CITY RETAINS THE ABILITY TO ADD ADDITIONAL EROSION & SEDIMENT CONTROL PRACTICES IF NECESSARY. REFER TO SDD08E08 AND SDD08E09, ATTACHED.

8. OFF-SITE SOIL BORROW, SPOIL, AND STORAGE AREAS (IF APPLICABLE) ARE CONSIDERED AS PART OF THE PROJECT SITE AND MUST ALSO COMPLY WITH THE EROSION CONTROL REQUIREMENTS FOR THIS PROJECT. THIS INCLUDES THE INSTALLATION OF BMP S TO CONTROL EROSION AND SEDIMENTATION AND THE ESTABLISHMENT OF PERMANENT GROUND COVER ON DISTURBED AREAS PRIOR TO FINAL APPROVAL OF THE PROJECT. CONTRACTOR IS RESPONSIBLE FOR MODIFYING THE EROSION CONTROL PLAN TO INCLUDE BMP'S FOR ANY OFF-SITE THAT ARE NOT ANTICIPATED OR SHOWN ON THE EROSION CONTROL PLAN.

9. CONTRACTORS SHALL INSPECT ALL EROSION CONTROL DEVICES, BMPS, DISTURBED AREAS, AND VEHICLE ENTRY AND EXIT AREAS, AND KEEP A RECORD OF THIS INSPECTION TO VERIFY THAT THE DEVICES AND EROSION CONTROL PLAN ARE FUNCTIONING PROPERLY. INSPECTIONS TO OCCUR WEEKLY AND AFTER SIGNIFICANT RAIN EVENTS OF 0.5" OR MORE.

10. SITE ENTRY AND EXITS SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT THE TRACKING AND FLOWING OF SEDIMENT AND DIRT ONTO OFF-SITE ROADWAYS. ALL SEDIMENT AND DIRT FROM THE SITE THAT IS DEPOSITED ONTO AN OFF-SITE ROADWAY SHALL BE REMOVED IMMEDIATELY.

REFERENCE SHEET-COSR-18 AND SHEET-COSR-19 FOR TYPICAL INSTALLATIONS

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SHEET-CON-05

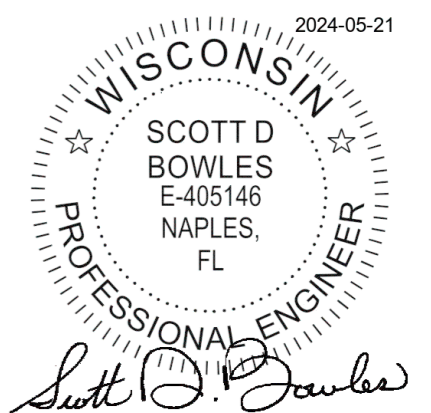
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11. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL SILT AND DEBRIS FROM THE AFFECTED OFF-SITE ROADWAYS THAT ARE A RESULT OF THE CONSTRUCTION, AS REQUESTED BY OWNER. AT A MINIMUM, THIS SHOULD OCCUR ONCE PER DAY FOR THE OFF-SITE ROADWAYS.

12. ALL FINES IMPOSED FOR SEDIMENT OR DIRT DISCHARGED FROM THE SITE SHALL BE PAID BY THE RESPONSIBLE CONTRACTOR.

13. TEMPORARY SEEDING OR OTHER APPROVED STABILIZATION SHALL BE INITIATED IMMEDIATELY OF ANY AREA, UNLESS ADDITIONAL CONSTRUCTION IN THE AREA IS EXPECTED WITHIN 14 DAYS OF THE LAST DISTURBANCE.

14. CONTRACTOR SHALL FOLLOW GOOD HOUSEKEEPING PRACTICES DURING CONSTRUCTION, ALWAYS CLEANING UP DIRT, LOOSE MATERIAL, AND TRASH AS CONSTRUCTION PROGRESSES.

15. UPON COMPLETION OF FINE GRADING, ALL SURFACES OF DISTURBED AREAS SHALL BE PERMANENTLY STABILIZED. STABILIZATION IS ACHIEVED WHEN THE AREA IS EITHER COVERED BY PERMANENT IMPERVIOUS STRUCTURES, SUCH AS BUILDINGS, SIDEWALK, PAVEMENT, OR A UNIFORM PERENNIAL VEGETATIVE COVER ACROSS ENTIRE SITE. REFER TO CITY OF SUPERIOR SIDEWALK DRAWINGS ATTACHED, SHEET-COSS-01 THROUGH 15.

16. THE CONTRACTOR SHALL REVEGETATE UNPAVED AREAS DISTURBED BY CONSTRUCTION PRIOR TO ACCEPTANCE OF THE PROJECT. REVEGETATION SHALL CONSIST OF SEED SOWING, GROUND COVER(NOT INCLUDING STRAW MULCH), FERTILIZING AND WATERING. REVEGETATION SHALL BE ACCEPTABLE WHEN VEGETATION ACHIEVES A UNIFORM PERENNIAL COVERAGE OF 70%. THIS ITEM SHALL BE CONSIDERED AS A SUBSIDIARY COST TO THE PROJECT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED. COMPLETE WORK IN ACCORDANCE WITH THE REQUIREMENTS WI DNR TECHNICAL STANDARDS 1059. SEED MIXTURE SHALL BE NO. 40, OR AS DETAILED IN THE PLANS, AS REFERENCED IN SECTION 630 OF THE WI DOT STANDARD SPECIFICATIONS. SEED SHALL BE SOWN USING HYDROSEED METHODS.

REFERENCE SHEET-COSR-18 AND SHEET-COSR-19 FOR TYPICAL INSTALLATIONS

TRAFFIC CONTROL:

1. THE CONTRACTOR SHALL PREPARE A TRAFFIC CONTROL PLAN FOR THE ENTIRE PROJECT. THE CONTRACTOR WILL SUBMIT THE TRAFFIC CONTROL PLAN TO THE OWNER/ENGINEER PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES.

THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE CONTROLS IDENTIFIED IN THE PLAN AND CHANGES TO THE PLAN ONCE CONSTRUCTION BEGINS.

NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR COMPLIANCE WITH THE PROJECT TRAFFIC CONTROL PLAN. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR COMPLIANCE WITH REQUESTS FOR DOT INSPECTOR, IF APPLICABLE.

CONTRACTOR TO REFERENCE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD)

2. BARRICADES AND SIGNS SHALL BE PLACED IN SUCH A MANNER AS NOT TO INTERFERE WITH THE SIGHT DISTANCE OF DRIVERS ENTERING THE ROADWAYS OR SIDE STREETS. TO FACILITATE LANE SHIFTING, BARRICADES AND SIGNS USED IN LANE CLOSURES OR TRAFFIC STAGING MAY BE ERECTED AND MOUNTED ON PORTABLE SUPPORTS.

THE DESIGN OF THESE SUPPORTS SHALL CONFORM TO THE WMUTCD AND ARE SUBJECT TO THE APPROVAL OF THE CONTRACTOR.

3. ALL TRAFFIC CONTROL DEVICES (SIGNS, MARKINGS, BARRICADES, ETC.) USED TO WARN MOTORIST OF THE CONSTRUCTION ACTIVITY MUST CONFORM TO THE LATEST VERSION OF THE WMUTCD.

4. BARRICADES AND WARNING SIGNS, AS APPROPRIATE, TO FOLLOW CITY/COUNTY/STATE STANDARDS, WMUTCD.

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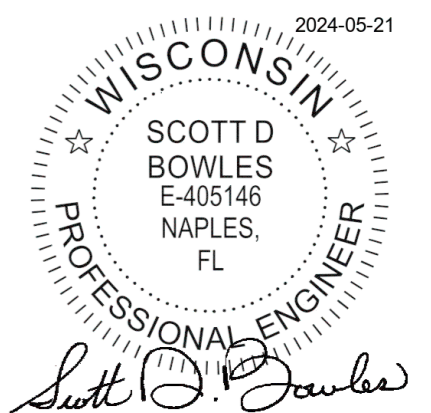
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CONDUIT:

1. THE FIBER OPTIC CONDUIT NETWORK SHALL BE MAINTAINED AT A CONSTANT HORIZONTAL AND VERTICAL LOCATION AS SHOWN IN THE ROADWAY CROSS SECTIONS OF THE ROADWAY PLANS, DRAINAGE PLANS, STRUCTURE PLANS AND OTHER PLAN COMPONENTS OF THIS PROJECT. REFERENCE WISDOT HMM 09-15-25 LOCATION REQUIREMENTS.
2. IF CONSTANT HORIZONTAL AND VERTICAL LOCATIONS CANNOT BE MAINTAINED BASED ON EXISTING UTILITIES OR OBSTACLES CONDUIT SHALL MAINTAIN A MINIMUM DEPTH OF 36 INCHES, MAINTAIN DISTANCES BASED ON EXISTING UTILITIES IDENTIFIED VIA LOCATES OBTAINED BY CONTRACTOR, AND PER THE DIRECTIVES OF APPROVED PERMITS. REFERENCE SHEET-TYP-01, UTILITY CROSSING.
3. CONDUIT RUN SHALL NOT EXCEED 180° OF BENDS OR PER MANUFACTURER RECOMMENDATIONS BETWEEN MANHOLES OR JUNCTION BOXES.
4. THE HDPE CONDUIT ENTERING A PROPOSED FIBER OPTIC PULLBOX SHALL NOT EXCEED 45 DEGREE ENTRY ANGLE OR PER MANUFACTURER RECOMMENDATIONS. REFERENCE SHEET-TYP-01, GRADE LEVEL HANDHOLE.
5. CONTRACTOR SHALL INSTALL MAXIMUM LAYING LENGTHS OF HDPE CONDUIT ELIMINATING CONNECTIONS OR JOINTS IN BETWEEN CONDUIT RUNS. A CITY OF SUPERIOR REPRESENTATIVE PRIOR APPROVAL IS REQUIRED FOR ANY CONNECTIONS JOINED WITH EITHER ELECTROFUSION COUPLING OR USING ANY OTHER MANUFACTURER'S RECOMMENDATIONS.
6. ALL EMPTY FIBER OPTIC CONDUITS SHALL BE CAPPED AND FURNISHED WITH A PULL STRING FOR FUTURE USE. IF DAMAGE HAS OCCURRED TO THE CONDUIT, THE CONTRACTOR SHALL REPLACE THE ENTIRE LENGTH OF CONDUIT BETWEEN THE CORRESPONDING JUNCTION BOXES OR ENCLOSURES.
7. IDENTIFY ALL CIRCUITS, FIBER, AND EQUIPMENT WITH APPROVED ID TAGS. REFERENCE SHEET-TYP-05.

8. THE TONE WIRE SHALL BE CONNECTED CONTINUOUSLY THROUGH EACH PULL BOX WITH A COPPER SPLIT BOLT FOR CONTINUITY TESTING AND SPLICING. THE TONE WIRE SHALL HAVE A MINIMUM OF 5 FT SPARE IN EACH PULL BOX. REFERENCE WISDOT 655 ELECTRICAL WIRING & SHEET-TYP-01
9. IF UTILIZING ARMORED FIBERCABLE, GROUND RODS WILL BE PLACED IN PULL BOXES AT NO MORE THAN 2,000' APART. ALL DEAD-END VAULTS AND VAULTS WITH MORE THAN TWO PATHS SHALL HAVE A GROUND ROD INSTALLED. REFERENCE SHEET-TYP-01.
10. GROUND RODS SHALL BE 8FT x 5/8" ALL COPPER RODS AND PLACED AT THE BOTTOM OF THE PULL BOX OR PER MANUFACTURER DIRECTION.
11. GROUND RODS SHALL HAVE A BONDING CLAMP INSTALLED ON THEM WITH A MINIMUM OF 15 FT OF 12 AWG TONE WIRE CONNECTED TO THE BONDING CLAMP. THE TONE WIRE FROM THE GROUND IS NOT TO BE CONNECTED TO THE CONTINUOUS TONE WIRE EXCEPT AT DEAD END PULL BOXES. WISDOT 655 ELECTRICAL WIRING
12. UPON COMPLETION, THE CONTRACTOR SHALL DEMONSTRATE THAT THE WIRE IS CONTINUOUS AND UNBROKEN THROUGH THE ENTIRE RUN OF DUCT BY PROVIDING FULL SIGNAL CONDUCTIVITY (INCLUDING SPLICES). IF THE WIRE IS BROKEN, THE CONTRACTOR SHALL REPAIR OR REPLACE IT. REFERENCE SHEET-TYP-01.
13. IF CITY AUTHORITIES REQUIRE, RAISED MARKERS INDICATING FIBER OPTIC CABLE BURIED BELOW SHALL BE INSTALLED AT NO MORE THAN 2,000' APART. ALL DEAD-END VAULTS AND VAULTS WITH MORE THAN TWO PATHS SHALL HAVE A RAISED MARKER INSTALLED. REFERENCE SHEET-TYP-04, FIBER OPTIC UTILITY MARKER.
14. ALL NEW UNDERGROUND CONDUIT SHALL BE PROPERLY SEALED AT BOTH ENDS WITH APPROVED DUCT PLUGS TO PREVENT THE ENTRY OF DUST, DIRT OR MOISTURE.

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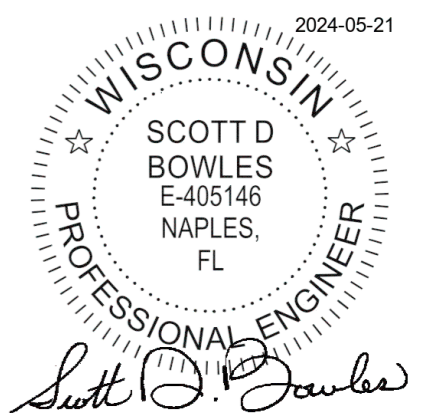
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15. ALL CONDUIT TRENCHES AND POTHOLES SHALL BE BACKFILLED COMPLETELY TO PROVIDE SAFE CROSSING BY THE END OF EACH WORKING DAY OR WHENEVER THE WORK ZONE BECOMES INACTIVE. THE CONTRACTOR SHALL NOT OPEN ANY AREA THAT CANNOT BE BACKFILLED IN THE SAME DAY/NIGHT OPERATION.

16. IT SHOULD BE NOTED THAT NO TEST BORINGS WERE MADE WHERE CONDUIT RUNS ARE TO BE INSTALLED BY JACKING OR TRENCHING. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE JOB SITE CONDITIONS BEFORE SUBMITTING BID PROPOSALS.

17. ALL HDPE CONDUIT SHALL BE SMOOTH OUTSIDE, HAVE A RATING OF SDR-11 OR THICKER. ALL PVC CONDUIT RATED SCHEDULE 40 OR THICKER.

18. IF OPEN TRENCH IS USED EARLY PROTECTION WARNING TAPE SHALL BE PLACED 12 INCHES ABOVE CONDUIT. REFERENCE SHEET-TYP-02, TYPICALS.

19. POTHOLING IS REQUIRED PRIOR TO CROSSING ANY CITY-OWNED UTILITY.

20. CONTRACTOR MUST MAINTAIN MINIMUM VERTICAL AND HORIZONTAL CLEARANCES FROM EXISTING UTILITIES AT ALL TIME AND AVOID CROSSING AT HIGHLY ACUTE ANGLES.

HANDHOLE, PULL BOX:

1. ALL FIBER OPTIC MARKERS SHALL HAVE "THE CITY OF SUPERIOR" MARKED ON THEM. APPROXIMATE LOCATIONS OF PULL BOXES ARE SHOWN IN THE PLANS. THE NUMBER OF PULL BOXES REQUIRED IN THE CONTRACT MAY VARY. CONTRACTORS SHALL SUBMIT AS-BUILT DRAWINGS AND GIS LOCATES/COORDINATES SHOWING FINAL LOCATIONS OF PULL BOXES.

2. PULL BOXES AND LIDS SHALL HAVE A MINIMUM ANSI/SCTE 77-2017 TIER 15 DESIGNATION FOR VAULTS PLACED IN GRASS OR NON-PAVED AREAS. FOR SIDEWALK APPLICATIONS PULL BOXES AND LIDS SHALL HAVE A MINIMUM ANSI/SCTE 77-2017 TIER 15 DESIGNATION.

3. THIS PLAN DOES NOT CALL FOR ANY PULL BOXES TO BE PLACED ON ROADWAYS, DRIVEWAYS, OR BRIDGES. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND PROJECT MANAGER IMMEDIATELY IF A PULL BOX NEEDS TO BE PLACED ON A ROADWAY OR DRIVEWAY.

4. ALL FIBER OPTIC MARKERS SHALL HAVE "City of SUPERIOR" AND CITY PHONE NUMBER MARKED ON THEM. THE WORDS "SWI COMM" SHALL BE CAST OR INSCRIBED IN THE SURFACE OF ALL BOX COVERS UNLESS OTHERWISE DIRECTED TO PROJECT SPECIFIC BRANDING INDICATED BY PROJECT MANAGER. REFERENCE SHEET-TYPE-04.

5. ALL VAULT INSTALLATIONS SHALL BE BACKFILLED IN LAYERS NOT MORE THAN EIGHT (8) INCHES IN LOOSE THICKNESS. THE BASE LAYER OF THE VAULT SHOULD CONSIST OF 3/4" GRAVEL, WITH LAYERS OF 1/4" OR LOWER 'CRUSH AND RUN' GRAVEL MATERIAL ABOVE. EACH LAYER SHALL BE THOROUGHLY COMPACTED BEFORE THE NEXT LAYER IS PLACED. 625.2 AND WISDOT 651.3.1.

3. DUE TO THE HEAVY CLAY MAKEUP OF THE REGIONAL SOIL, THE TOP LAYER OF THE EXCAVATION BACKFILL IS TO 'CRUSH AND RUN' GRAVEL MATERIAL 1/4" AND SMALLER, NOT TO BE FINISHED WITH OPEN GRADED ROCK LARGER THAN 1/4". TOPSOIL REPLACEMENT IS REQUIRED IN ALL AREAS WHERE TOPSOIL EXISTS. REFERENCE WISDOT 209.1 GRANULAR BACKFILL AND CMM - 325 STRUCTURE EXCAVATION. TOPSOIL REFERENCE SHEET-CON-02, ITEM 18.

FIBER OPTIC CABLE:

1. THE FIBER OPTIC CABLE INSTALLATION TECHNIQUES AND PROCEDURES SHALL BE AS SPECIFIED BY THE CABLE MANUFACTURER AND SHALL BE SUCH THAT THE OPTICAL AND MECHANICAL CHARACTERISTICS OF THE CABLES ARE NOT DEGRADED AT THE TIME OF INSTALLATION. THE CENTRAL STRENGTH MEMBER AND ARAMID YARN SHALL BE ATTACHED DIRECTLY TO THE PULLING EYE DURING CABLE PULLING. "BASKET GRIP" OR "PULLING SOCK" TYPE ATTACHMENTS TO THE CABLE OUTER TENSILE RATING SHALL BE USED ON ALL PULLS.

2. ALL FIBER OPTIC CABLE INSTALLATION PROCEDURES SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND INDUSTRY STANDARDS.

3. CONTRACTOR SHALL COORDINATE WITH THE PROJECT MANAGER PRIOR TO DISCONNECTING ANY EXISTING FIBERS AND ALL FIBER SPLICING AT CITY SITES.

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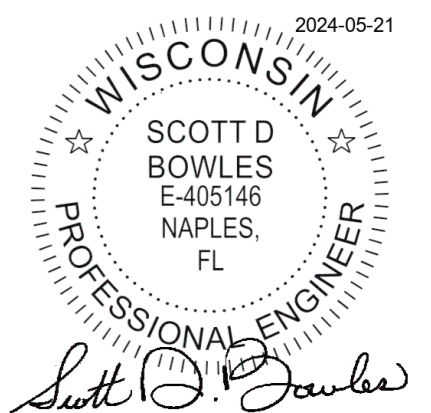
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4. ONLY IN CERTAIN CIRCUMSTANCES SHALL ENERGIZED CABLE BE PLACED IN THE SAME CONDUIT OR PULL BOX AS FIBER OPTIC CABLE, AND ONLY WITH WRITTEN APPROVAL AND SEPARATION GUIDELINES FROM CITY OF SUPERIOR.
5. FIBER COUNT IS SPECIFIED ON THE PLANS. SINGLE MODE ALL DIELECTRIC FIBER SHOULD BE USED. CABLE GROUNDING IS REQUIRED FOR ARMORED CABLE. TAILS FROM PATCH PANELS, CABINETS, AND POP-SITES SHALL BE ALL DIELECTRIC.
6. ONCE CABLE HAS BEEN INSTALLED INTO SPLICE CLOSURES FOLLOW MANUFACTURER'S RECOMMENDATIONS TO ENSURE PROPER SEAL OF CLOSURE.
7. SEPARATE ENTRY FOR DISTRIBUTION NETWORK. PROVIDE 60' SPARE FIBER FOR INLINE AND END OF LINE CLOSURE INSTALLS. IF CLOSURE SERVES A BRANCH/LATERAL, PROVIDE 120' SPARE FIBER FOR THE MAINLINE CABLE, 60' SPARE FIBER FOR THE LATERAL CABLE BEING SPLICED TO MAINLINE.
8. SAME STORAGE FIGURES FOR AERIAL FIBER CONSTRUCTION, FIBER TO BE STORED WITH SNOWSHOE STYLE UNITS.
9. ALL FIBER OPTIC CABLES SHALL BE TESTED VIA OTDR AFTER SPLICING IS COMPLETE. SPLICE RESULTS SHALL BE SUPPLIED TO THE PROJECT MANAGER FOR CITY OF SUPERIOR APPROVAL. TESTING STANDARDS ARE SPECIFIED IN THE SPECIFICATIONS FOR THIS PROJECT. REFERENCE SHEET-CON-09, CODES AND STANDARDS, LINE ITEM 1.
10. FUSION WELDING METHOD WILL BE UTILIZED ON ALL PERMANENT SPLICES, SINGLE AND MASS FUSION(RIBBON CABLE). THE AVERAGE OPTIC LOSS TO BE DETERMINED VIA OTDR TESTING. SEE SHEET-CON-09, CODES AND STANDARDS LINE ITEM 1.
11. CONVENTIONAL CONSTRUCTION METHODS MUST BE FOLLOWED WHEN PULLING FIBER OPTIC CABLE IN UNDERGROUND DUCTS. EMPLOYEES AND CONTRACTORS MUST BE FAMILIAR WITH STANDARD TELEPHONE COMPANY PRACTICES FOR INSTALLATION OF THE UNDERGROUND CABLE.

12. TESTING, VENTILATING, PUMPING AND SETTING UP VAULTS AND DUCT FOR PULLING OPERATIONS ARE THE SAME AS FOR COPPER CABLE.
13. A PULLING SOCK / BASKET GRIP IS TO BE ATTACHED TO THE OUTSIDE OF THE CABLE END FOR CABLE PULLING. IN ADDITION, THE CABLE STRENGTH MEMBERS CAN BE ATTACHED TO THE PULLING EYE IF DEEMED NECESSARY.
14. MAXIMUM PULLING TENSIONS FOR CABLE PULLED IN CONDUIT SHALL NOT EXCEED 600 LBS OR THE AMOUNT SPECIFIED BY THE CABLE MANUFACTURER IF LESS THAN 600 LBS.
15. A TENSION METER OR TENSION LIMITER WILL BE USED TO MONITOR PULLING TENSIONS.
16. USE A SWIVEL BETWEEN THE PULLING LINE AND PULLING FEATURE TO RELIEVE ROPE TWIST DURING THE PLACING OPERATION.
17. THE CABLE SHALL NOT BE SUBJECTED TO A DYNAMIC BENDING RADIUS OF LESS THAN THE MANUFACTURER'S SPECIFICATION OF THE CABLE DURING PULL. FINAL STATIC BENDING RADIUS TO BE NO LESS THAN THE MANUFACTURER'S SPECIFICATION OF THE CABLE.
18. THE CABLE SHOULD BE SECURED WITH TIE WRAPS TO PREVENT INTERFERING WITH FUTURE CABLE INSTALLATIONS.

PASSIVE CABINETS:

1. PASSIVE CABINETS SHALL BE MOUNTED TO CONCRETE PADS WITH AN ADJACENT VAULT PLACED FOR SPLICING. FOLLOW MANUFACTURER'S DIRECTIONS FOR INSTALLING AND MOUNTING VAULTS.
 2. VAULTS FOR PASSIVE CABINETS SHOULD FOLLOW THE SAME TYPICAL FOR ALL OTHER VAULTS.
 3. PASSIVE CABINETS TO BE LABELED ACCORDING TO CITY SPECIFICATIONS.
- REFERENCE SHEET-TYP-15 FOR FDH CABINET TYPICAL DRAWING

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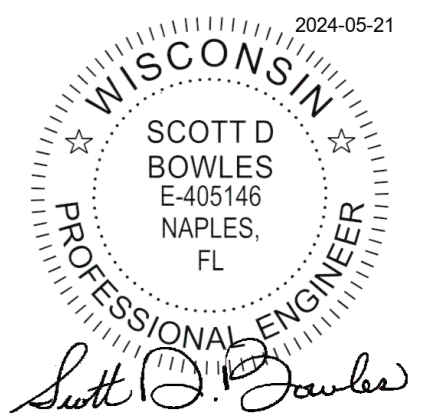
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AERIAL CONSTRUCTION:

1. ALL POLE ATTACHMENTS SHALL FOLLOW REGIONAL COMMUNICATION POLE ATTACHMENT STANDARDS.
2. AERIAL INSTALLATION OF FIBER CABLE IS THE SAME AS COPPER CABLE; USING THE SAME TOOLS, EQUIPMENT, MATERIAL AND PROCEDURES.
3. FIBER CABLE SHALL BE LASHED ON A SEPARATE STRAND MESSENGER OR EXISTING MESSENGER WITH COMMUNICATION CABLE ALREADY LASHED TO IT. MESSENGER WILL BE INSTALLED BY THE CONTRACTOR WHEN DIRECTED BY THE PROJECT PLANS.
4. THE INSTALLATION OF AERIAL CABLE MAY BE PERFORMED BY EITHER THE BACK PULL OR DRIVE OFF METHOD. THE METHOD TO BE USED MUST BE DISCUSSED AND APPROVED FOR EACH PHASE OF THE PROJECT.
 - a. THE BACKPULL METHOD REQUIRES PULLING THE ENTIRE CABLE INTO POSITION WHILE TEMPORARILY SECURING IT TO THE MESSENGER WITH CABLE BLOCKS. THE BLOCKS SHOULD BE LOCATED APPROXIMATELY EVERY 50 FEET ALONG THE ROUTE EXCEPT OVER ROADWAYS, DRIVEWAYS, RAILROADS, ETC., WHERE THEY SHOULD BE SPACED AS REQUIRED FOR PROPER CLEARANCE.
 - b. THE DRIVE OFF METHOD INVOLVES LASHING THE CABLE IN SECTIONS ALONG THE ROUTE. THE CABLE IS DRIVEN PAST THE FIRST SPAN AND LASHED. THIS METHOD IS REPEATED UNTIL THE END OF THE ROUTE IS REACHED.
5. DURING THE LASHING OPERATION, AN AERIAL CABLE GUIDE SHOULD BE USED TO PROTECT THE CABLE FROM HARMFUL BEND RADIUS.
6. FIBER OPTIC MARKERS SHALL BE PLACED ON THE CABLE AT EACH POLE. FIBER OPTIC MARKERS WILL BE SUPPLIED BY THE CITY.
7. REFERENCE AERIAL CONSTRUCTION TYPICALS ON SHEET-TYP-08 THROUGH SHEET-TYP-14.

CODES AND STANDARDS:

1. ACCEPTANCE TESTING SHALL BE DONE BY THE CONTRACTOR USING OTDR TESTING, BOTH SINGLE AND BI-DIRECTIONAL TESTING DEPENDING ON APPLICATION. DB LOSSES AND TOLERANCES TO BE DETERMINED BY OWNER, CITY OF SUPERIOR. TESTING RESULTS TO BE REVIEWED BY CONSTRUCTION MANAGEMENT AND OVERSIGHT.

SINGLEMODE: 1310nm AND 1550nm
 MAX. IND. FIBER LOSS AT 1310 NM 0.35 DB/KM
 MAX. IND. FIBER LOSS AT 1550 NM 0.25 DB/KM

SUMMARIZE THE RESULTS OF BOTH THE OTDR AND OPTICAL SOURCE/POWER METER TESTS IN SPREADSHEET/TABULAR FORMAT ADHERING TO THE FOLLOWING REQUIREMENTS:

- LIST FIBER OPTIC CABLE NAME, ROUTE, START POINT, END POINT
- LIST ALL FIBERS BY NUMBER AND DIRECTION (NB,SB,EB,WB)
- LIST TOTAL FIBER OPTIC CABLE LENGTH FOR EACH FIBER AS DOCUMENTED BY OTDR
- LIST ATTENUATION IN dB OF GAIN/LOSS FOR EACH FIBER OPTIC EVENT IN THE OTDR
- LIST FIBER OPTIC LOSS EVENT DESC. & LOCATIONS INCLUDING SPLICES, MISC EVENTS, & TERMINATIONS
- LIST THE ATTENUATION ACROSS THE CABLE IN dB/KM FOR EACH FIBER TESTED.
- LIST THE TOTAL SEGMENT LOSSFOR EACH FIBER AS DETERMINED BY THE OPTICAL SOURCE/POWER METER TEST
- PROVIDE BI-DIRECTION DATA INCLUDING EVENT DISTANCES, EVENT DESCRIPTIONS, AND ATTENUATION LOSSES FOR EACH FIBER CORRESPONDING TO A COMMON START POINT.
- PROVIDE BI-DIRECTIONAL DATA ON SEPARATE LINES, SIDE-BY-SIDE WITHIN THE SAME SHEET
- PROVIDE 1310 NM AND 1550 NM TEST RESULTS ON SEPARATE SHEETS IN IDENTICAL FORMATS.

REFERENCE WISDOT 678.3.4 COMMUNICATION SYSTEM TESTING FOR MORE INFORMATION

2. THE CITY RESERVES THE RIGHT TO HAVE THE CONTRACTOR REPAIR OR REPLACE ANY DEFECTIVE ITEMS OR DAMAGE INCURRED TO EXISTING FACILITIES AT THE CONTRACTOR'S EXPENSE

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ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-CON-10

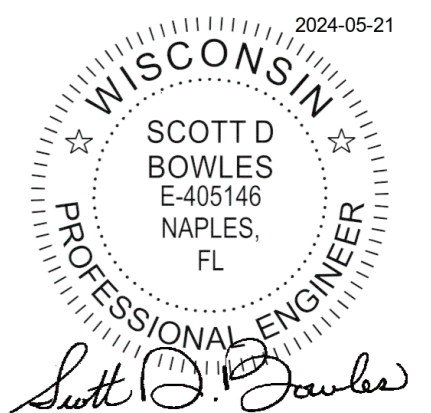
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04/30/2024	VERSION 1	



JOB CONDITIONS AND PARKING:

1. NO STOCKPILING OF MATERIALS OR PARKING OF EQUIPMENT DURING NON-WORKING HOURS WILL BE ALLOWED ON CITY PROPERTY, I.E. STREETS, ALLEYS, PARKING LOTS, BIKE PATHS OR SIDEWALKS. WISDOT 106 CONTROL OF MATERIALS.
2. THE CONTRACTOR SHALL CONDUCT THEIR OPERATIONS AND SCHEDULE CLEANUP SO AS TO CAUSE THE LEAST POSSIBLE OBSTRUCTION AND INCONVENIENCE TO TRAFFIC, PEDESTRIAN, CYCLISTS AND ADJACENT PROPERTY OWNERS. WISDOT 106 CONTROL OF MATERIALS.
3. THE CONTRACTOR SHALL LIMIT THEIR ACTIVITIES WITHIN THE AREAS TO THE ESTABLISHED SCHEDULE. NO WORK SHALL PROCEED ON THE SITE UNTIL TEMPORARY BARRIERS AND PROTECTION ARE FURNISHED AND INSTALLED. THE COST OF ALL LABOR, MATERIALS, TOOLS AND EQUIPMENT USED PROVIDING A SAFE PROJECT SHALL BE INCLUDED IN THE BID ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
4. NO TRASH, EXCESS MATERIAL, RUBBISH, ETC., ARE TO BE PLACED IN THE MANHOLES, VAULTS, OR SPLICE BOXES. ALL EXCESS MATERIAL IS TO BE REMOVED. THE CONTRACTOR IS RESPONSIBLE FOR THE DISPOSAL OF ALL EXCESS CONSTRUCTION MATERIAL. WISDOT 106 CONTROL OF MATERIALS.
5. CITY WILL NOT MAKE ARRANGEMENTS FOR CONTRACTOR TO PARK VEHICLES AND MATERIALS.
6. WHEREVER PARKING IS RESTRICTED, THE CONTRACTOR WILL BE REQUIRED TO OBTAIN, AT THEIR EXPENSE, PARKING PERMITS FROM THE CITY'S POLICE DEPARTMENT.
7. CONTRACTOR IS NOT TO ACCESS ELECTRIC VAULTS, THESE SHOULD ONLY BE HANDLED BY POWER UTILITY EMPLOYEES.

3. RULES FOR CONSTRUCTION OF UNDERGROUND ELECTRIC SUPPLY AND COMMUNICATION SYSTEMS, AND THE PUBLIC SERVICE COMMISSION OF WISCONSIN.

4. ALL AERIAL CONSTRUCTION AND CABLE PLACEMENT ON OVERHEAD POLE LINES SHALL ABIDE BY THE NATIONAL ELECTRICAL SAFETY CODE, OR NESC (LATEST REVISION)

5. CITY OF SUPERIOR NOISE GUIDANCE TO BE FOLLOWED MONDAY - FRIDAY, 6AM - 8PM. NO WORK SHALL BE COMPLETED ON HOLIDAYS UNLESS PRIOR WRITTEN APPROVAL IS GRANTED. REFERENCE SHEET-CON-01, GENERAL NOTES LINE ITEM 09.

6. CITY OF SUPERIOR TRUCK ROUTE MAP SHALL BE FOLLOWED. REFERENCE PAGE 69 FOR TRUCK ROUTE MAP. CITY OF SUPERIOR CODE OF ORDINANCE, CHAPTER 112 SECTION 33 -HEAVY TRAFFIC (TRUCK) ROUTE MUST ALSO BE FOLLOWED. REFERENCE SHEET-COS-TR-01.

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SHEET: 13 OF 69

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Tree Planting, Preservation, and Protection

A Description

The City of Superior acknowledges the importance of trees to the community's health, safety, welfare, and tranquility. Trees increase property values, provide visual continuity, provide shade and cooling, decrease wind velocities, control erosion, conserve energy, reduce stormwater runoff, filter airborne pollutants, reduce noise, provide privacy, provide habitat and food value, and release oxygen. No trees will be removed within the project limits without the approval of the engineer.

Definition of Terms

A. Tree Protection Zone:

A specified area above and below ground and at a given distance from the trunk set aside for the protection of a tree's roots and crown to provide for the viability and stability of a tree to be retained where it is potentially subject to damage by development. Tree Protection Zone shall be determined to be the diameter of the tree trunk measured in inches at chest height multiplied by 1.5 and expressed as feet. That distance is measured from the trunk of the tree creating a radius around the tree, as shown in the details within the Tree Protection Zone Details.

B. Materials

Trees and other vegetation are to be protected and preserved during construction. Trees and other vegetation will be properly protected during construction to maximize their survival rate. In order to achieve an appropriate balance between protecting trees and allowing necessary construction, the practices that follow will be employed.

All trees within and adjacent to the project limits shall be protected and maintained against damage during construction. All workers on the site shall be educated in tree preservation practices. Tree protection devices shall be placed before material deliveries, excavation, or grading begins and is to be maintained in good repair for the duration of the construction work, unless otherwise directed. Tree protection shall remain until the landscape restoration work begins.

C. Construction

Protection of trees during construction: Protection of existing trees will be accomplished with the establishment of the Tree Protection Zone (TPZ) for each tree and using tools such as fencing placed along the construction limits, carefully choosing the style of machinery the specifications allow to travel behind the existing curb and gutter, shoring, construction boxes, and protective ground sheeting. However, it will not be limited to just these tools. The Contractor can use other tools to protect the trees and landscaping at their discretion and as approved by the Engineer, such as hiring a commercial arborist to advise how to best protect the trees. If this arborist should contest a damage assessment performed by the Engineer, the City will consider the arborists argument, but the Engineer's ultimate decision will prevail.

Handheld operating tools, air-excavation or other non-mechanical methods are encouraged to be used for removal or construction activities within the Tree Protection Zone.

Prior to any site work, all trees to be preserved must be protected, and maintained, in accordance with the Tree Protection Zone Details.

Tree Protection

Trees in the area of disturbance and in the vehicle access route are to be protected by fencing in the following manner:

No material or construction equipment shall be stored within the tree protection zone.

No protective devices, signs, utility boxes or other objects shall be nailed to the trees on the site.

Tree protection fencing shall be erected and approved by the Engineer at least 24 hours before construction begins.

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-TREES-01

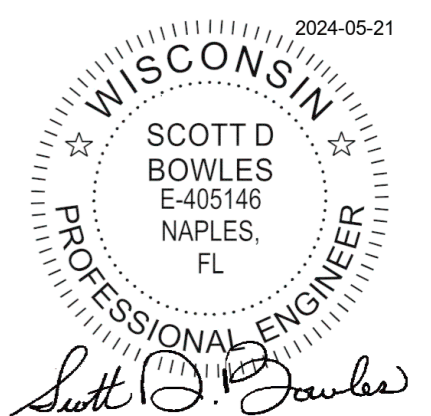
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Grade Changes

Grade cuts shall be minimized or eliminated within the tree protection zone.

Areas within the tree protection zone disturbed by construction activity shall be mulched with a 2-3" deep layer of shredded bark mulch as temporary protection of roots. Mulching shall be done within 4 hours of disturbance.

Trenching and Tunneling

Trenching shall be done outside the tree protection zone. Trenchless techniques shall be employed within the tree protection zone.

Pruning of branches shall be done under the requirements and direction of the engineer.

Bridging Roots

One option for working near trees with large root issues is to bridge over the existing roots. This is done by either supporting the raised section with concrete pillars near the roots or increasing the base layer over the root growth area. When increasing the base course material; course sand or pea gravel must be used along with a thin layer of foam board. Any bridged sections must still maintain ADA allowed slopes of 1:20. It is anticipated bridging roots will be required behind the sidewalks and on side streets.

Root Grinding

When dealing with a larger root (2" diameter or larger) it is better to grind the root rather than fully sever the root. This is done by either a stump/root grinder, chainsaw or debarking tool. Roots must maintain at least 1/3 of their original diameter when grinding roots. When possible, apply a layer or ridged foam or pipe insulation between the shaved root and the new concrete surface to allow for roots to callus. Any work on a root with a diameter greater than 2" must be approved by the Engineer prior to commencement. No root grinding or cutting is permitted within 4 feet from the face of the tree.

Clean Root Cutting

Root cutting shall be a LAST RESORT option when doing construction around trees. Where a trees root systems interfere with the construction of curb and gutter, drain tile, or other utilities; Contractor may address the roots in accordance with the following:

Cleanly cut tree roots as directed by the Engineer.

Immediately and cleanly cut damaged and exposed roots. Cut back damaged roots of trees designated for protection to sound healthy tissue and immediately place topsoil over the exposed roots. Immediately cover root ends exposed by excavation activities with 6 inches of topsoil as measured outward from the cut root ends. No cutting of roots larger than 2" will be allowed unless approved by the Engineer. No root grinding or cutting is permitted withing 4 feet from the face of the tree.

D. Measurement

The department will measure Tree Root Preservation and Protection by the tree regardless of tree protection zone size. Measurement will be considered complete when excavations in the Tree Protection Zone are completed.

E. Payment

The department will pay for the measured quantity at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
	Tree Root Preservation and Protection Type 1	Each

Payment is full compensation for providing all labor, equipment, and materials necessary to protect the trees and roots to the level as specified in the plans and specifications. Payment will include protective construction fencing for each tree. All protection shall be inspected by the Engineer prior to start of construction.

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-TREES-02

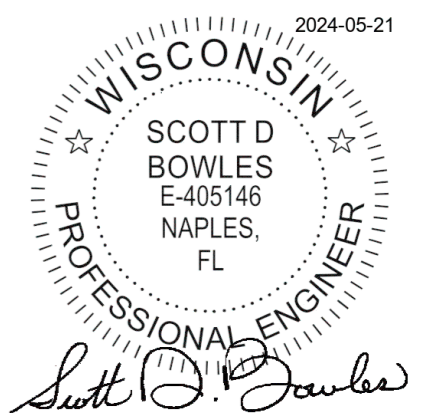
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F. Schedule of Damages

Damages to trees will be measured according to the following table.

SCHEDULE OF DAMAGES (PER TREE OR PRIVATE LANDSCAPING FEATURE)			
TYPE OF DAMAGE	LEVEL OF DAMAGE		
	LOW	MODERATE	SEVERE
Trees			
Above Ground			
<i>Canopy</i>			
<i>Branches</i>			
Less than 2 inches diameter	Each		
Greater than 2 inches and less than 4 inches diameter.		Each	
Greater than 4 inches diameter			Each
<i>Trunk Stem Circumference Damage</i>			
\$200/sf bark loss			
Below Ground			
<i>Root Zone</i>			
<i>Construction within TPZ</i>			
Material storage	Each		
Equipment storage	Each		
Soil Compaction	Each		
<i>Root Cutting or Grinding</i>			
Unapproved grinding or damage to 2/3 or more of a root within 4 feet from the face of the tree.			Each
Unapproved grinding or damage to 2/3 or more of a root with a diameter of 2" or greater.			Each

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
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SHEET-TREES-03

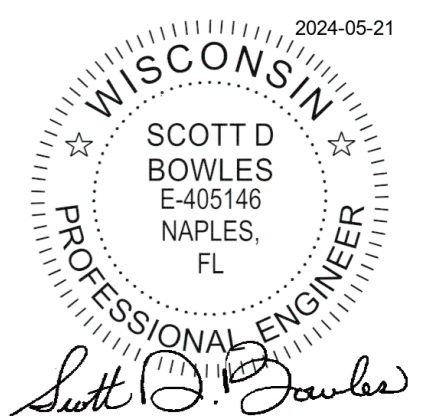
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G. Schedule of Deductions

Deductions from the amount due to the Contractor for the tree and private landscaping protection item will be calculated in accordance with the following table.

Level of Damage	Damage Fee ¹
Trees	
Low of all Types	\$200
Moderate of all Types	\$325
Severe of all Types	Greater of 100 times the diameter of the tree in inches expressed in dollars or \$1000

Should the Contractor accrue damages; the engineer will maintain a running account of those damage fees throughout the project. Damage fees will be assessed against the contractor in the last application for payment. The Engineers running damage account will be available to the Contractor for review upon request.

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-TREES-04

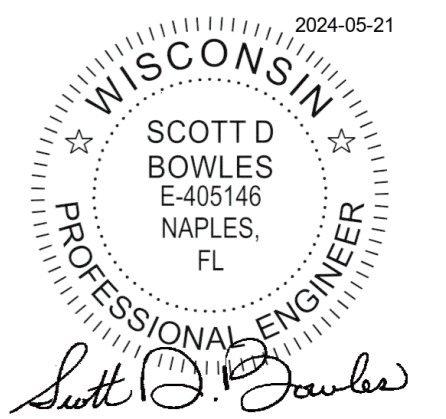
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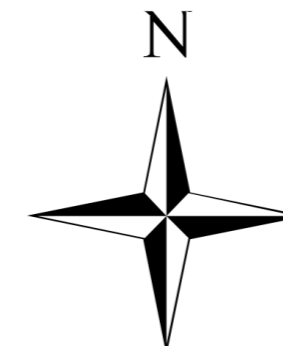
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TYPICALS

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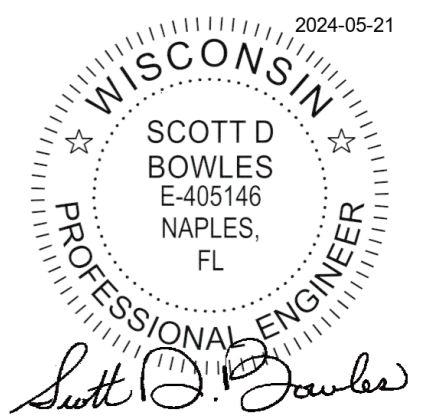
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
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SHEET-TYP-01

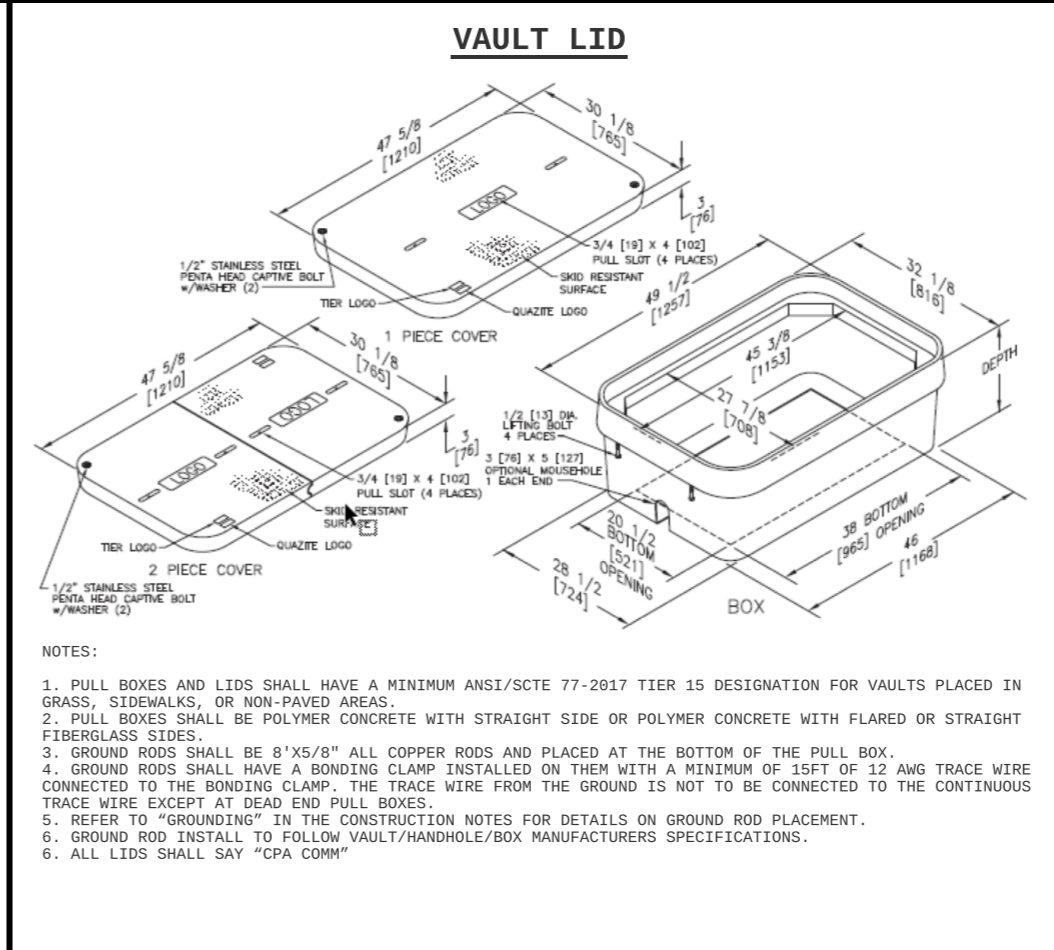
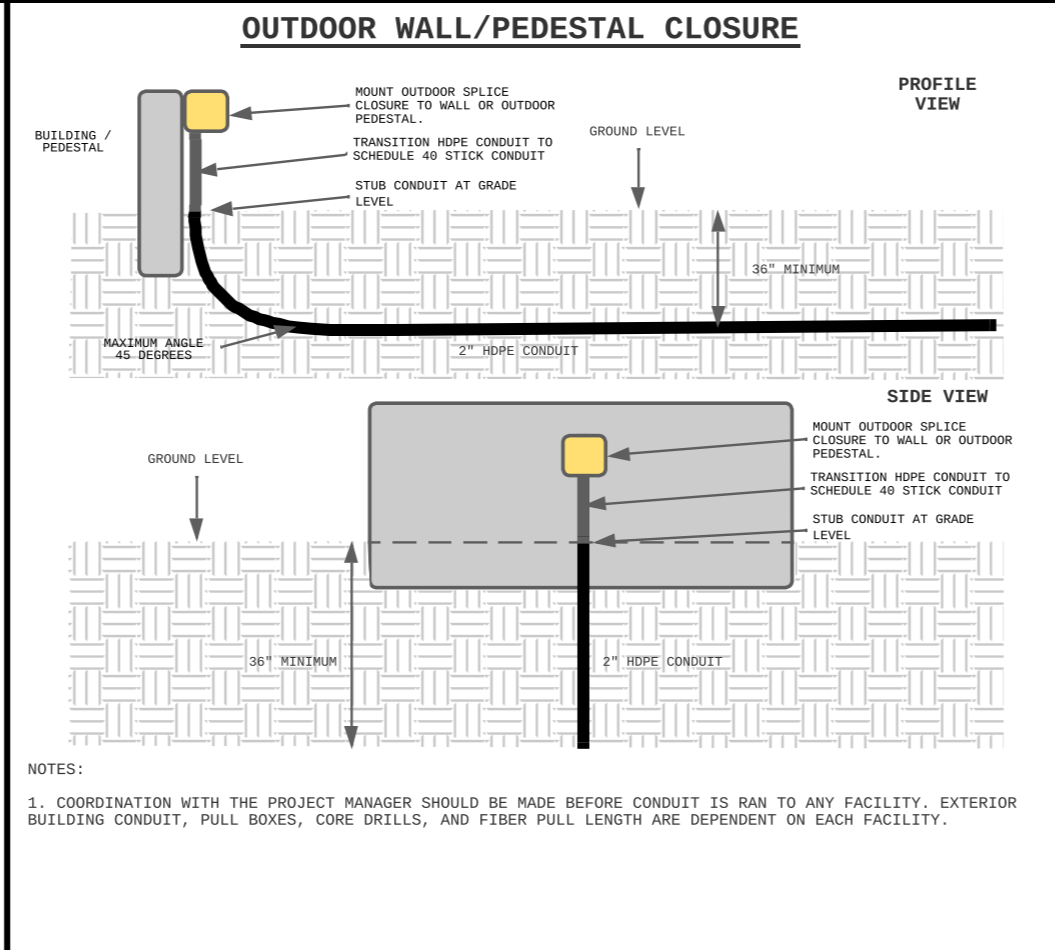
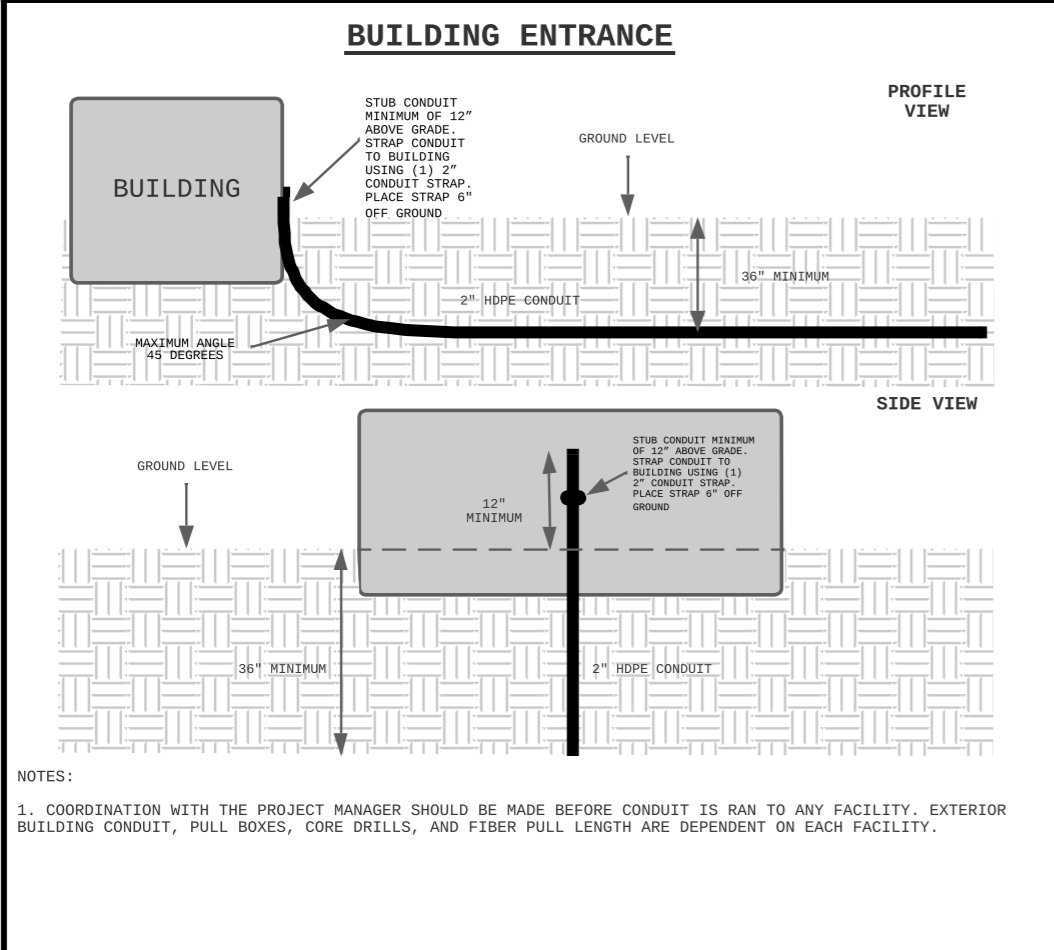
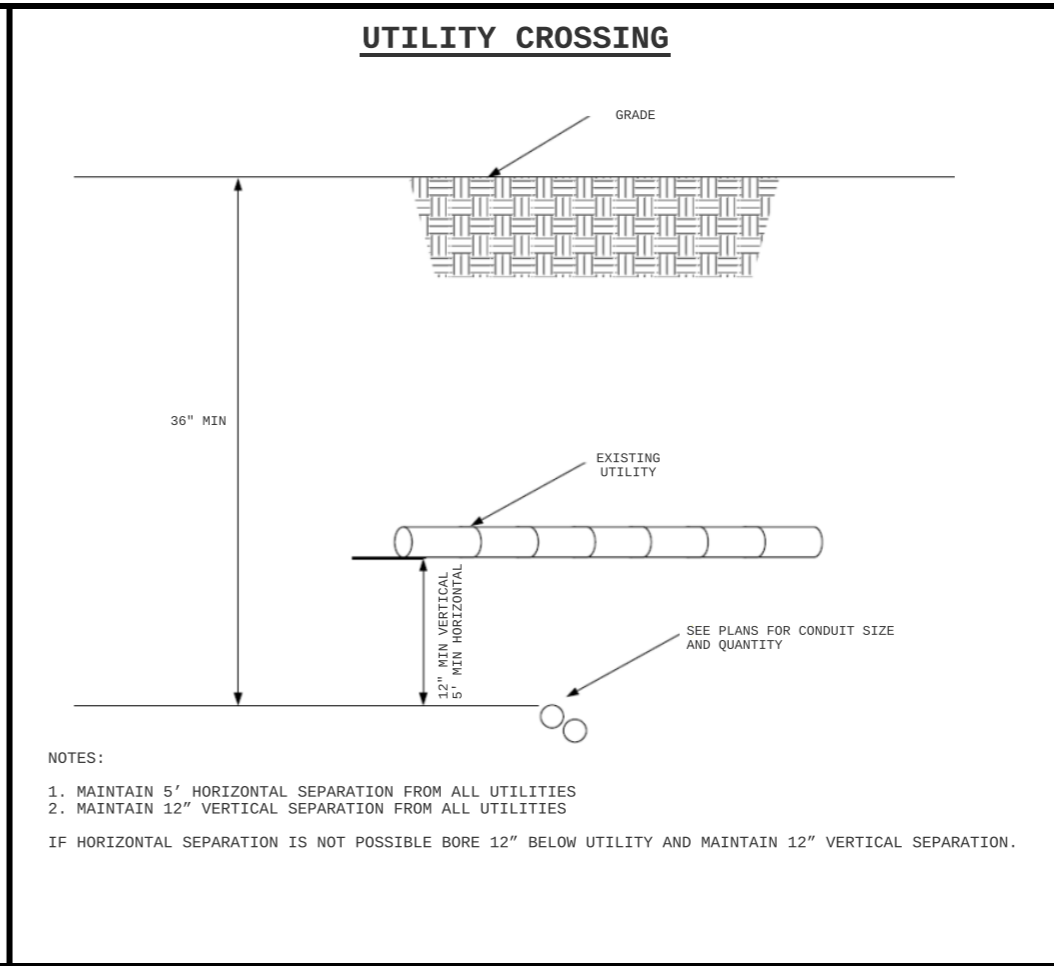
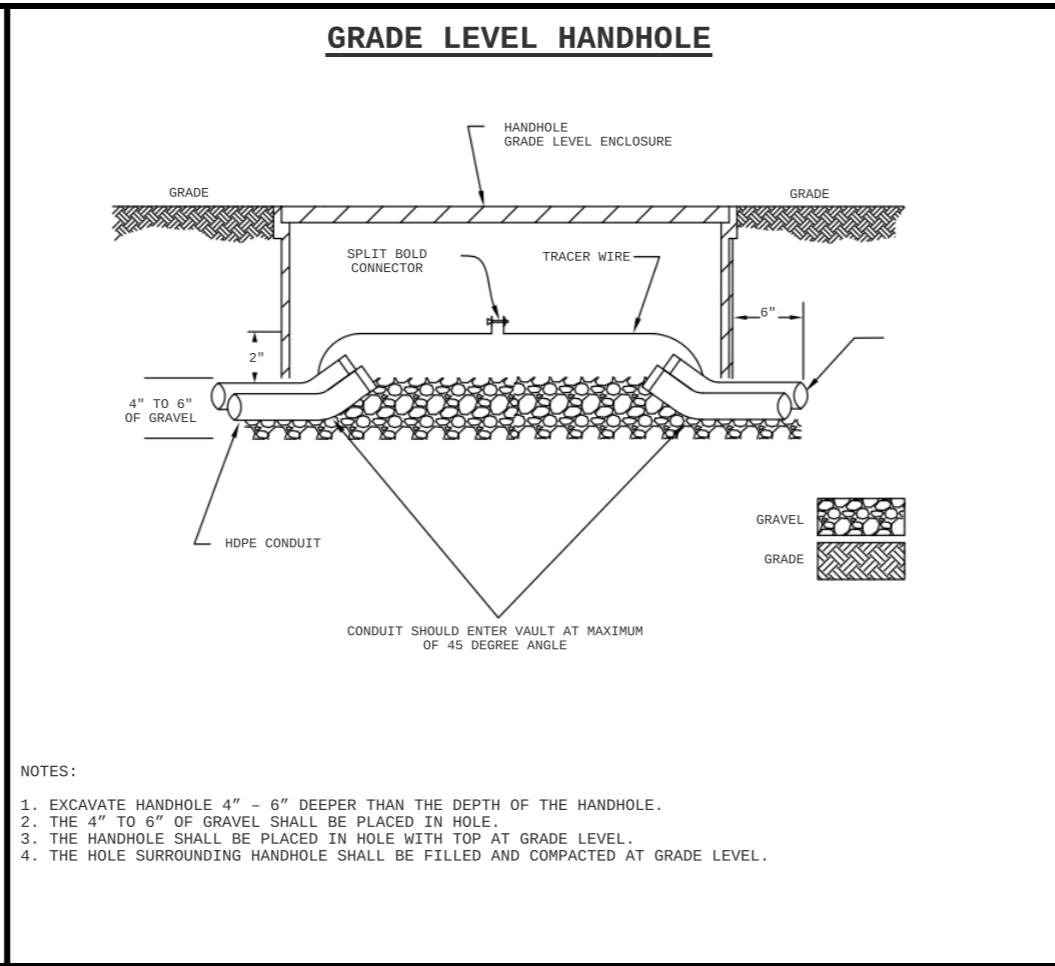
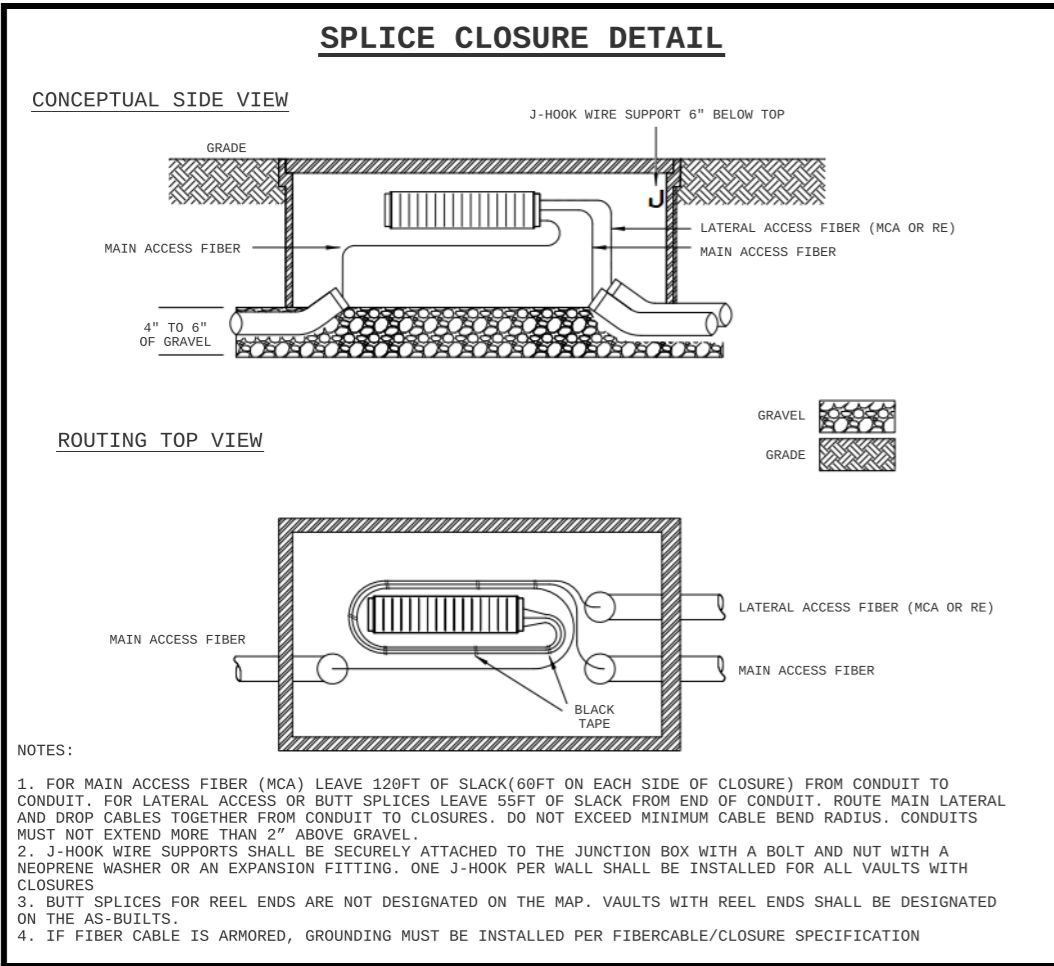
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REFERENCE SHEET-CON-06 & 07 FOR MORE INFORMATION

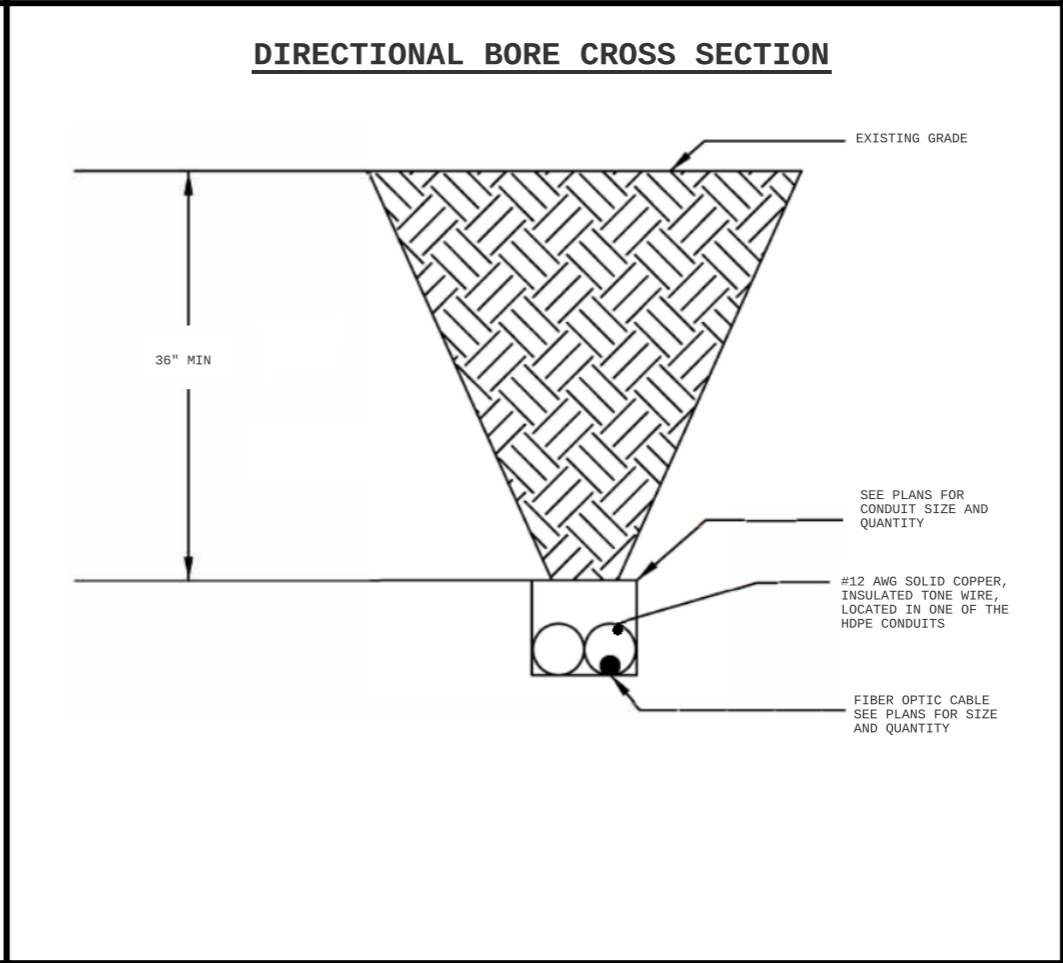
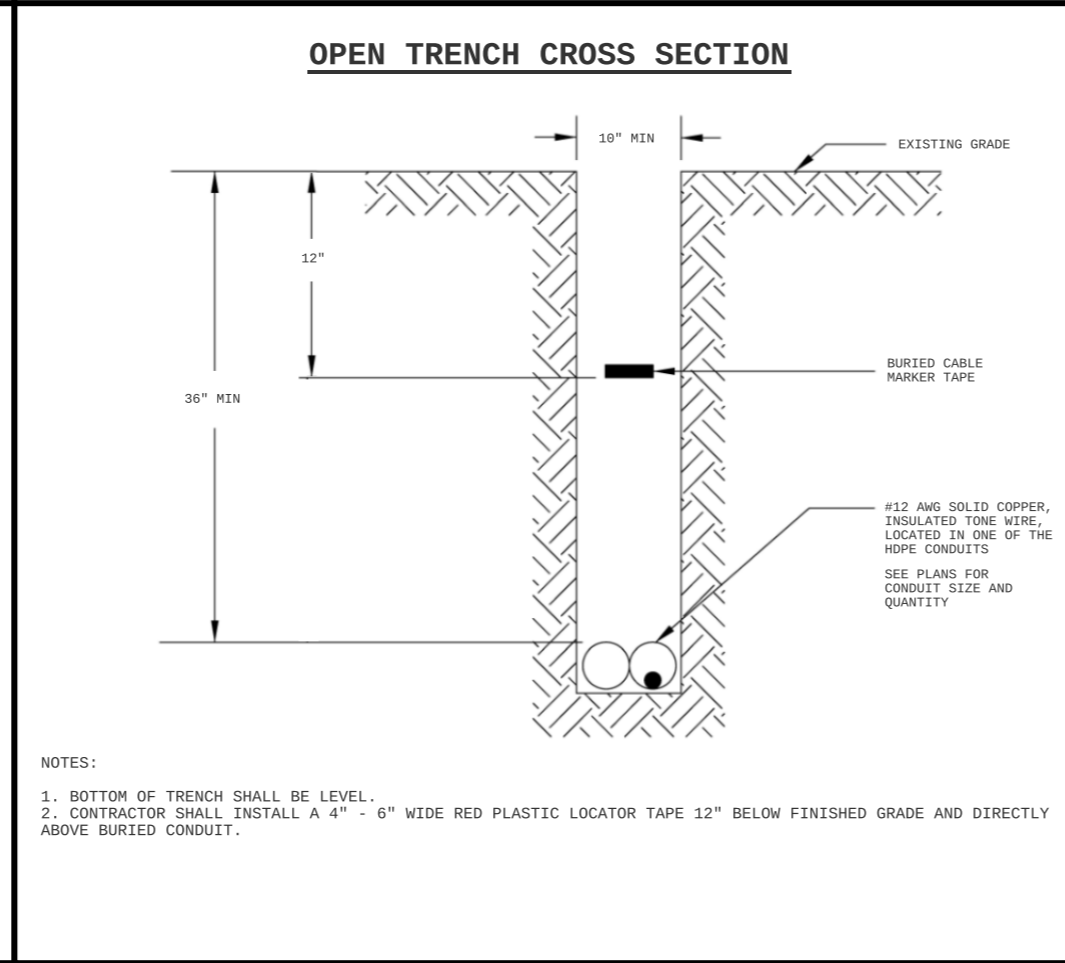
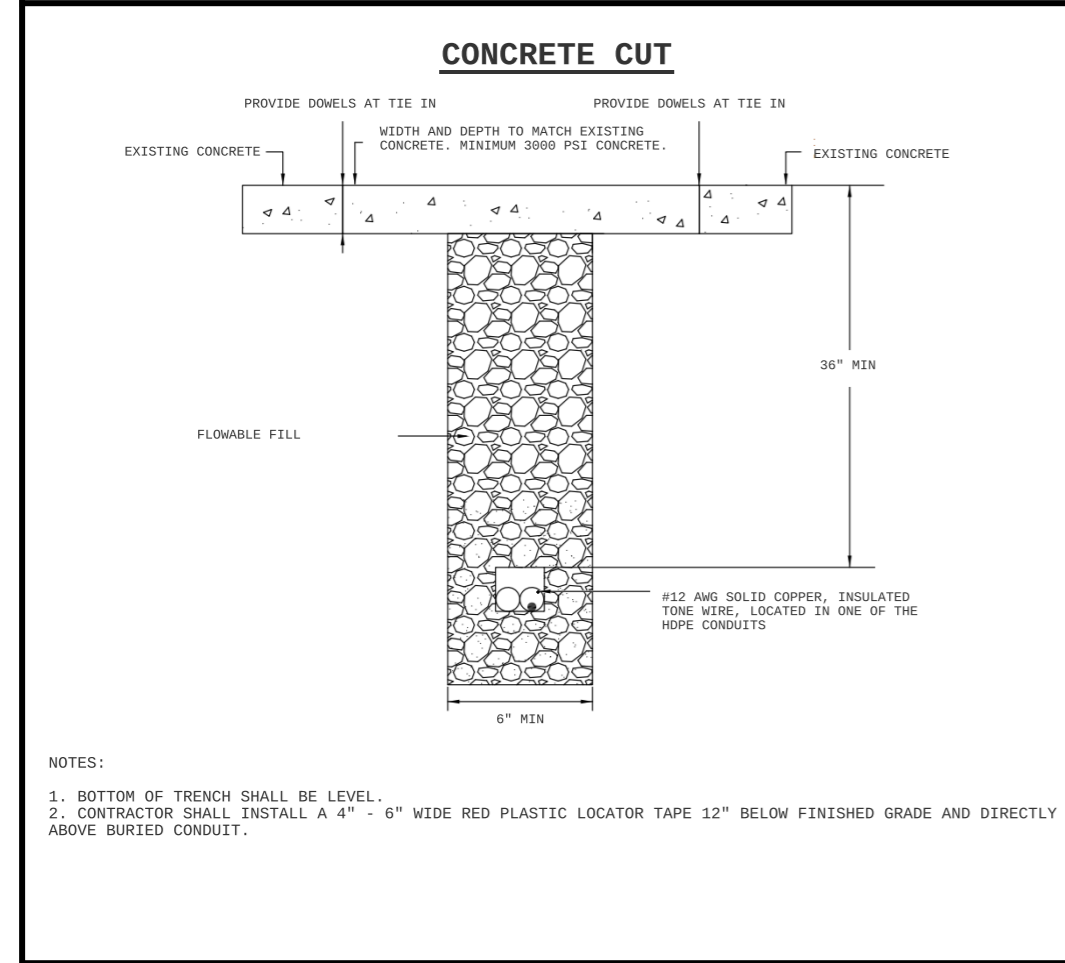
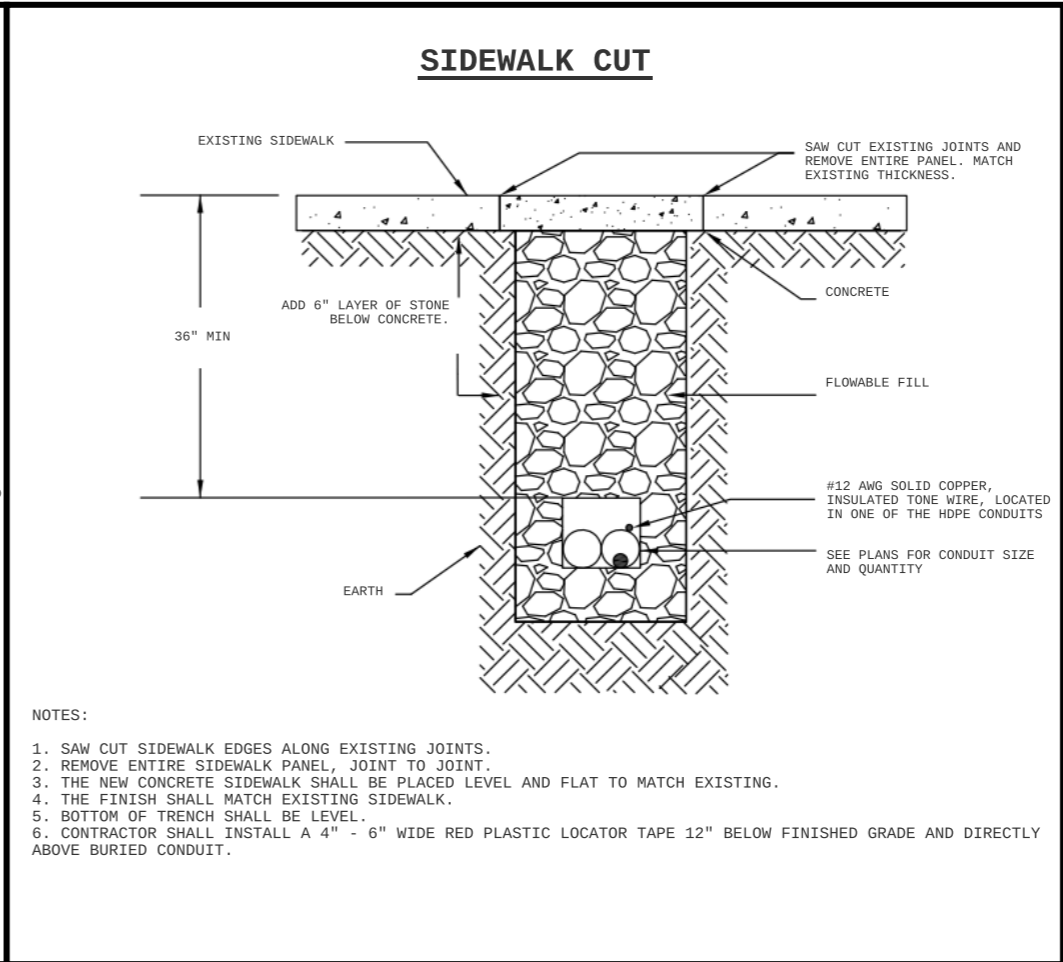
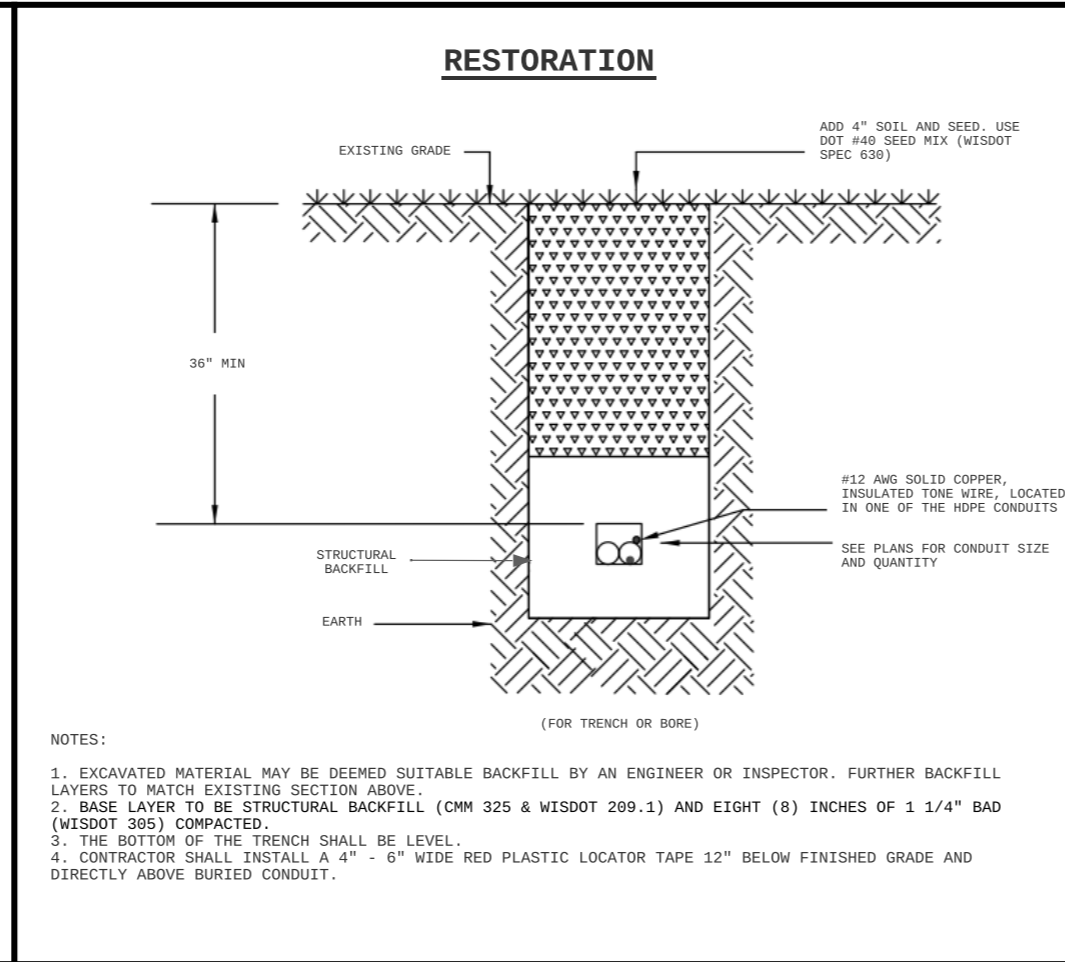
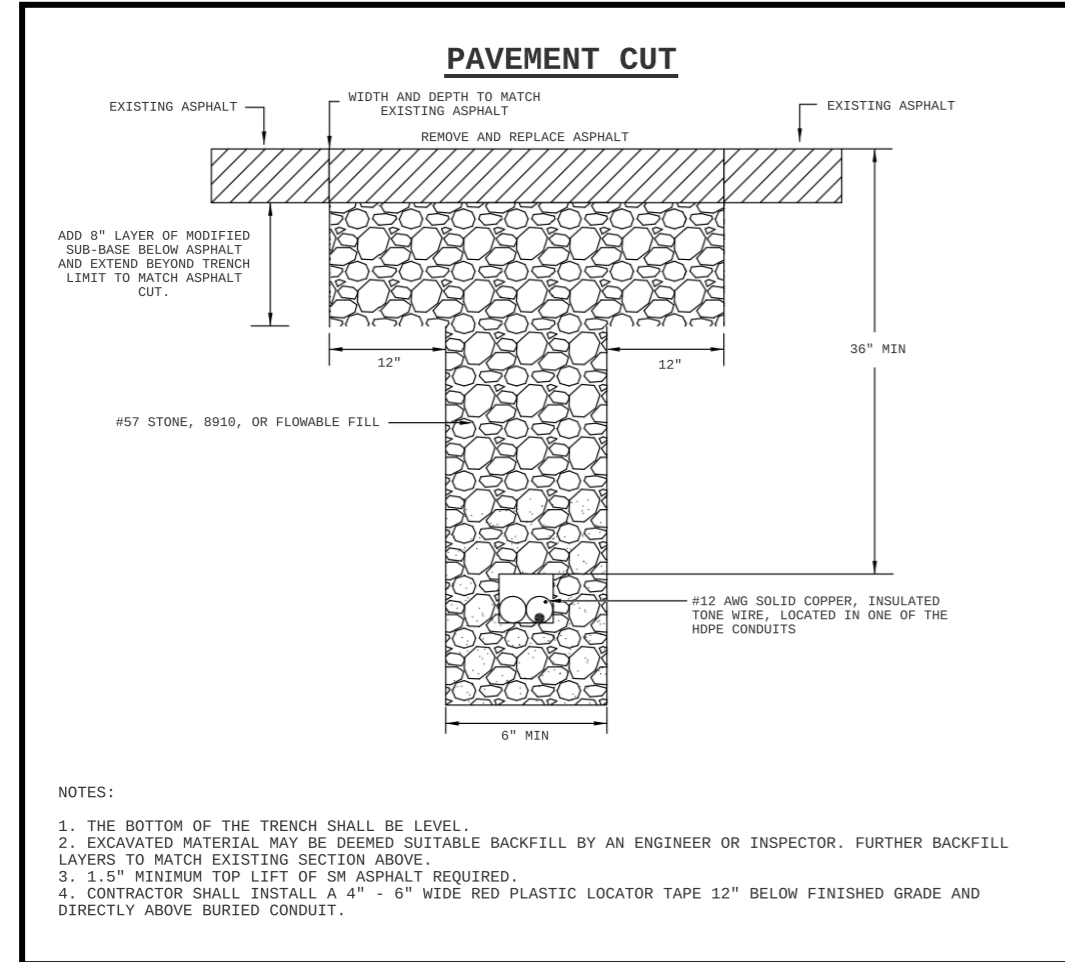
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TYPICALS



REFERENCE SHEET-CON-03 AND SHEET-CON-07 FOR MORE INFORMATION

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-TYP-02

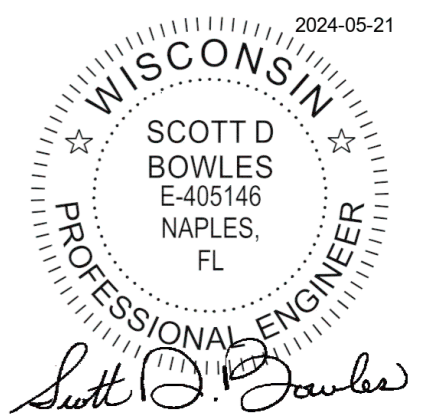
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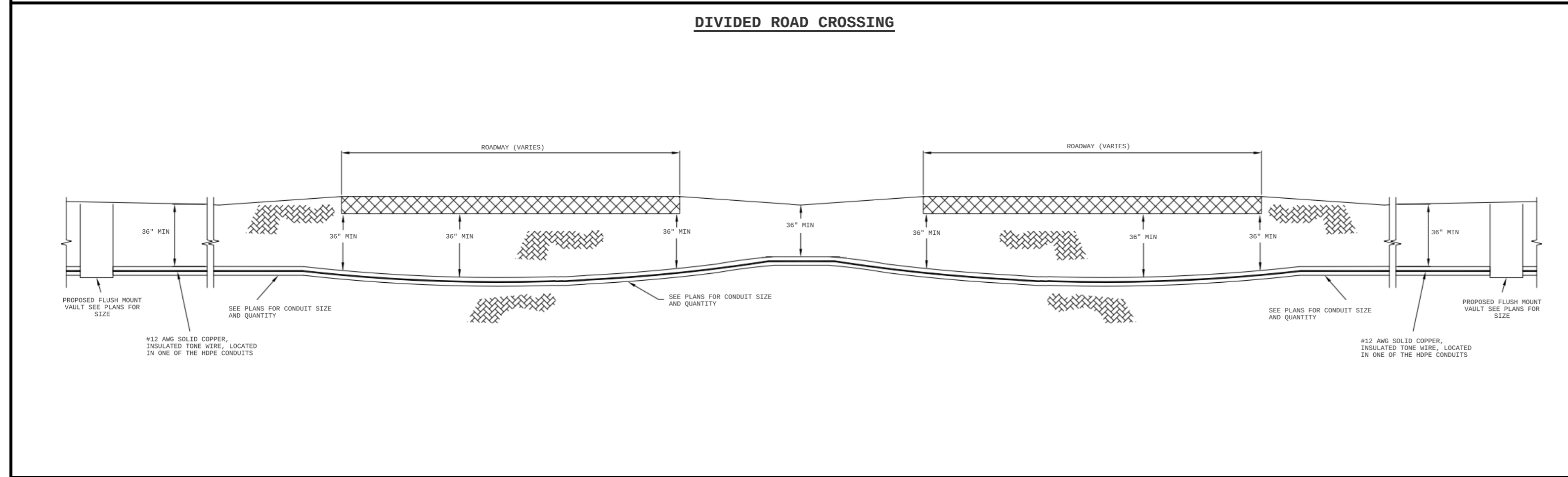
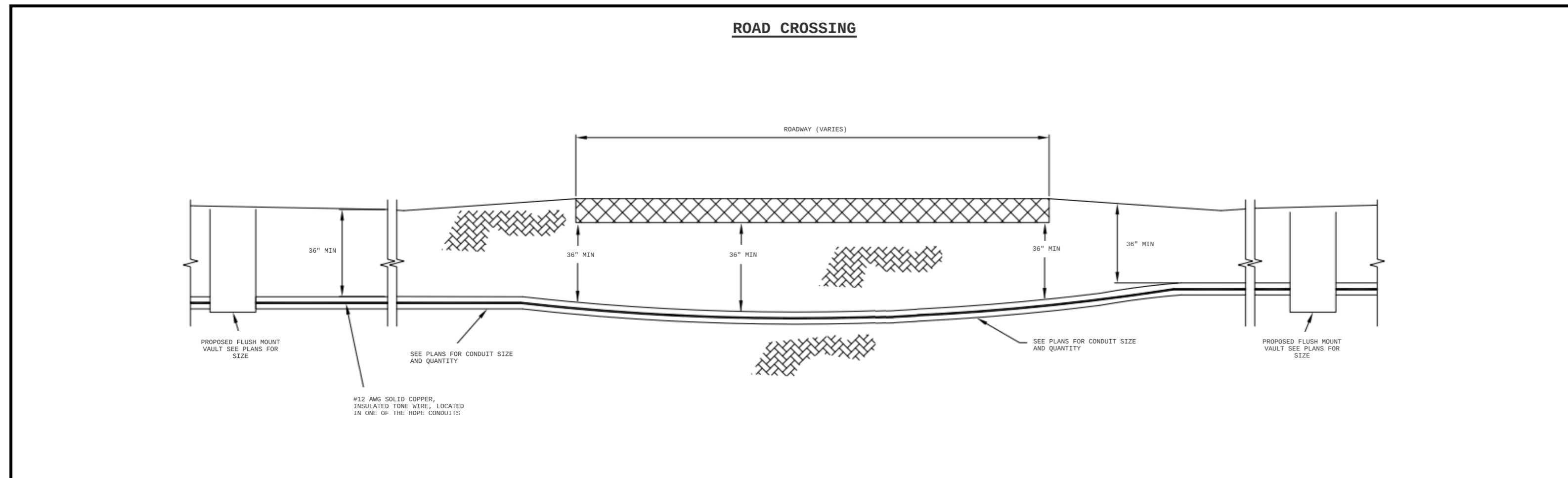
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REFERENCE SHEET-CON-03 FOR MORE INFORMATION

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SHEET-TYP-03

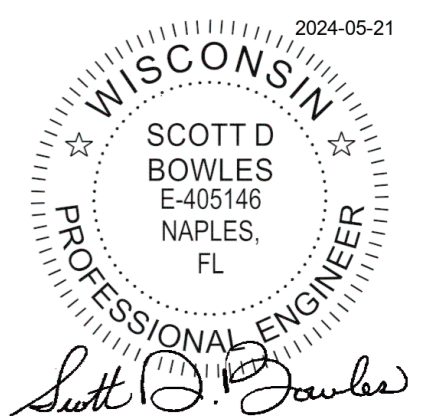
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TYPICALS

FIBER OPTIC UTILITY MARKER

NOTES:

1. 8' - 2#/FT. GREEN STEEL U-CHANNEL POST OR CITY OF SUPERIOR APPROVED EQUAL.
2. 72" H-41-RF TUBULAR ROUTER MARKER OR CITY OF SUPERIOR EQUAL.
3. ROUTE MAKER WRAP DECAL, BLACK TEXT ON ORANGE BACKGROUND.
4. RAISED MARKERS INDICATING FIBER OPTIC CABLE BURIED BELOW SHALL BE INSTALLED WHERE INDICATED ON THE PLANS.
5. ALL RAISED MARKERS INSTALLED SHALL BE MARKED ON THE AS-BUILT DRAWINGS.
6. ALL FIBER OPTIC MARKERS SHALL HAVE "THE CITY OF SUPERIOR" MARKED ON THEM.
7. CABLE MARKERS SHALL ONLY BE PLACED ON RURAL FIBER INSTALLS UNLESS OTHERWISE SPECIFIED. USE WISDOT SPEC 671.3.3 FIBER OPTIC CABLE MARKER.
8. MARKERS SHALL BE PLACED NO MORE THAN 2,000' APART.
9. CONTRACTOR SHALL INDICATE WHERE MARKERS PLACED ON ALL REDLINE DRAWINGS

CREEK / DITCH CROSSING

NOTES:

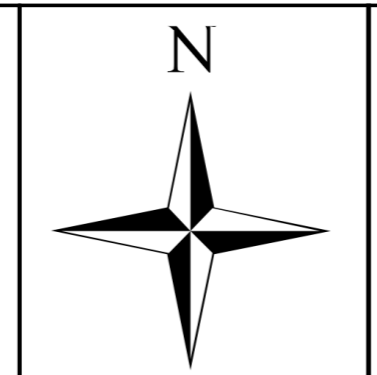
1. REFER TO THE CONSTRUCTION NOTES FOR MORE DETAILS ON DITCH AND CREEK CROSSINGS.
2. IF CROSSING CULVERT VERIFY THERE IS 36" OF COVER. IF THERE IS 36" OF COVER CONDUIT MAY BE INSTALLED OVER CULVERT.

REFERENCE SHEET-CON-04, SHEET-CON-07 FOR MORE INFORMATION

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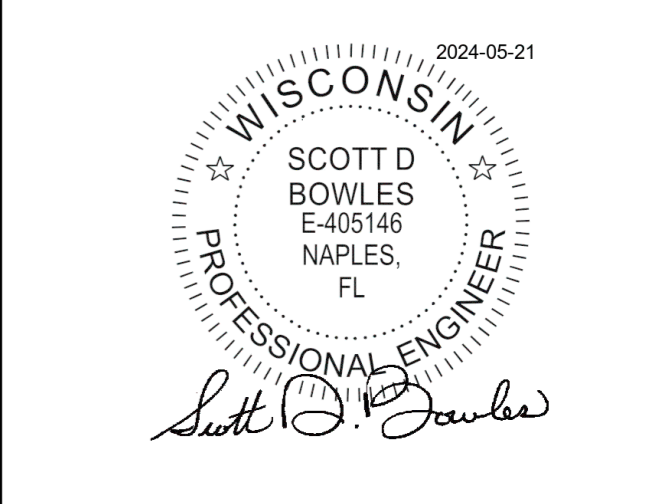
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FIBER OPTIC NETWORK
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SHEET-TYP-04

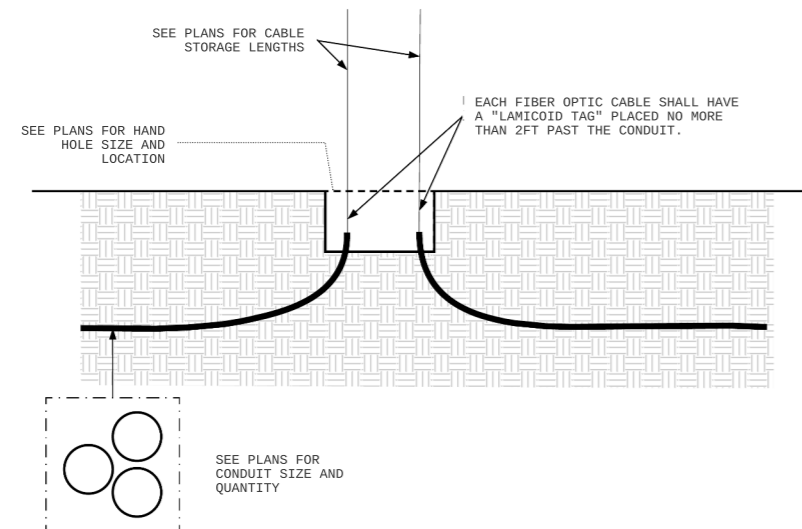
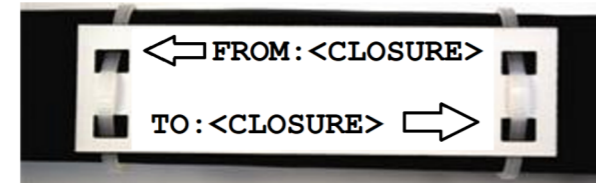
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TYPICALS



CLOSURE LABELS 1

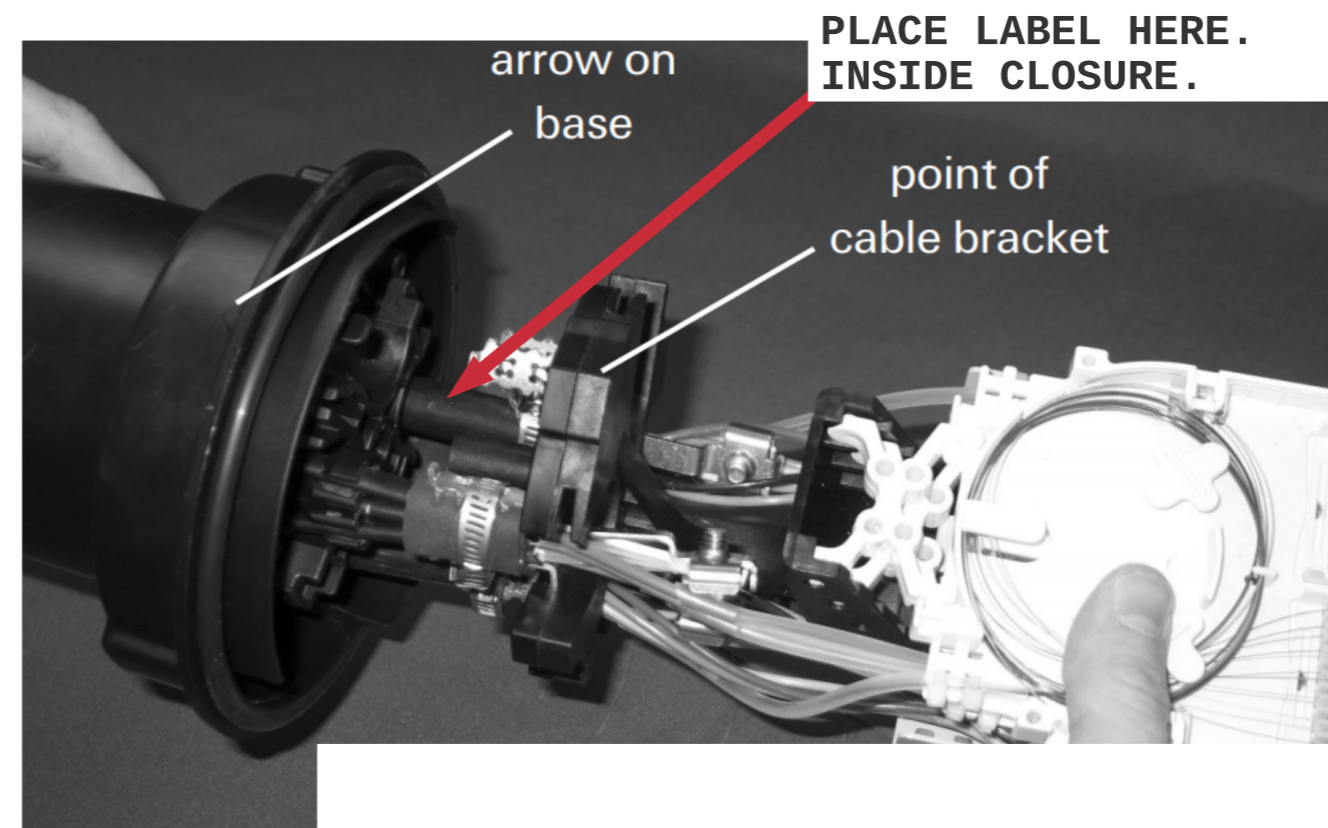
NOTES:

1. TAGS SHALL BE INSTALLED WHEN CABLE IS INSTALLED. THIS IS TO AID SPLICERS IN IDENTIFYING CABLES AND FUTURE MAINTENANCE ACTIVITIES.
2. TAGS ARE ONLY TO BE INSTALLED IN HANDHOLDS WITH CLOSURES. PULL THROUGH HANDHOLDS DO NOT REQUIRE TAGS.
3. TAGS SHALL BE FASTENED TO CABLES WITH A MINIMUM OF TWO ZIP TIES.
4. EXAMPLE TAG LABEL - "SUW102b-L006c, 48CT"
5. METAL EMBOSSED TAGS WITH METAL STRAPS TO BE UTILIZED, EXAMPLE; PANDUIT MEHT187 / 30PR57

CLOSURE LABELS 2

NOTES:

1. EACH CABLE INSIDE CLOSURE SHALL BE LABELED WITH A STICK ON LABEL. THE LABEL SHALL BE COMPLETELY CONTAINED WITHIN THE CLOSURE. THE LABEL SHALL INDICATE WHICH CLOSURE THE CABLE IS GOING TO.
2. THE PICTURE SHOWN DISPLAYS A COMMSCOPE FOSC STYLE CLOSURE WITH THE LID REMOVED.



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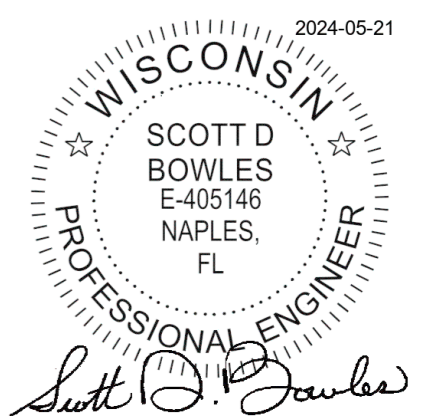
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EXAMPLE SPLICE CLOSURE EXTERIOR LABELS AND FIBERCABLE LABELS

EXAMPLE SPLICE CLOSURE INTERIOR LABELS, FIBER ASSIGNMENT TABLE FOR SPLICE TRAY



CITY OF SUPERIOR FIBER NETWORK NAMING SCHEME

[CITY][HUT][LEG] - [FDH][LEG] - [CLOSURE]

SUW102b-L005a-N04

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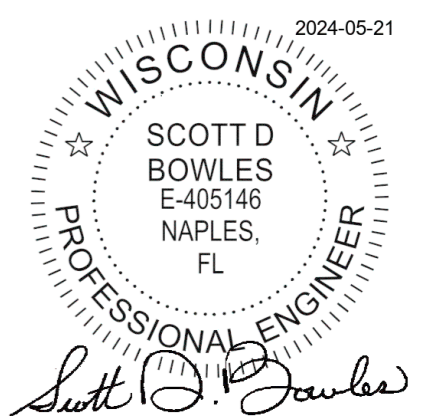
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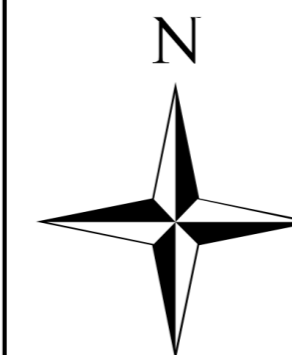
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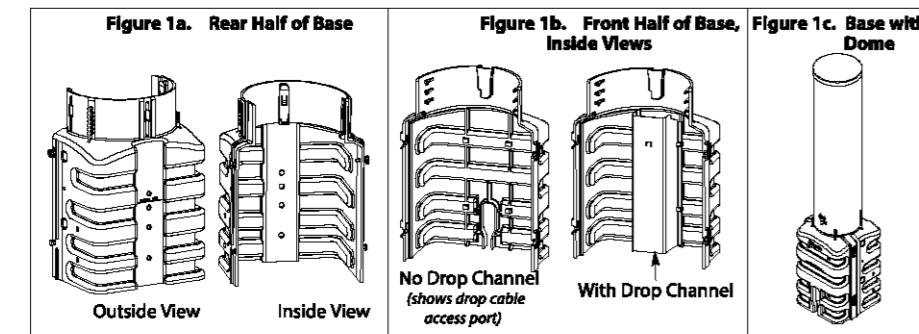
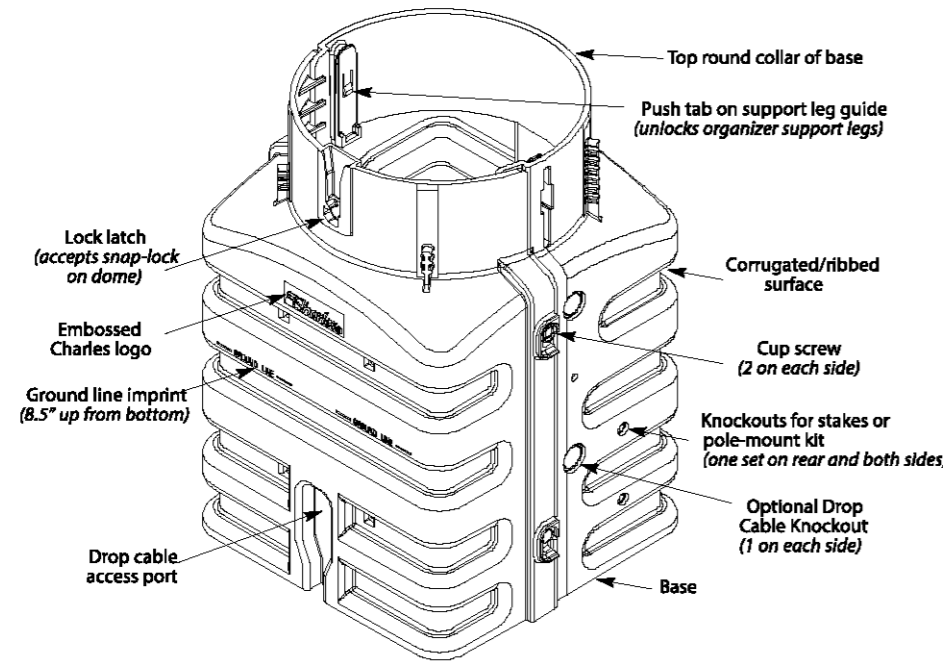
TYPICALS

PEDESTAL

THESE PEDESTAL PLANS REFER TO THE CHARLES INDUSTRIES BDO SERIES OF PEDESTALS. CONTRACTORS SHALL USE THE LATEST INSTALLATION PRACTICES FROM CHARLES INDUSTRIES. FURTHER INFORMATION MAY BE FOUND ONLINE AT THIS ADDRESS [HTTPS://WWW.CHARLESINDUSTRIES.COM/SUPPORT/RESOURCES/PRACTICES-INSTALLATION-GUIDELINES/](https://www.charlesindustries.com/support/resources/practices-installation-guidelines/)

IF ANOTHER PEDESTAL MODEL OR ANOTHER MANUFACTURER IS USED THE CONTRACTOR SHALL FOLLOW THE INSTALLATION INSTRUCTION PROVIDED BY THE MANUFACTURER. CHARLES INDUSTRIES BDO PEDESTAL BASE INSTALLATION INSTRUCTIONS PRODUCT PURPOSE

THE CHARLES PEDLOCK® PEDESTAL IS AN ABOVE-GRADE DEVICE THAT PROVIDES ENVIRONMENTAL PROTECTION FOR BURIED FEED AND DISTRIBUTION CABLES, AS WELL AS CUSTOMER SERVICE DROPS IN FIBER-TO-THE-HOME (FTTH) AND FIBER- TO-THE-PREMISES (FTTP) DEPLOYMENTS. PRODUCT MOUNTING AND LOCATION



1. THE PEDESTAL BASE IS INSTALLED IN A TRENCH OR HOLE IN THE GROUND, UP TO THE GROUND LINE (GL) INDICATOR, AT THE FTTP OR FTTH DISTRIBUTION POINT. WHEN THE BASE IS INSTALLED, THE PEDESTAL IS EASILY SECURED WITH THE OVERLAPPING OUTERDOME. THE OUTERDOME IS SECURED TO THE BASE WITH A SELF-LOCKING LATCH.
2. PREPARE TRENCH. BE CAREFUL NOT TO DAMAGE ANY BURIED CABLES OR WIRES WHILE DIGGING. DIG AND PREPARE THE CABLE TRENCH, PER LOCAL COMPANY PRACTICES.
3. PLACE CABLES, CONDUIT OR INNERDUCT IN TRENCH. PLACE OR LAY CABLE/CONDUIT IN THE TRENCH PER LOCAL PRACTICE. IN THE FINAL POSITION, CONDUIT HEIGHT SHOULD BE 1.5 INCHES BELOW THE BOTTOM OF THE BASE COLLAR, BUT NO HIGHER THAN THE BOTTOM OF THE COLLAR. NOTE: THE MOST ACCURATE CUT CAN BE MADE AFTER THE BASE HAS BEEN SET TO ITS PROPER DEPTH.
4. ASSEMBLE THE BASE. IF THE CABLE LOOP OR TAIL CAN BE FIT THROUGH THE BASE COLLAR, THE TWO HALVES CAN BE ASSEMBLED PRIOR TO POSITIONING THE BASE. LIFT THE FRONT HALF OVER AND ONTO THE TWO LOCKING BOLTS AND THE TWO TABS. LEVEL BOTH HALVES AND LOCK THEM IN PLACE BY TIGHTENING THE TWO BASE BOLTS.
5. DETERMINE BASE INSTALLATION LOCATION. POSITION THE BASE IN THE APPROXIMATE DESIRED POSITION IN THE TRENCH. THE FRONT OR DROP SIDE OF THE BASE (CHARLES LOGO) GENERALLY FACES THE STREET. USING EITHER THE BACK HALF OF THE BASE OR THE ENTIRE ASSEMBLY, POSITION THE FEED CABLES/CONDUIT TOWARDS THE REAR OF THE BASE. NOTE: THIS POSITIONING FACILITATES THE ATTACHMENT OF CABLE(S) DURING SPLICING PROCEDURES.
6. POSITION AND LEVEL BASE IN TRENCH AND BEGIN BACKFILL. POSITION THE BASE AND. NOTE: MAINTAIN THIS LEVEL AS BACKFILL IS BEING ADDED AND TAMPED. ONCE THE CABLES/CONDUIT HAS BEEN POSITIONED, THE BASE CAN BE PLACED IN THE TRENCH (WITH THE OPTIONAL ATTACHED STAKE). AS THE TRENCH IS BACKFILLED, PERIODICALLY TAMP THE SOIL, ALWAYS PUSHING THE SOIL TOWARDS THE BASE. THIS PRACTICE HELPS TO REMOVE AIR FROM THE BACKFILL SOIL, MAKING SETTling LESS LIKELY TO OCCUR. THE BASE IS DESIGNED TO MAINTAIN ITS ORIENTATION AFTER INSTALLATION; THEREFORE, IT IS IMPORTANT TO VERIFY THAT THE BASE IS LEVEL DURING THE ENTIRE INSTALLATION PROCEDURE. NOTE: SHOULD IT BE NECESSARY TO STRAIGHTEN A PEDESTAL AT ANY FUTURE TIME (SUCH AS IN THE EVENT OF UNEVEN GROUND SETTling), NEVER ATTEMPT TO STRAIGHTEN AN INSTALLED PEDESTAL BY MANIPULATING, PUSHING, OR PULLING ON THE ATTACHED DOME, AS PEDESTAL DAMAGE MAY RESULT. TO RE-PLUMB AND STRAIGHTEN A PEDESTAL POST-INSTALLATION, FIRST REMOVE THE SOIL FROM AROUND THE BASE, THEN RE-ADJUST THE BASE UNTIL A PROPER LEVEL IS ACHIEVED.
7. INSTALL BACKFILL SOIL, MOISTURE BARRIER AND PEA GRAVEL. ALTERNATELY BACKFILL THE BASE, INSIDE AND OUTSIDE, TAMPING THE SOIL AS IT IS ADDED. THE SOIL ON THE INSIDE OF THE BASE SHOULD BE EVEN WITH THE TOP OF THE SECOND RIB FROM THE BOTTOM. ON THE OUTSIDE, THE BACKFILL SHOULD BE EVEN WITH OR ABOVE THE GROUND LINE. ADDING BACKFILL ONE RIB HIGHER, AND CLOSE TO THE CHARLES' LOGO, WILL MAKE THE BASE MORE STABLE. CAUTION: NEVER MOUND BACKFILL SOIL ON THE OUTSIDE OF THE BASE TO MAKE IT APPEAR THAT THE BASE HAS BEEN INSTALLED TO THE RECOMMENDED DEPTH, AS THIS MOUND WILL WASH AWAY.
8. WHEN THE INTERNAL BACKFILL IS AT THE PROPER HEIGHT, INSTALL THE RED MOISTURE BARRIER. PLUG ALL OPEN CONDUITS PRIOR TO POURING IN ANY PEA GRAVEL. POUR 5 TO 6 INCHES OF PEA GRAVEL INTO THE BASE. THE GRAVEL SHOULD BE NO HIGHER THAN THE UPPERMOST RIB. NOTE: IF THE CONDUIT HAS BEEN TRIMMED TO THE HEIGHT DESCRIBED IN STEP 3, THE GRAVEL WILL BE 1 TO 1.5 INCHES BELOW THE TOP OF THE DUCT.
9. INSTALL FIBER ORGANIZER. A KEY ON EACH LEG ALLOWS IT TO BE ORIENTED IN ONLY ONE DIRECTION. ALIGN THE LEGS ONTO THE TABS ON THE SIDES OF THE BASE COLLAR. PUSH DOWN UNTIL TWO AUDIBLE CLICKS ARE HEARD.
10. INSTALL DOME(S). NOTE: THE DOME CAN ONLY BE FULLY INSTALLED WHEN THERE IS NO CABLE IN THE WAY. CFDP-EPS PEDESTALS HAVE AN INNER (BLACK) DOME THAT FITS OVER THE ORGANIZER AND SNAPS ONTO THE TOP SNAP CLIP. POSITION THE OUTER DOME AND ALIGN THE LOCK WITH THE BASE LATCH.

RAILROAD, WATERWAY, BRIDGE, OR OTHER CROSSING

NOTES:

1. THESE PLANS DO NOT INCLUDE PLANS OR TYPICALS FOR RAILROAD, BRIDGE, OR WATER WAY CROSSINGS. SEPARATE DRAWINGS FROM ENTRUST COMMUNICATIONS FOR EACH CROSSING WILL BE SUBMITTED WITH PERMITS FOR EACH CROSSING.
2. ANY CROSSING THAT REQUIRES ADDITIONAL PERMITS ARE NOTED ON THE PLANS. DO NOT CONSTRUCT THESE AREAS WITHOUT THE PROPER APPROVAL AND PERMITS.

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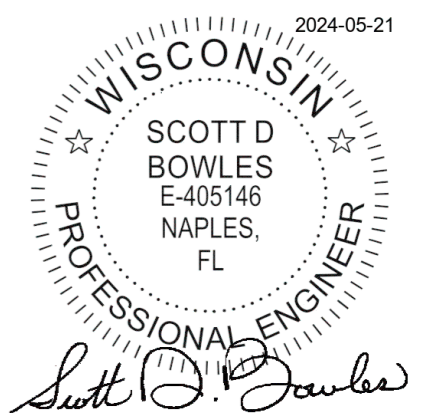
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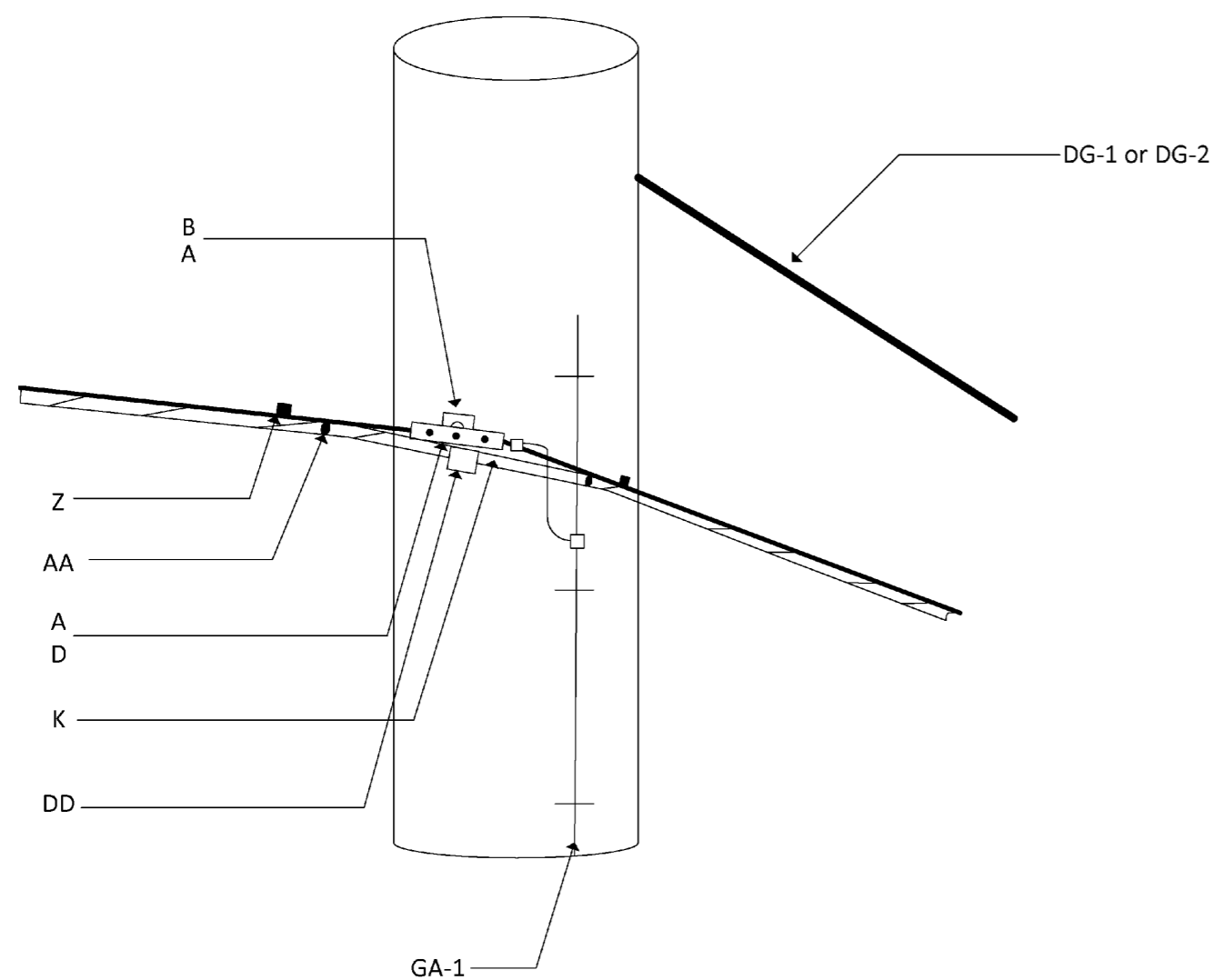


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Curve Suspension Strand (CV-1)

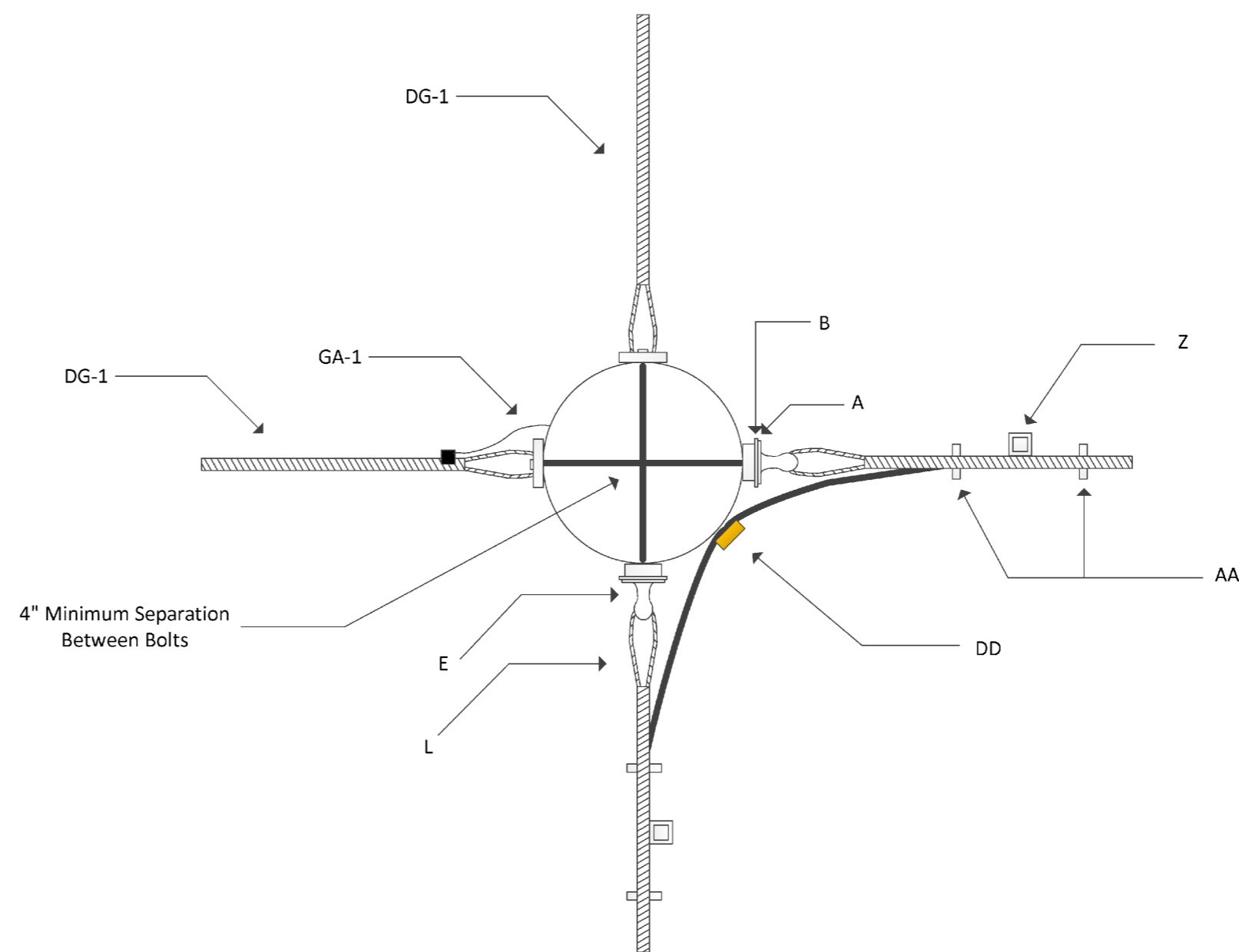


CV-1 (Fiber)	Curve 1	
Item	Qty	Description
K	1	3 Bolt Curve Suspension Clamp
D	1	Machine Bolt (Various Length)
B	2	Square Washers
A	2	Square Nuts
Z	2	Lashing Wire Clamps
AA	2	Strap and Spacers
DD	1	Fiber Tag
DG-1	1	Down Guy Assembly
GA-1	1	Ground Assembly

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

USE CITY SECONDARY STANDARDS FOR SECONDARY POLE ATTACHMENTS

Double Dead End (DD-1)



DD-1	Double Dead End	
Item	Qty	Description
E	2	Eye Bolt
B	4	Square Washers
A	2	Square Nuts
DG-1	2	DG Assembly
AA	4	Strap and Spacers
DD	1	Fiber Tags
Z	2	Lashing Wire Clamps
L	2	Dead End Grips
GA-1	1	Ground Assembly

ENTRUST COMMS

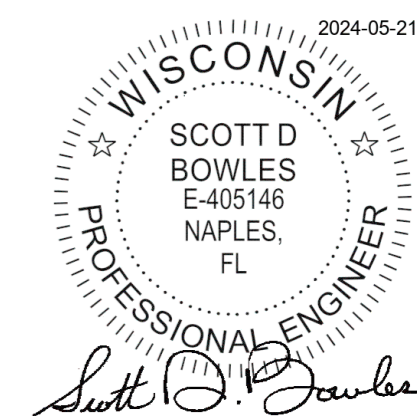
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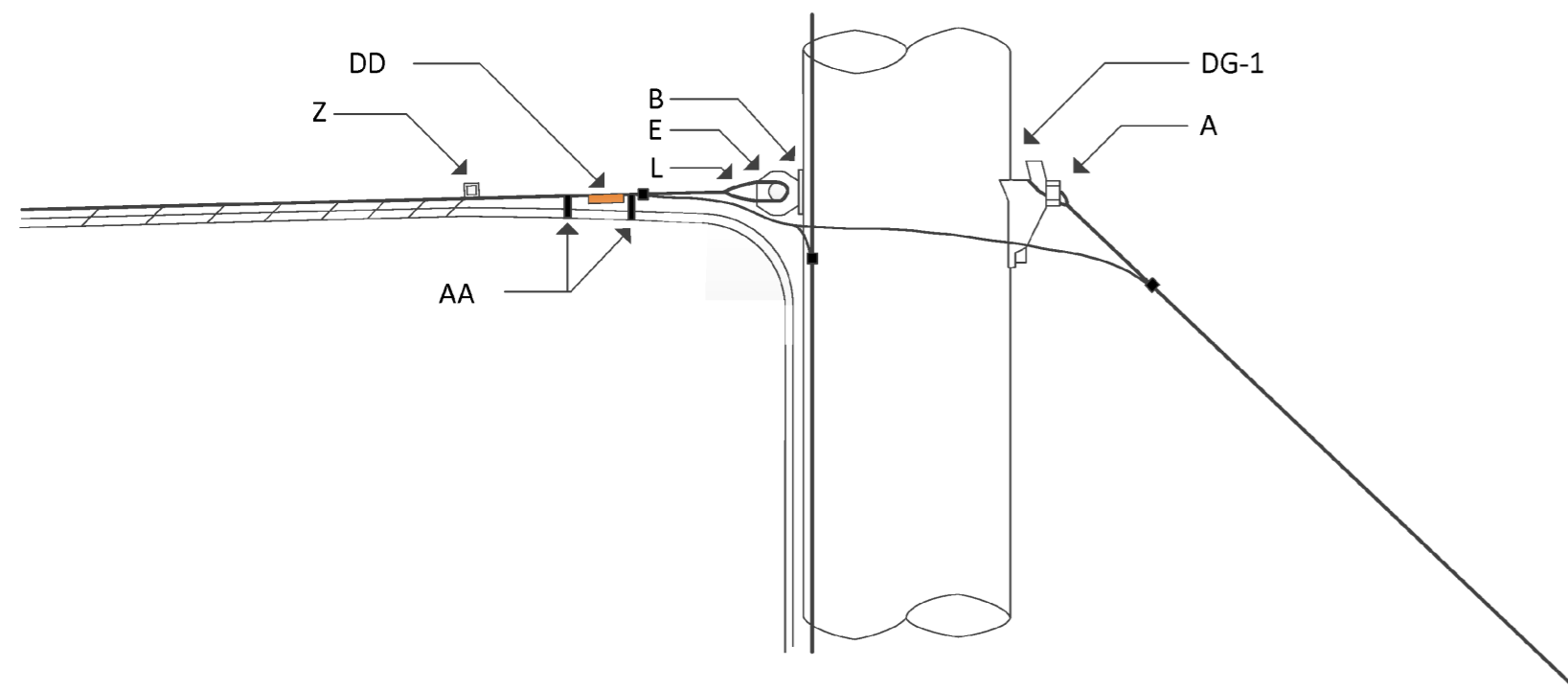


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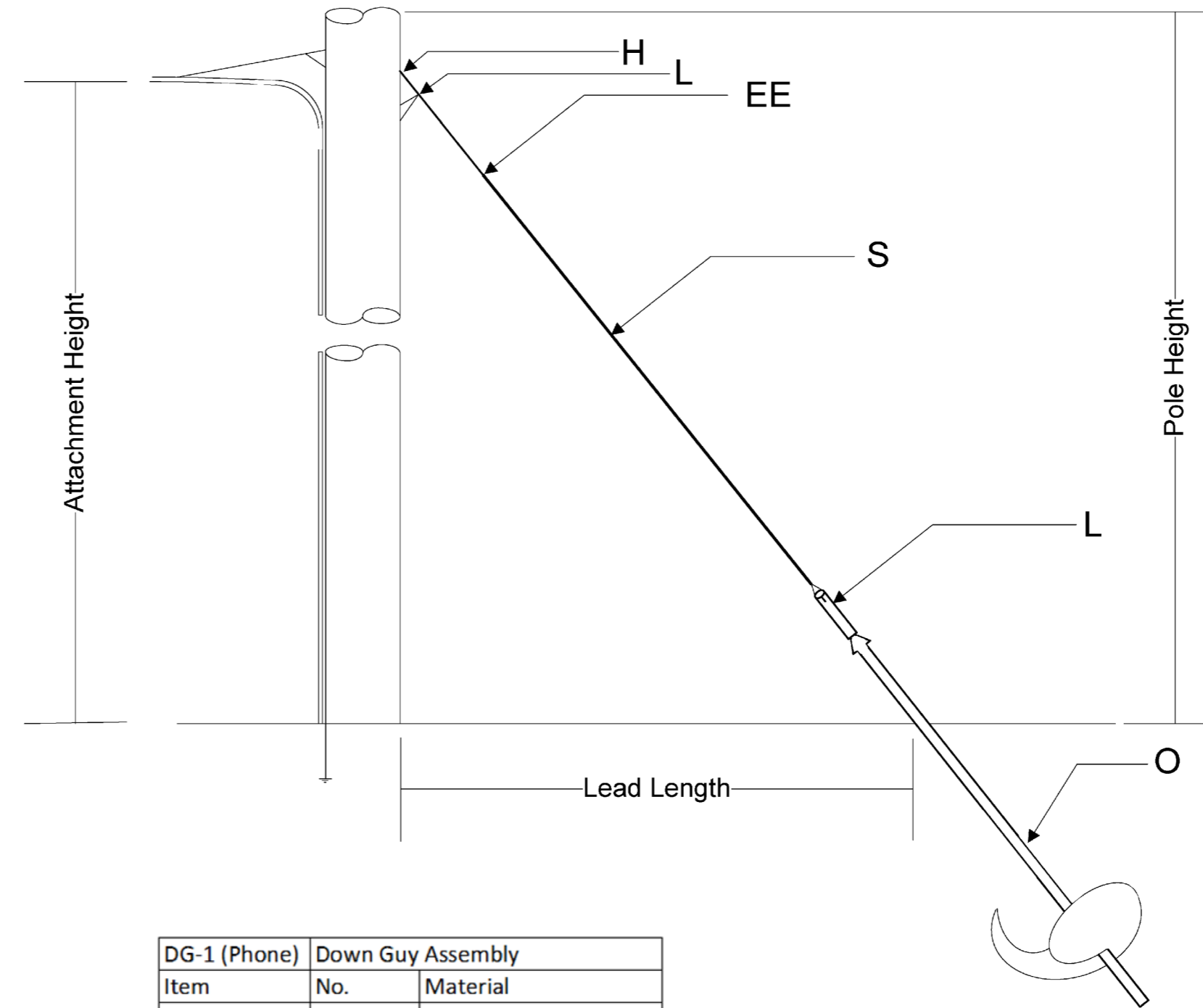
Dead End (DE-1)



DE-1	Dead End	
Item	Qty	Description
E	1	Eye Bolt
A	1	Square Nuts
DG-1	1	DG Assembly
AA	2	Strap and Spacers
L	1	Dead End Grips
DD	1	Fiber Tags
Z	1	Lashing Wire Clamps
B	1	Square Washer
GA-1	1	Ground Assembly

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

Anchor & Down Guy Detail (DG-1, DG-2)



DG-1 (Phone)	Down Guy Assembly	
Item	No.	Material
H	1	Guy Hook Attachment
EE	1	35' Strand
O	1	Anchor
S	1	Guy Guard
L	2	Dead End Grips

USE CITY SECONDARY STANDARDS FOR SECONDARY POLE ATTACHMENTS

ENTRUST COMMS

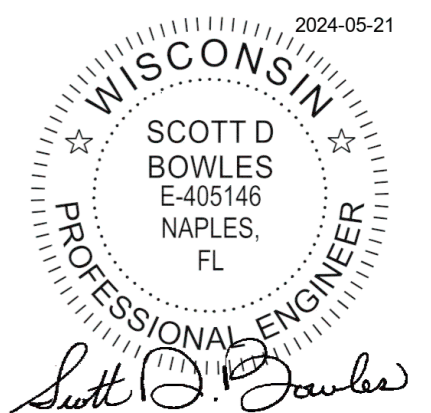
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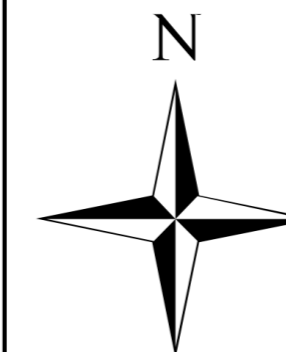
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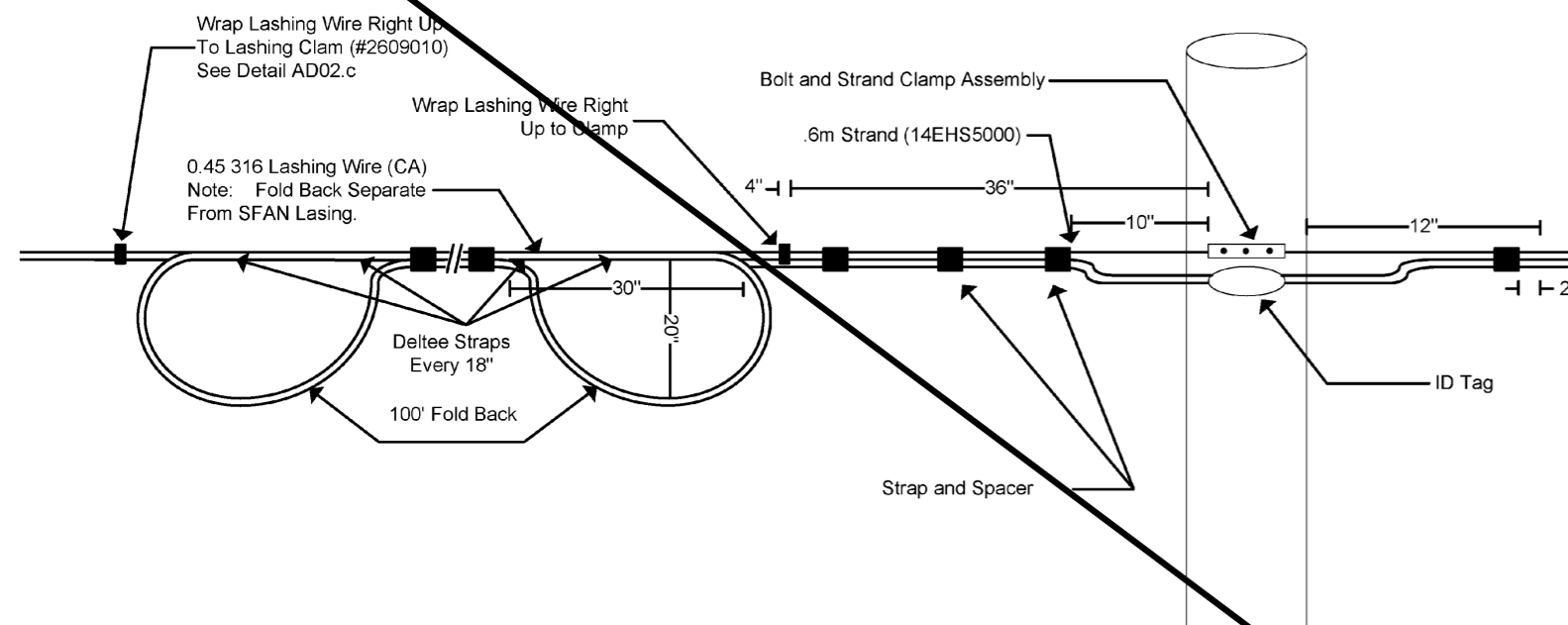
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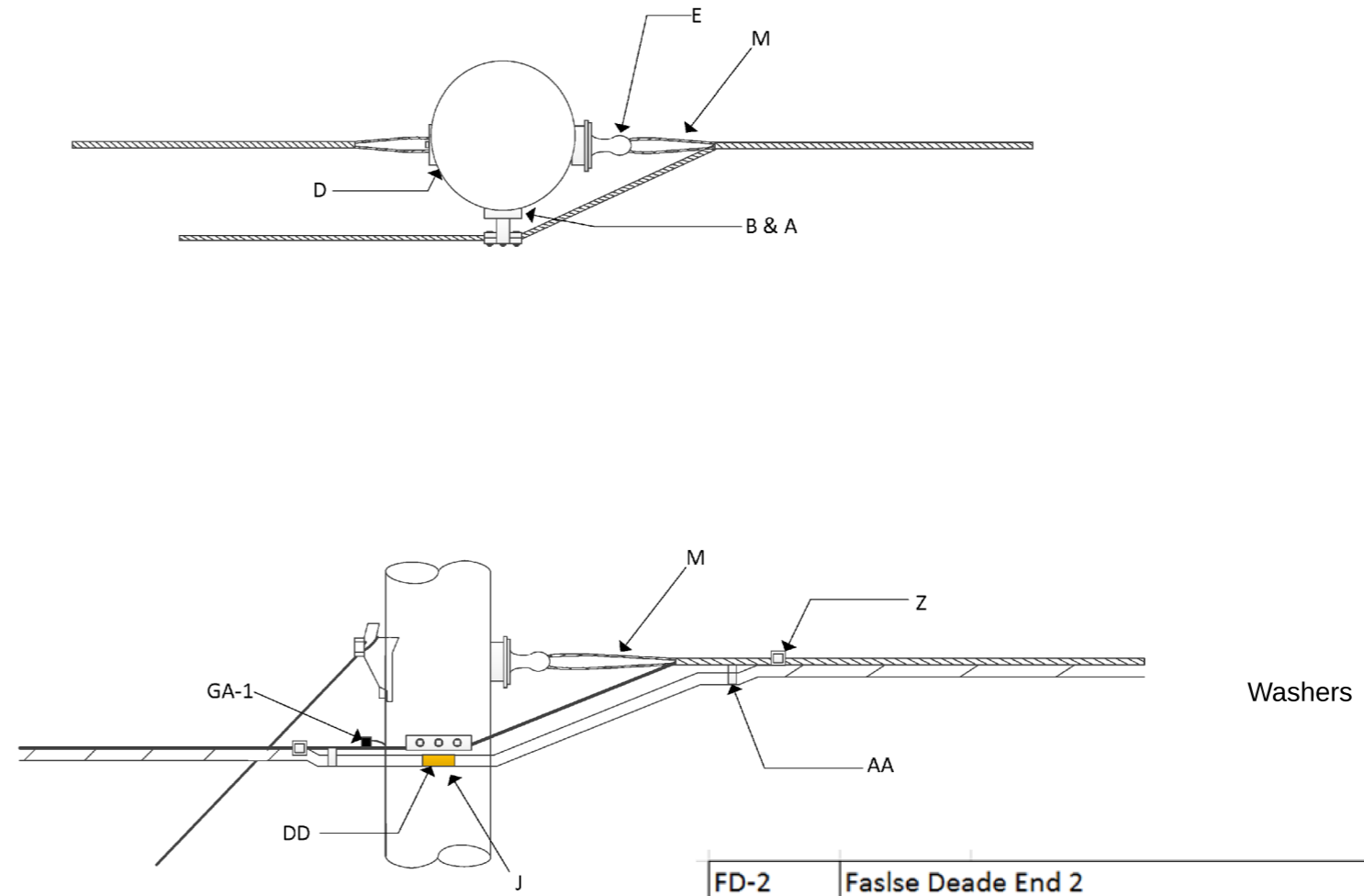


USE SNOW SHOE STYLE STORAGE FOR ALL EXCESS AND SLACK AERIAL FIBERCABLE. INSTALL TO MANUFACTURER SPECIFICATIONS

Excess Aerial Cable Slack Storage No Sno-Shoe



False Dead End (FD-2)



FD-2	False Dead End 2	
Item	No.	Material
J	1	3 Bolt Straight Suspension Clamp
D	1	Machine Bolt (Various Lengths)
B	3	Square Washers
A	3	Square Nuts
Z	2	Lashing Wire Clamps
DD	1	Fiber Tags
AA	4	Strap and Spacers
E	1	Eye Bolt
DG-1	1	DG Assembly
M	1	False and Dead End Grips
GA-1	1	Ground Assembly

USE CITY SECONDARY STANDARDS FOR SECONDARY POLE ATTACHMENTS

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

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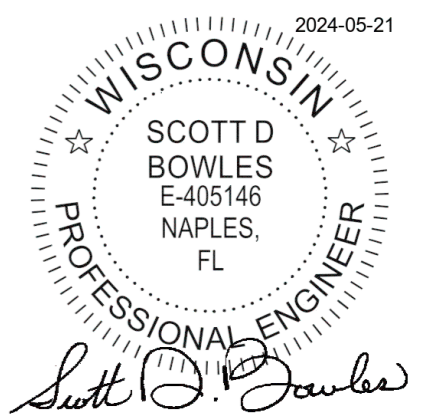
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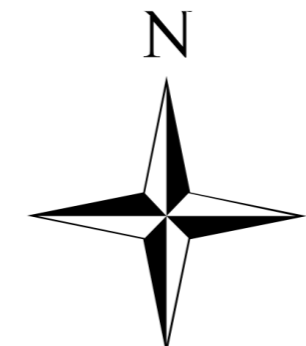
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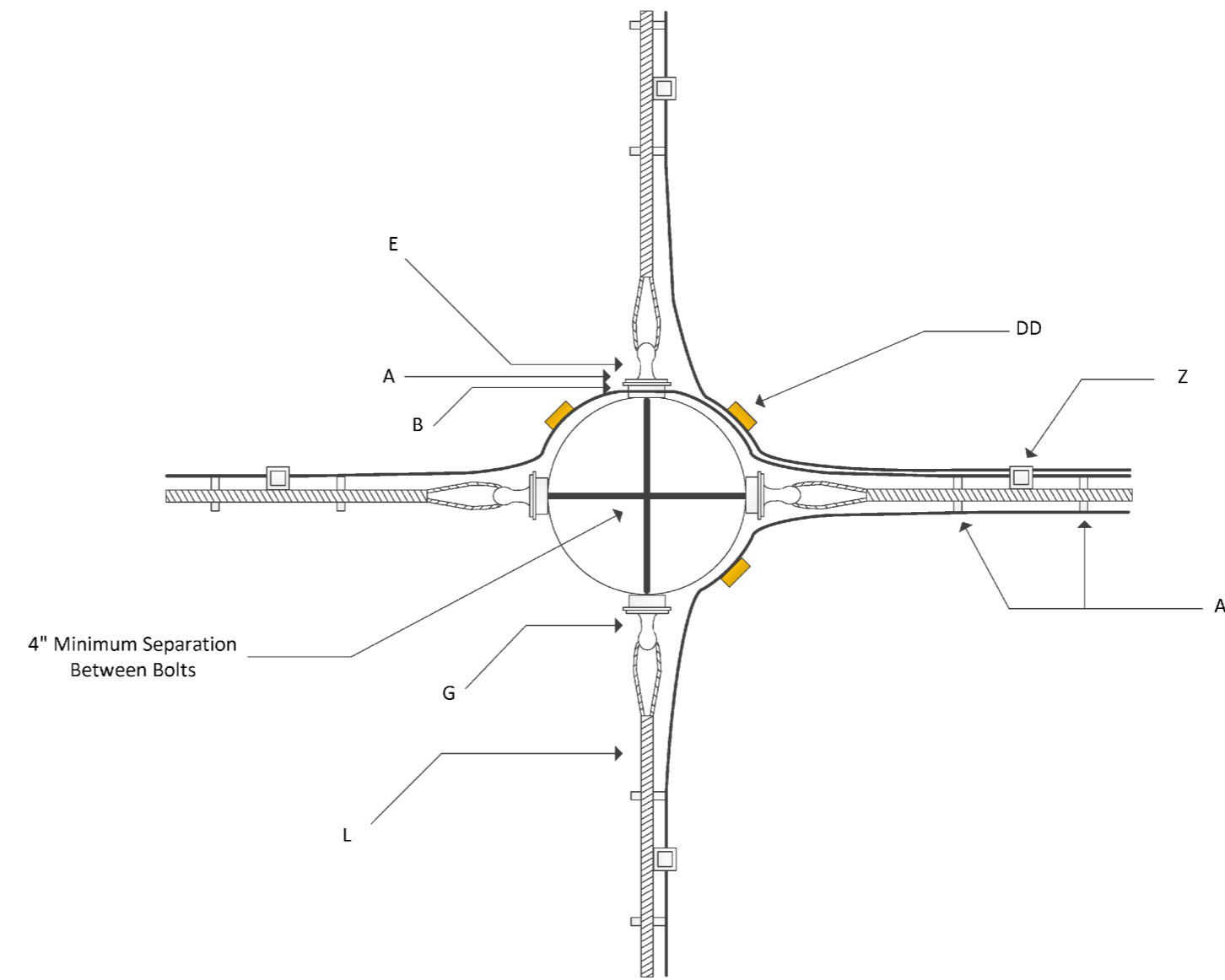
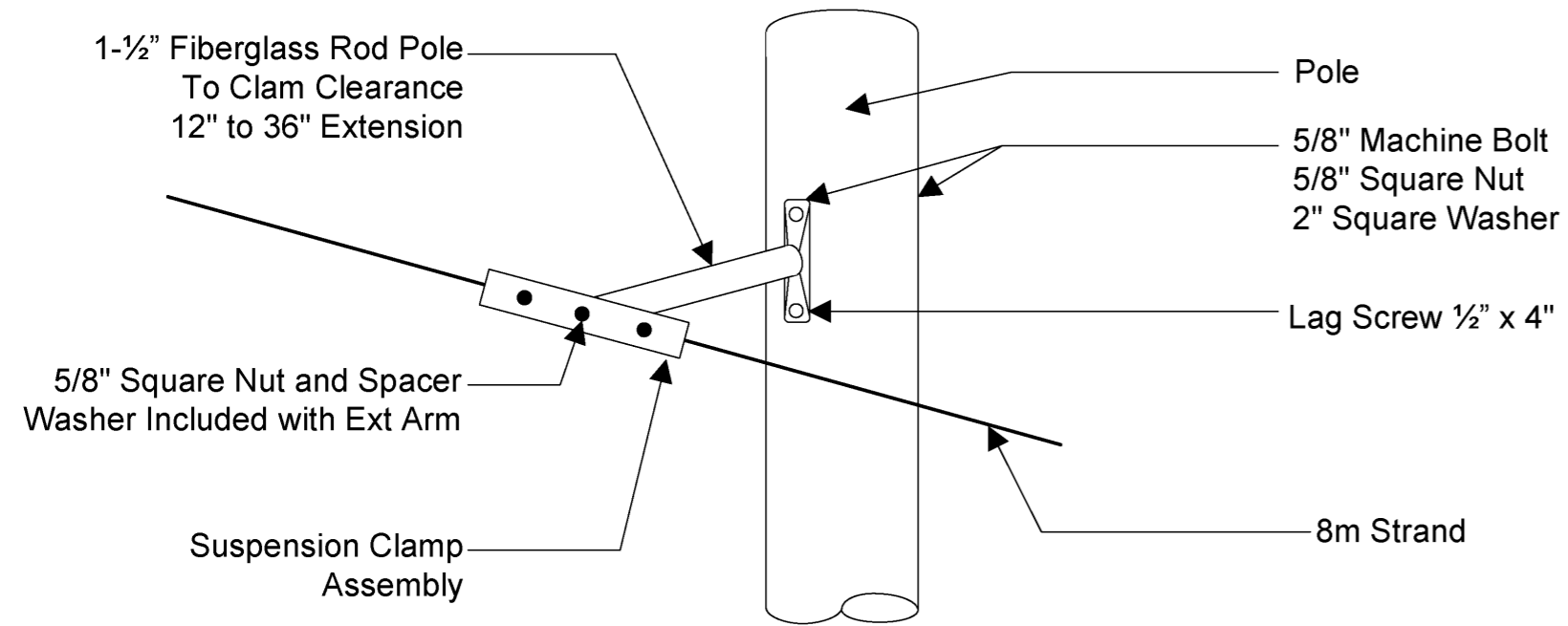
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Four Way (FW-1)

Fiberglass Extension



Item	Description
FW-1	Four Way
E	Eye Bolts
G	Eye Nuts
A	Square Nuts
B	Square Washers
Z	Lashing Wire Clamps
AA	Strap and Spacers
DD	Fiber Tag
L	Dead End Grips
GA-1	Ground Assembly

USE CITY SECONDARY STANDARDS FOR
SECONDARY POLE ATTACHMENTS

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

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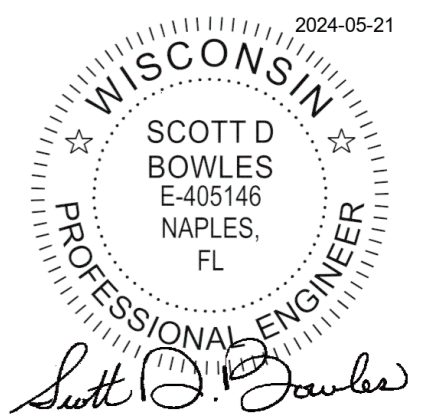
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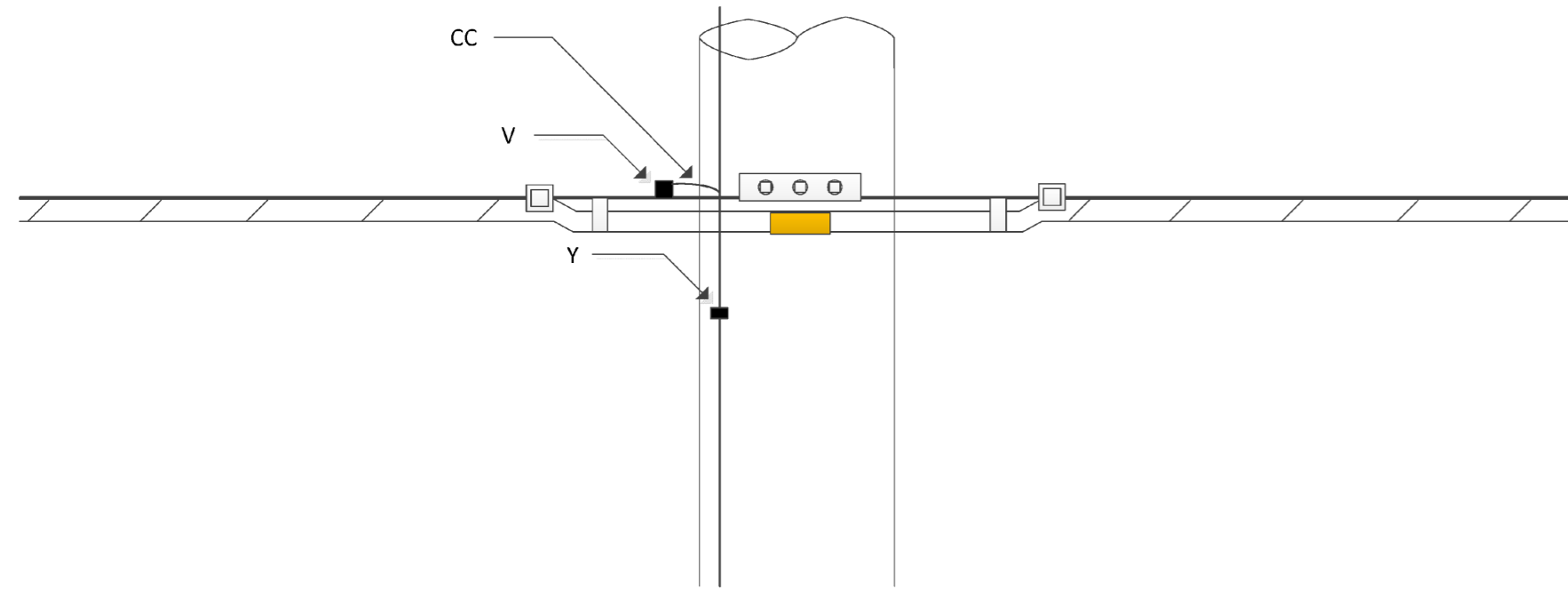
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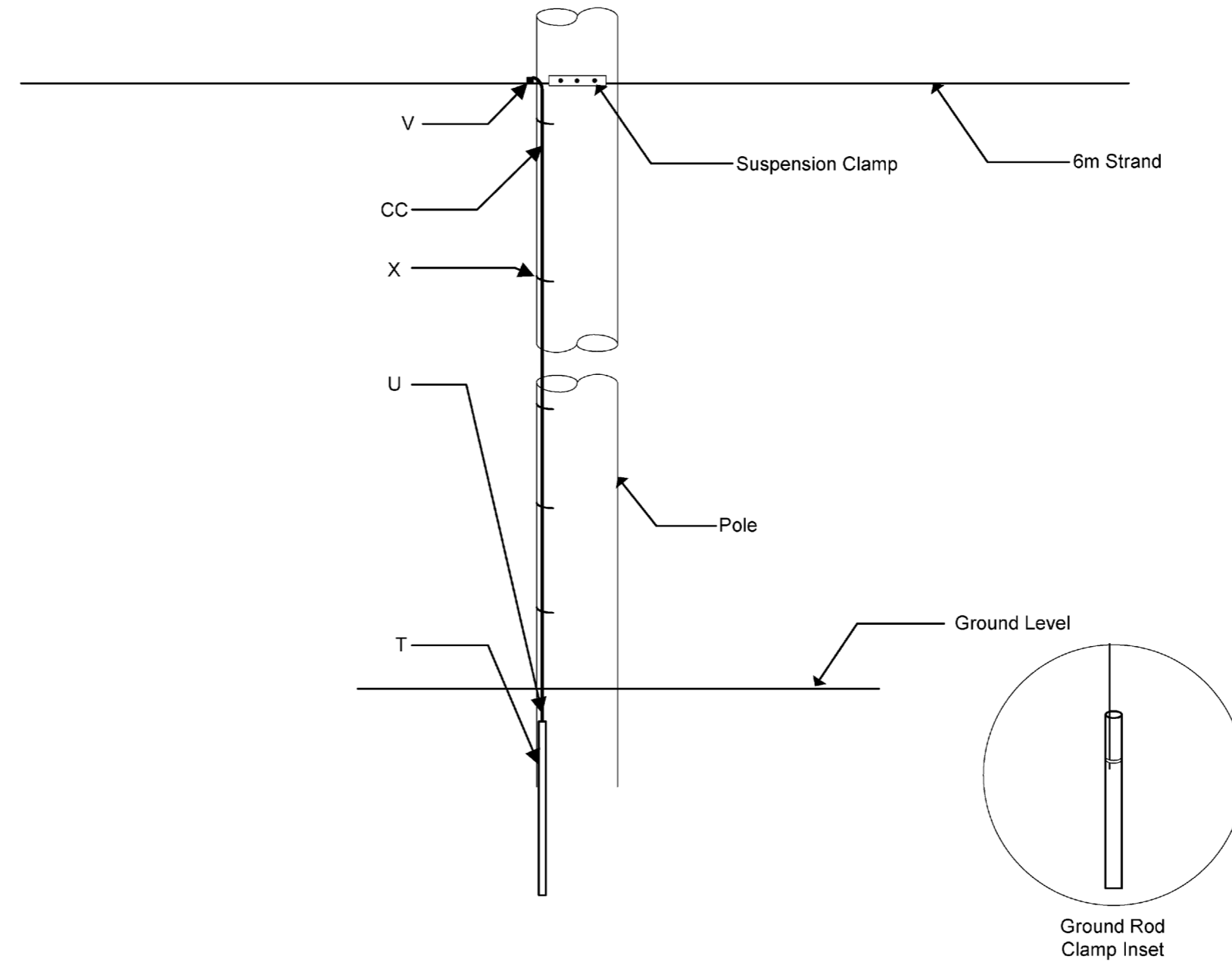
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Ground Assembly (GA-1) (PM2a)



GA-1 Ground Assembly (PM2a)		
Item	Qty	Description
V	1	K1 Ground Clamp
CC	1	8" #6 Bare Copper
Y	1	Split Bolt

Ground Assembly GA-2



GA-2 Ground Assembly 2		
Item	No.	Material
V	1	K1 Ground Clamp
CC	1	25' #6 Bare Copper
T	1	8' 5/8 Ground Rod
U	1	5/8 Ground Rod Clamp
X	0	Copper States

USE CITY SECONDARY STANDARDS FOR SECONDARY POLE ATTACHMENTS

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

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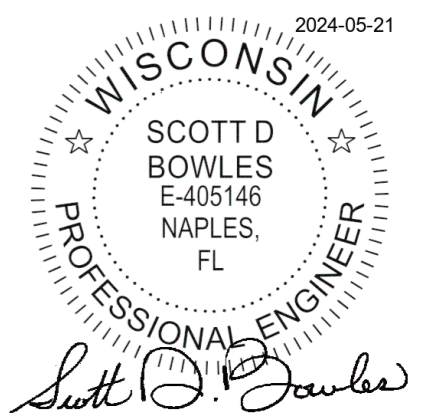
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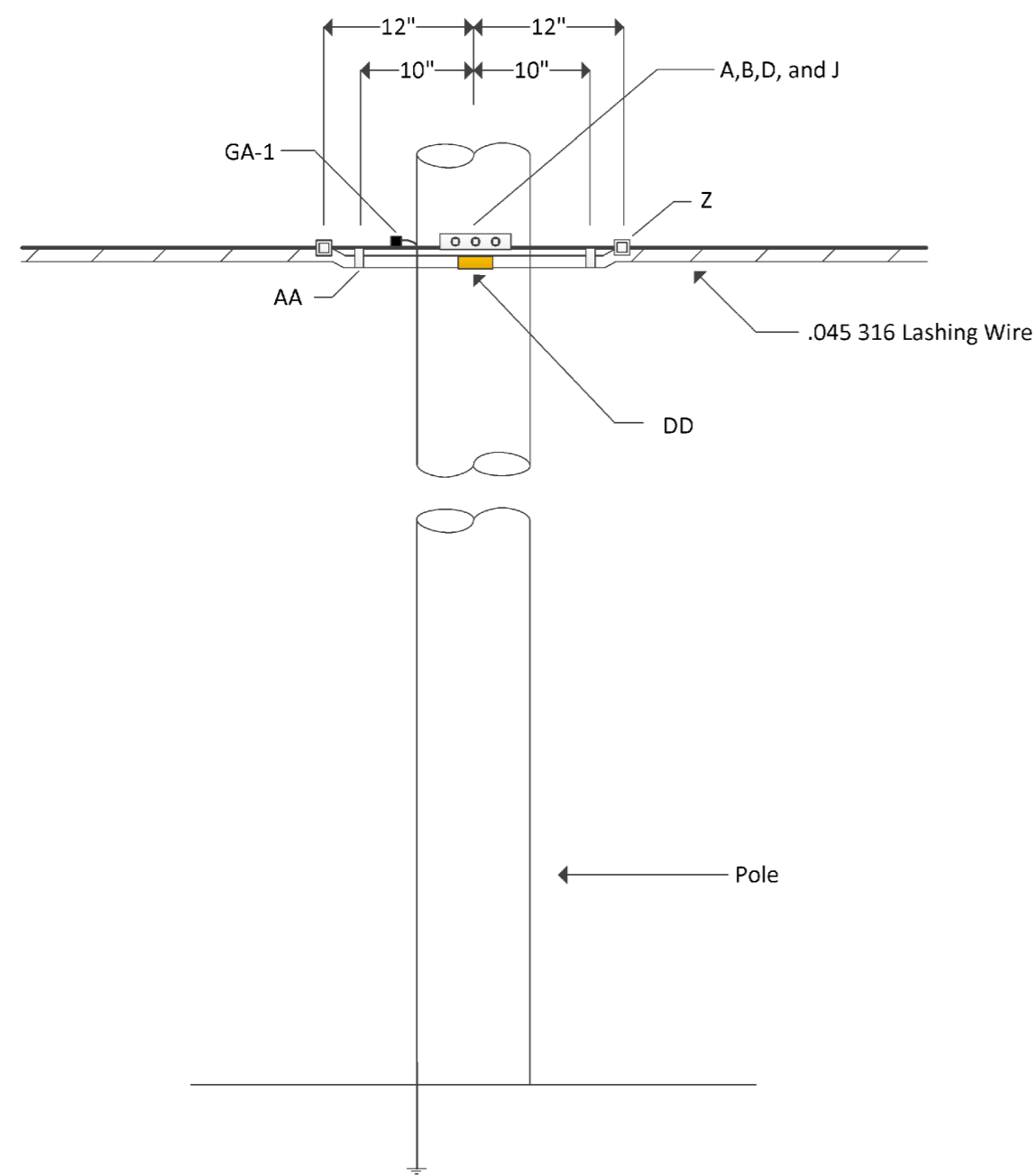


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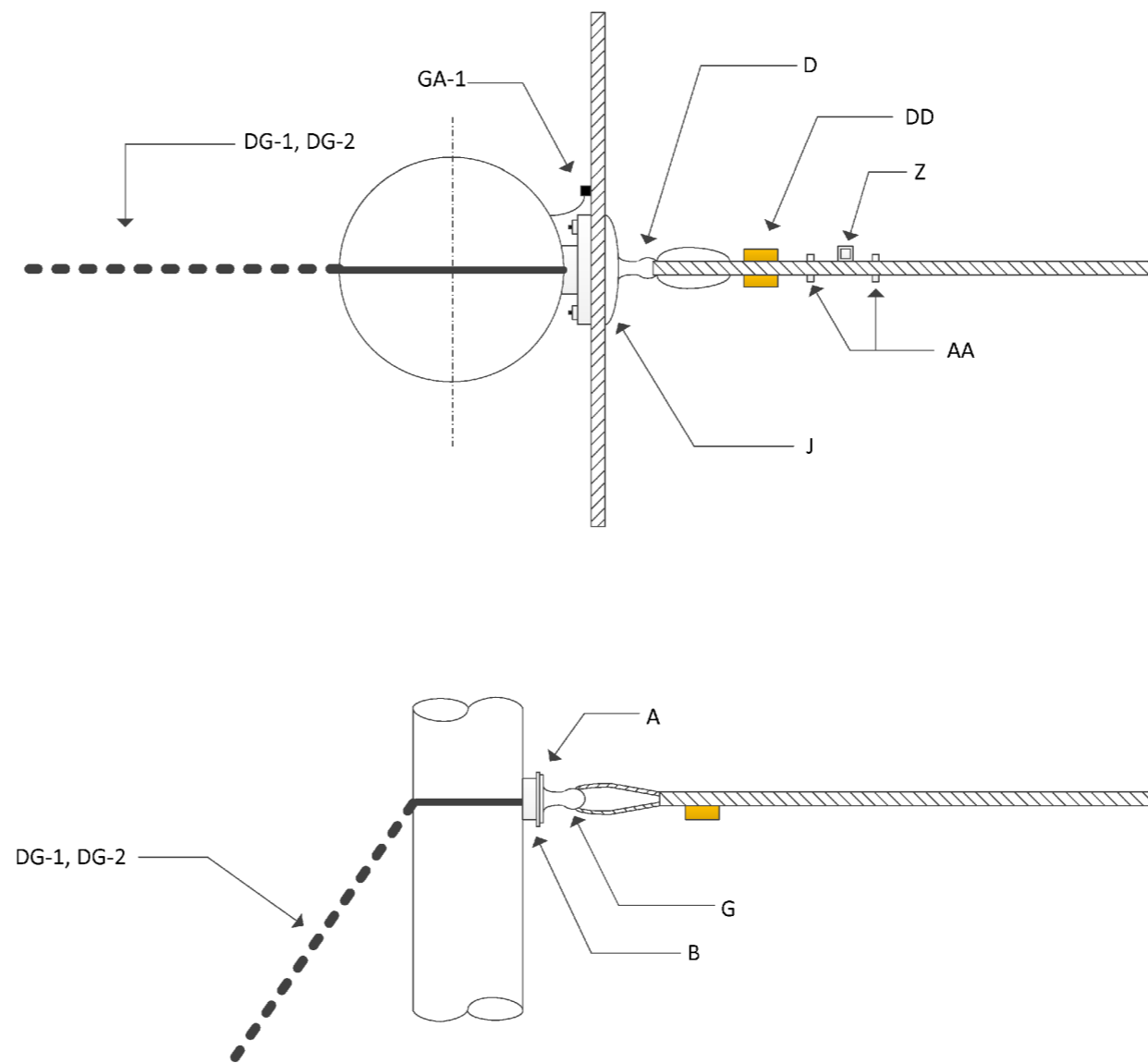
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Straight Through (ST-1)



ST-1	Straight Through 1	
Item	Qty	Description
J	1	3 Bolt Straight Suspension Clamp
D	1	Machine Bolt (Various Length)
B	2	Square Washers
A	2	Square Nuts
Z	2	Lashing Wire Clamps
DD	1	Fiber Tag
AA	2	Strap and Spacers
GA-1	1	Ground Assembly

T-Pole (TP-1)



TP-1	T Pole	
Item	Qty	Description
J	1	3 Bolt Straight Suspension Clamp
D	1	Machine Bolt (Various Length)
B	1	Square Washers
AA	1	Square Nuts
Z	3	Lashing Wire Clamps
AA	4	Strap and Spacers
DD	2	Fiber Tag
G	1	Thimble Eye Nuts
DG-1	1	Down Guy Assembly
GA-1	1	Ground Assembly

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

ENTRUST COMMS

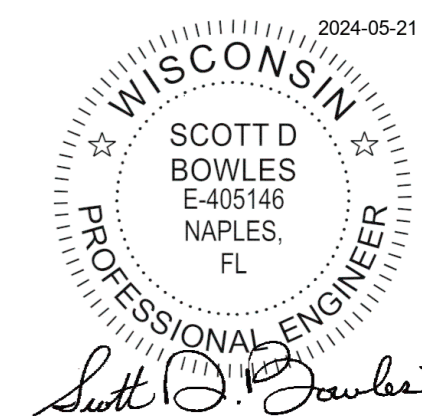
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SHEET-TYP-13

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CHECKED BY: JW

REVISIONS

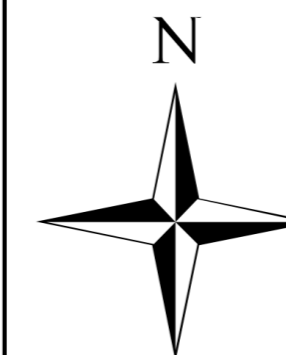
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



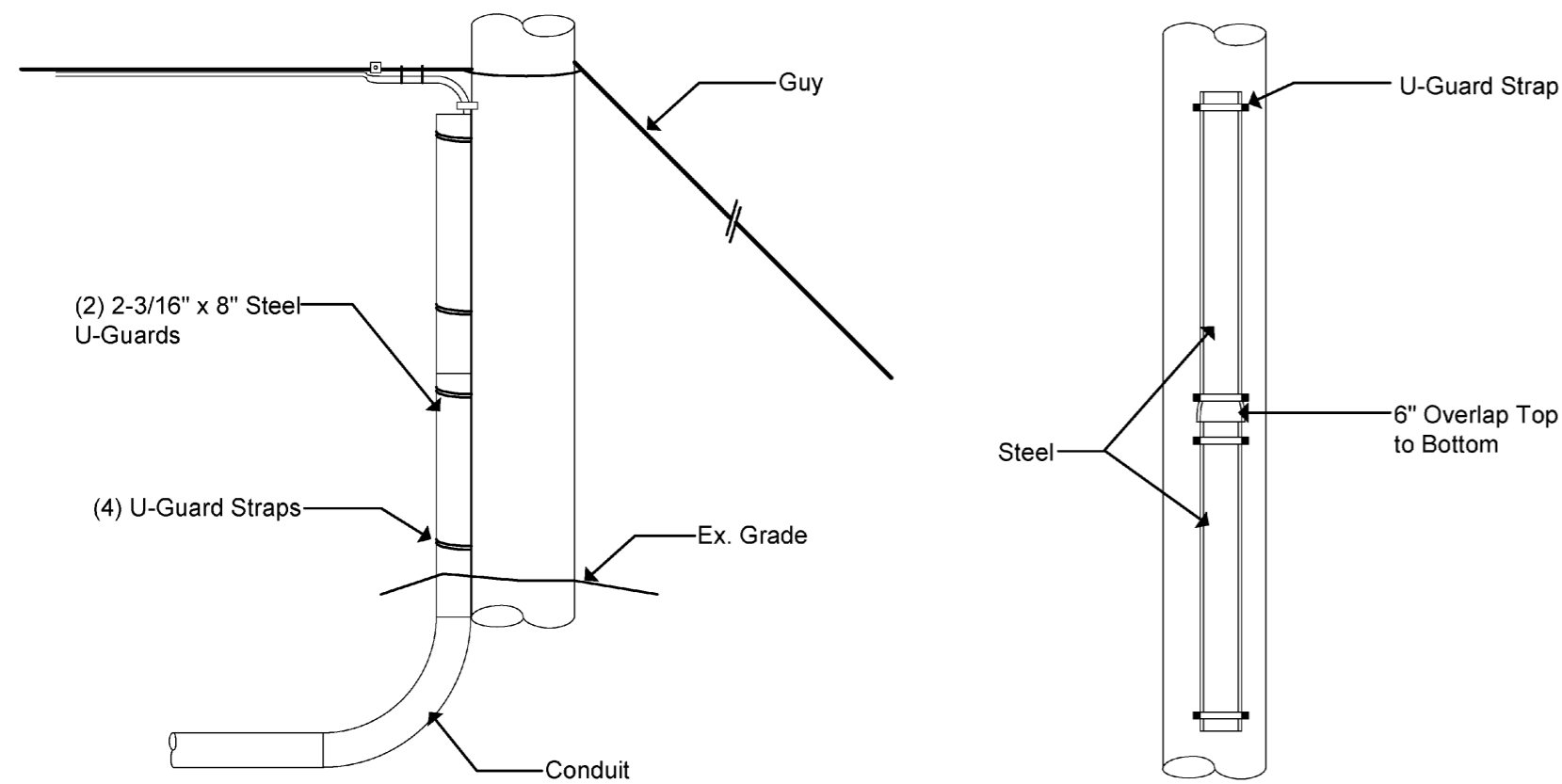
SOME UTILITIES SHOWN ON THESE PLANS ARE PER GIS DATA. LOCATIONS AND DEPTHS FOR ALL UTILITIES ARE APPROXIMATE. NOT ALL UTILITIES ARE SHOWN ON PLANS

PRIOR TO CONSTRUCTION CALL call811.org (TOLL FREE) AT 1-800-242-8511 OR 811 FOR LOCATION OF UNDERGROUND UTILITIES

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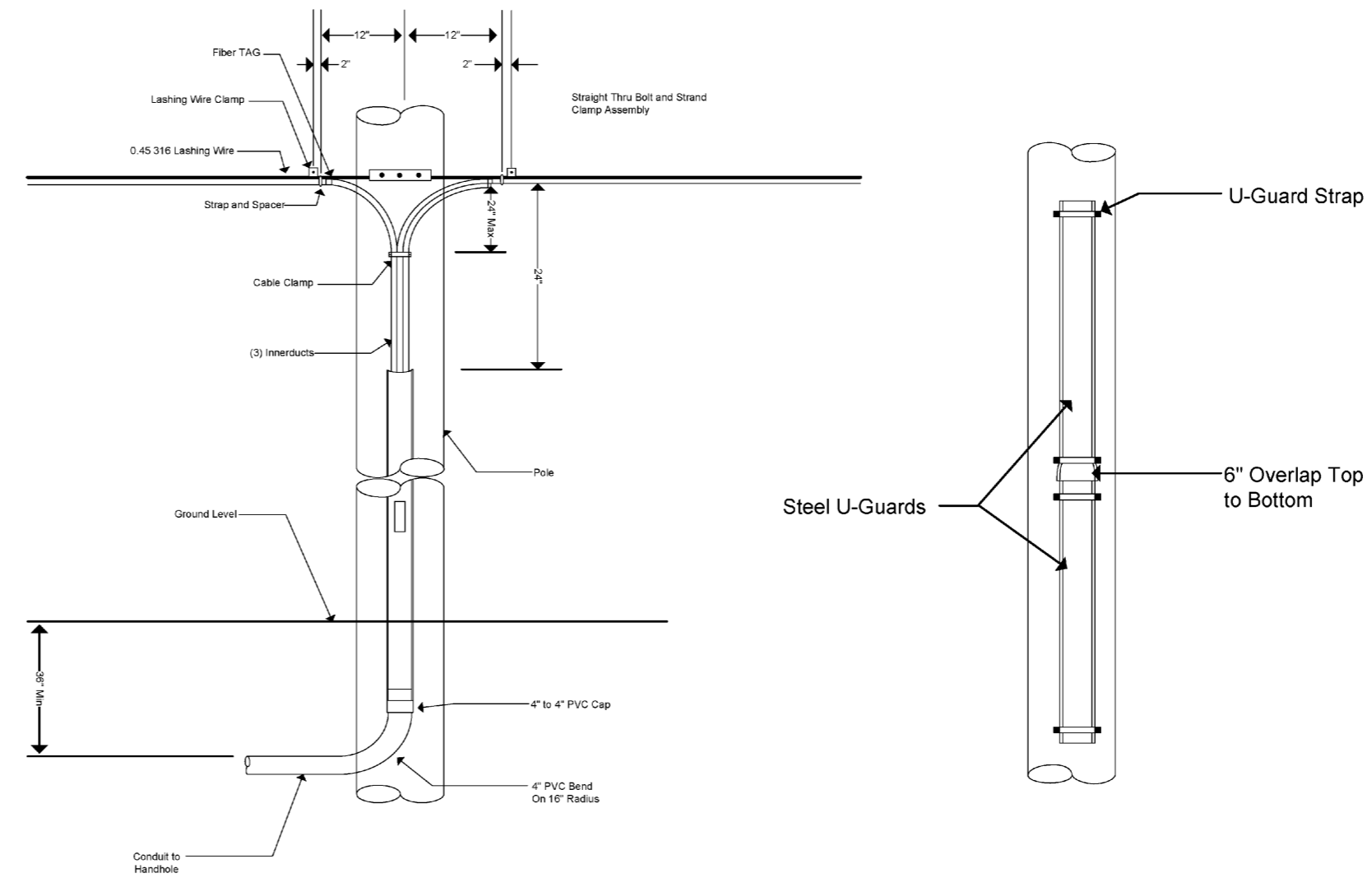
Typical Deadend Pole Arrangement for Aerial to Buried Cable



ALL RISERS INSTALLED ON CITY OF SUPERIOR
JOINT OWNED POLES TO BE 4 INCH DIAMETER

REFERENCE SHEET-CON-09 FOR MORE INFORMATION

Typical Riser Pole Detail



ALL RISERS INSTALLED ON CITY OF SUPERIOR
JOINT OWNED POLES TO BE 4 INCH DIAMETER

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-TYP-14

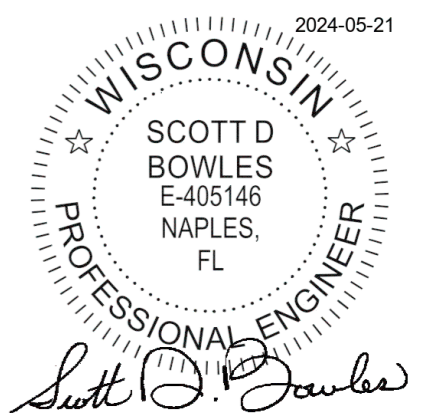
ENGINEER: SM

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MINIMUM DEPTH OF 36" UNLESS OTHERWISE
NOTED



DATE: 5/21/2024

SHEET: 31 OF 69

FILE: City of Superior, WI Construction
Standards_v4_05202024.pdf

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-TYP-15

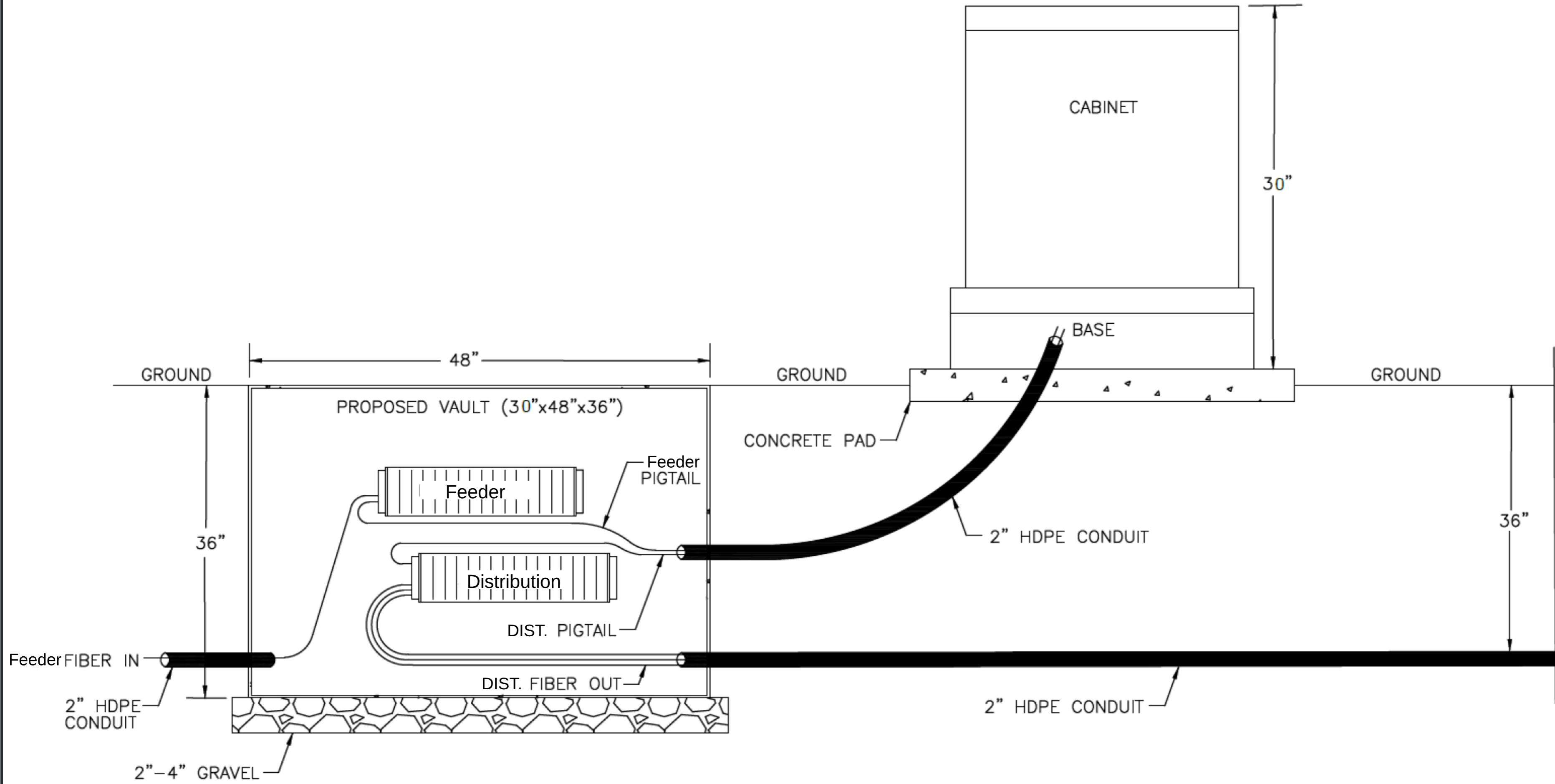
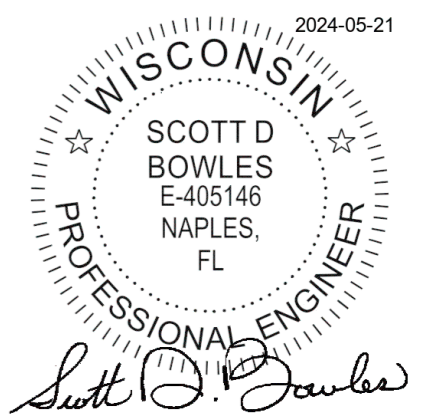
ENGINEER: SM

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REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



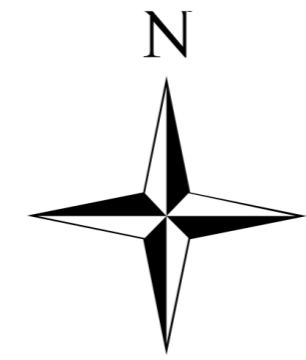
Fiber Distribution Hub Detail - Concrete Pad Mounted, 100' Pre-terminated SC/APC Tails

REFERENCE SHEET-CON-08 FOR MORE INFORMATION

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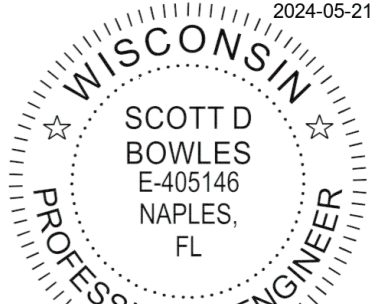
NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED



ENGINEER: SM
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2024-05-21

 Scott D. Bowles

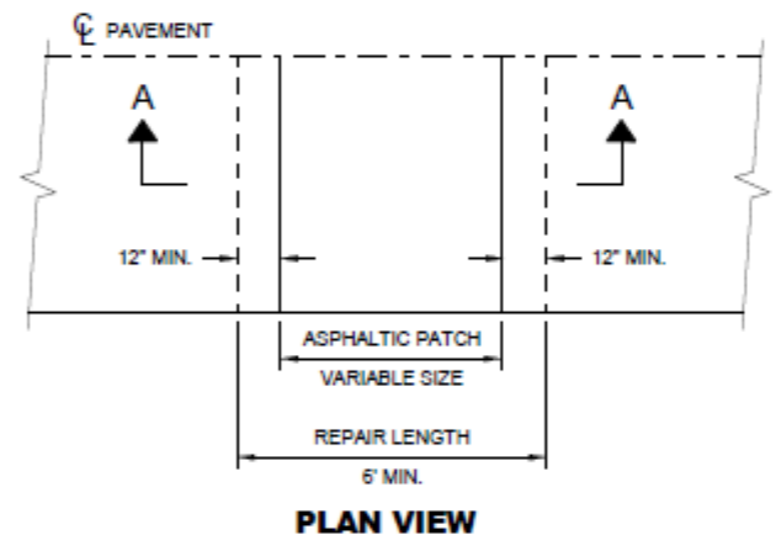
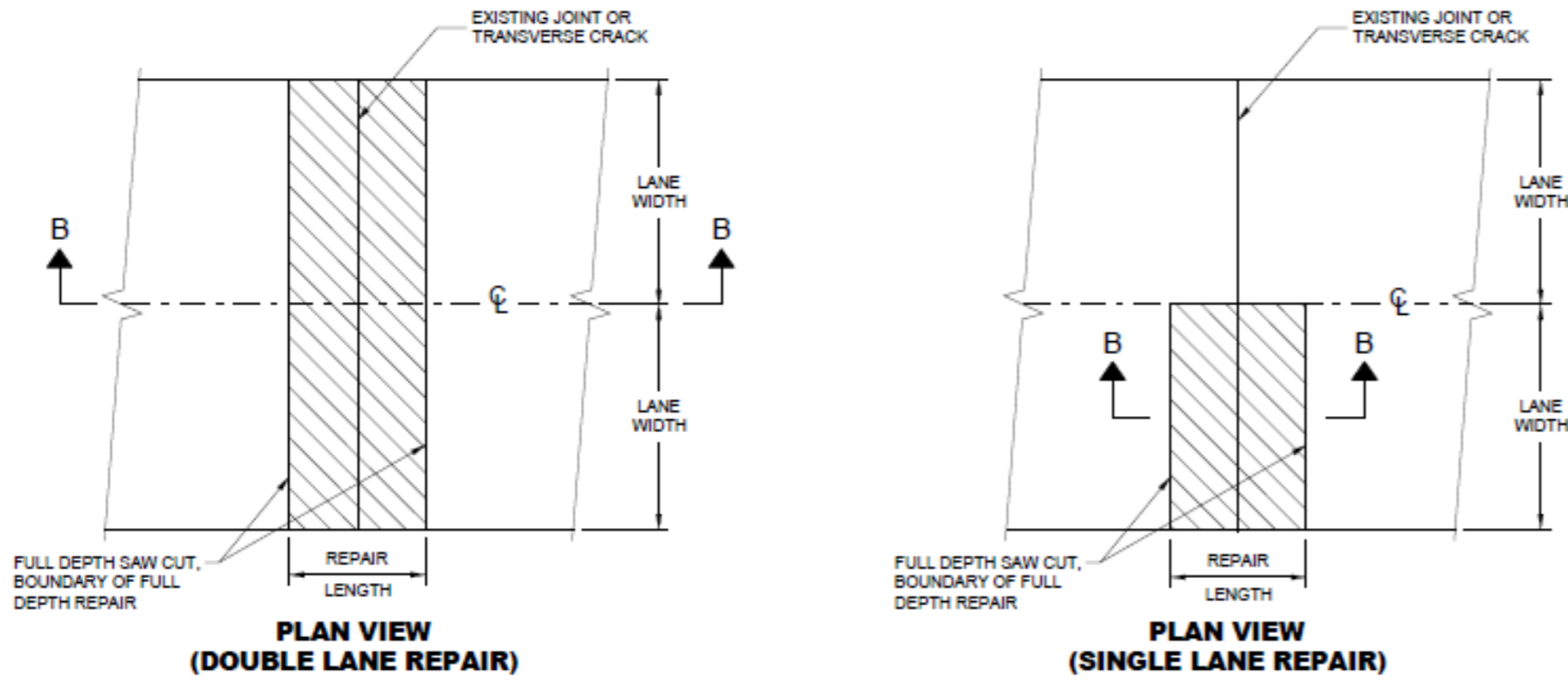
GENERAL NOTES

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES.

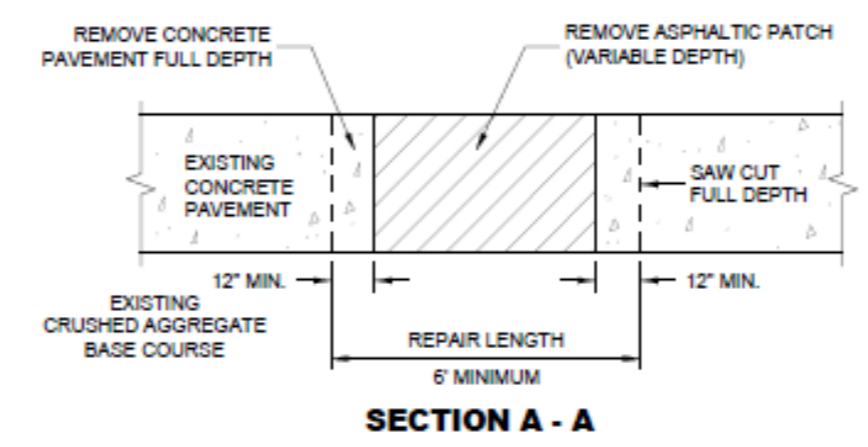
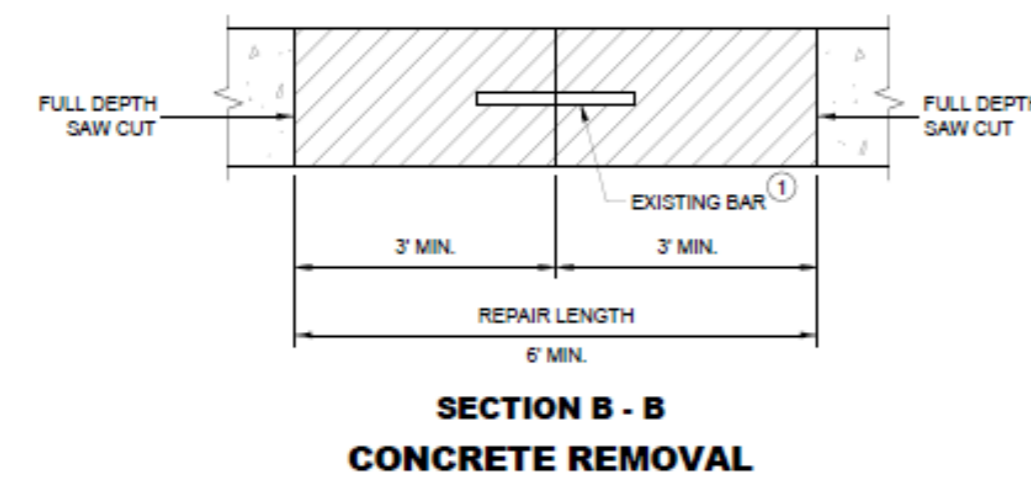
PROVIDE A 6 FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREA TO ADJACENT TRANSVERSE JOINT OR CRACK.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NON-DOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

① DOWEL BARS MAY NOT BE PRESENT.



FULL DEPTH CONCRETE PAVEMENT REMOVAL



**CONCRETE PAVEMENT
REPAIR AND REPLACEMENT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

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SDD 13C09 - 17a

SDD 13C09 - 17a

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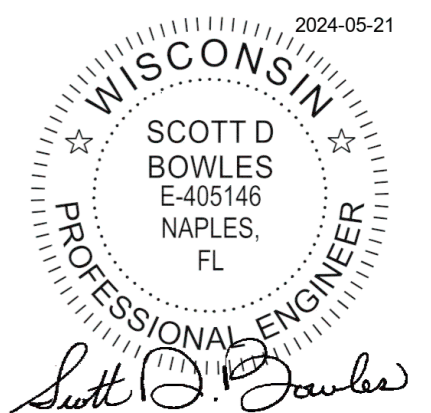
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ENGINEER: SM
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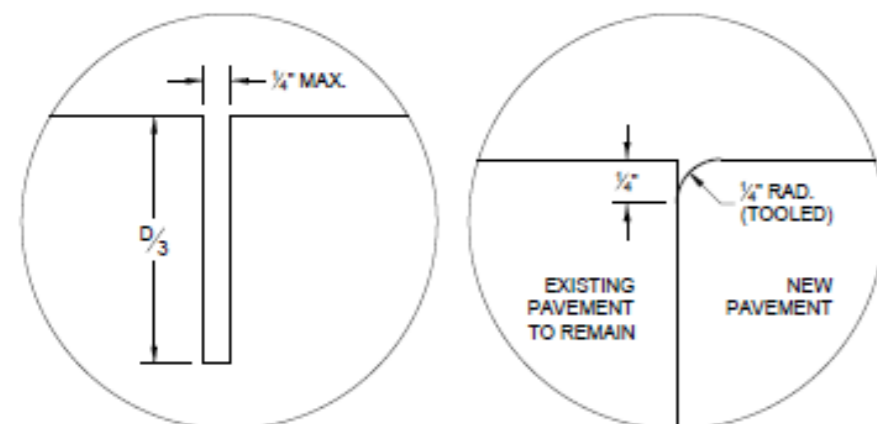
REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

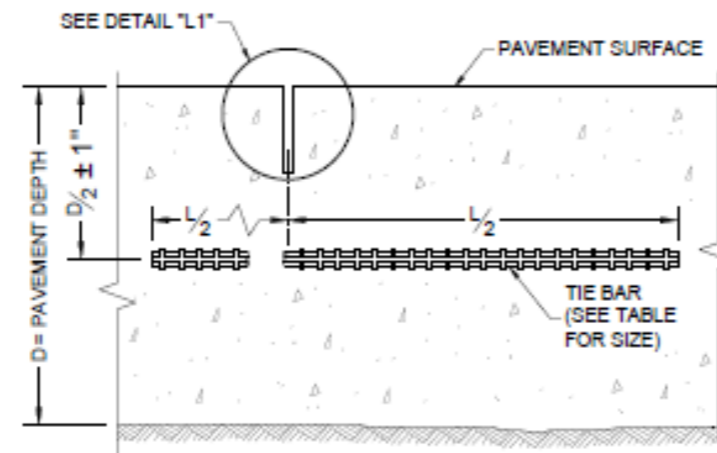


GENERAL NOTES

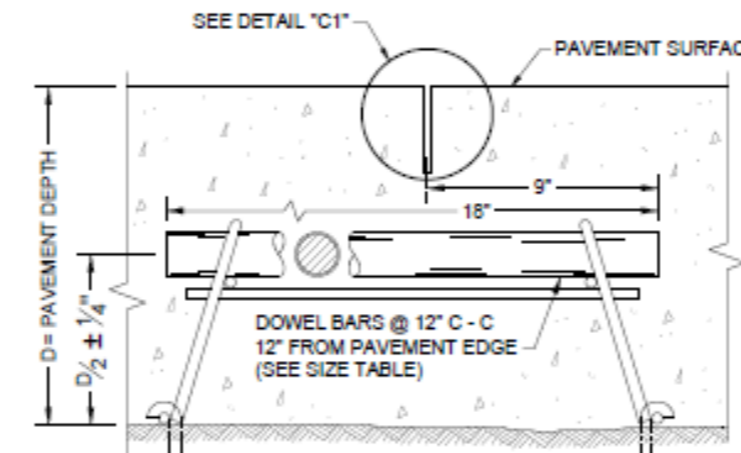
- INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.
- CONCRETE PAVEMENT REPAIRS OF EXISTING NON-DOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.
- ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
- FOR MULTI-LANE CONCRETE PAVEMENT REPLACEMENTS, PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.
- ① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.



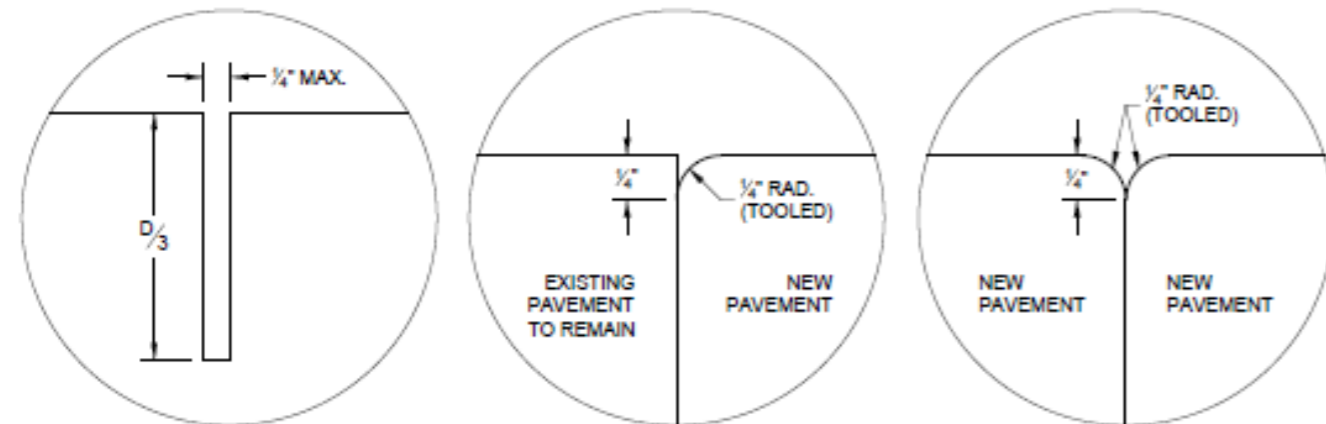
TRANSVERSE JOINTS



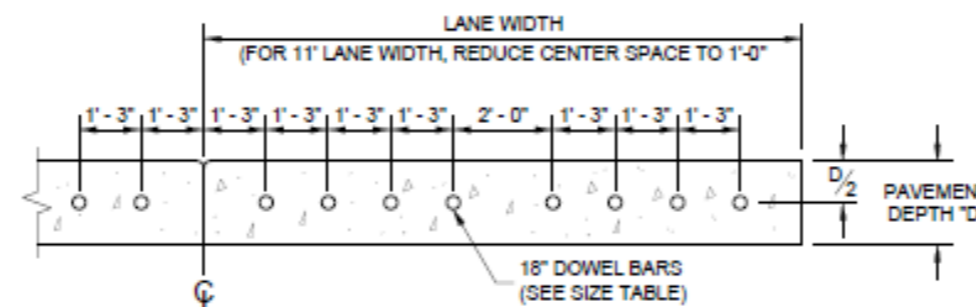
**SECTION C - C
SAWED LONGITUDINAL JOINT**



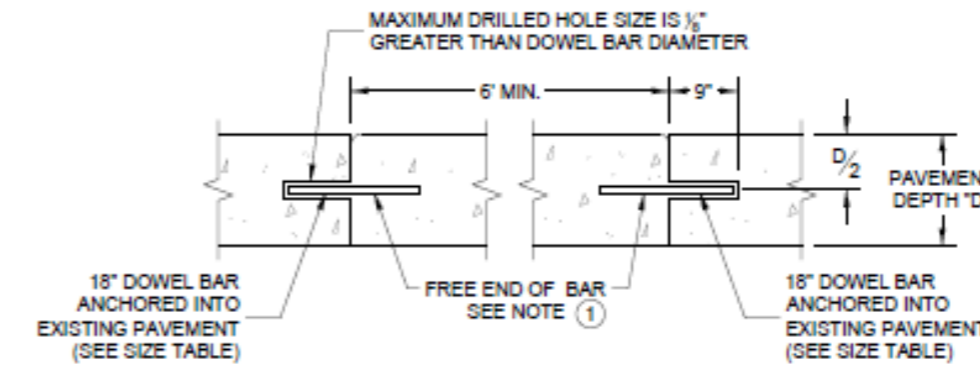
**SECTION F - F
DOWELED CONTRACTION JOINT**



LONGITUDINAL JOINTS



**SECTION E - E
DRILLED DOWEL BAR CONSTRUCTION JOINT**



SECTION D - D

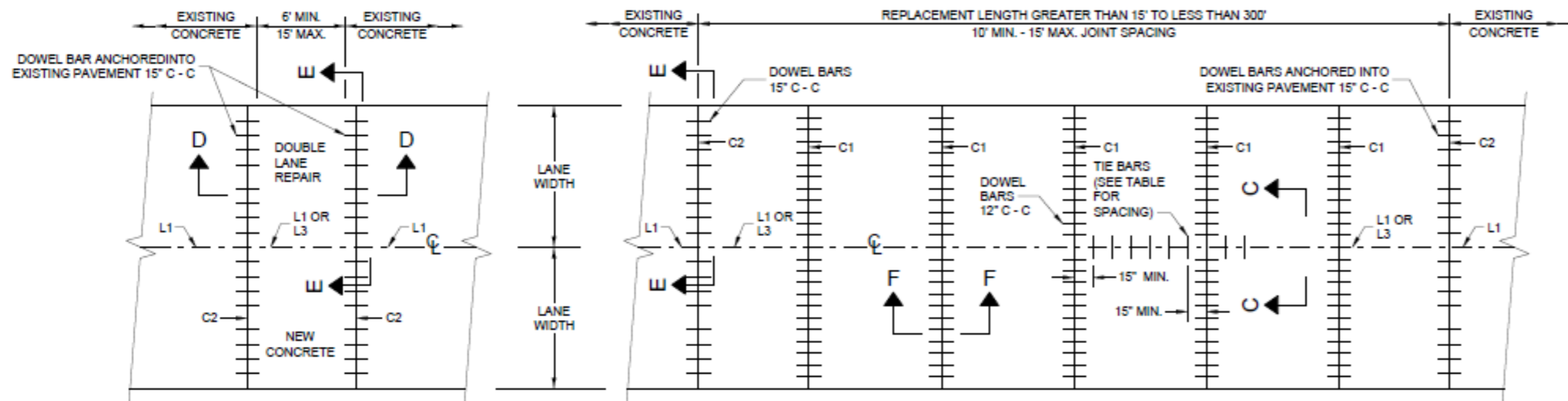
TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4*	30"	24**

- * SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)
- ** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	DRILLED DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
6", 6 1/2"	NONE	NONE	12"
7", 7 1/2"	1"	1"	14"
8" & ABOVE	1 1/2"	1 1/2"	15"



**PLAN VIEW
MULTILANE CONCRETE PAVEMENT REPAIR**

**PLAN VIEW
MULTILANE CONCRETE PAVEMENT REPLACEMENT**

**CONCRETE PAVEMENT
REPAIR AND REPLACEMENT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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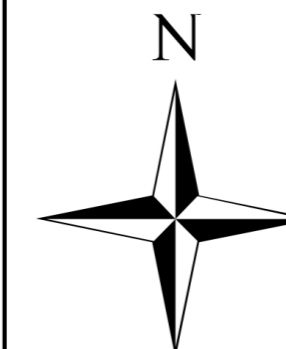
SDD 13C09 - 17b

SDD 13C09 - 17b

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ENTRUST COMMS

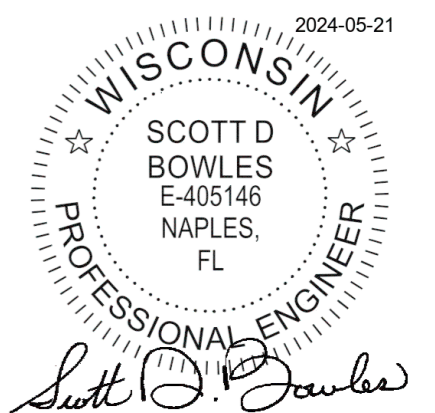
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-03

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

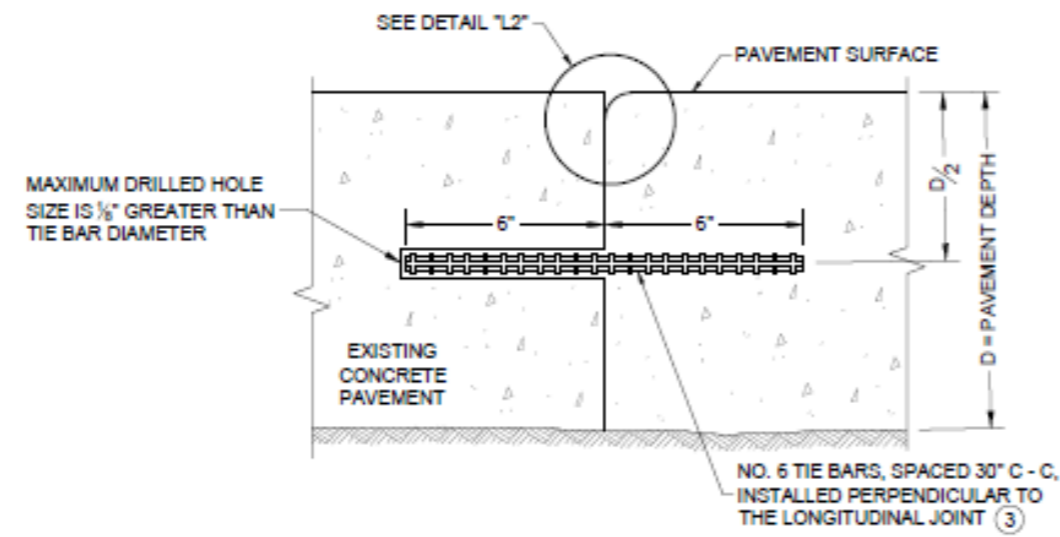
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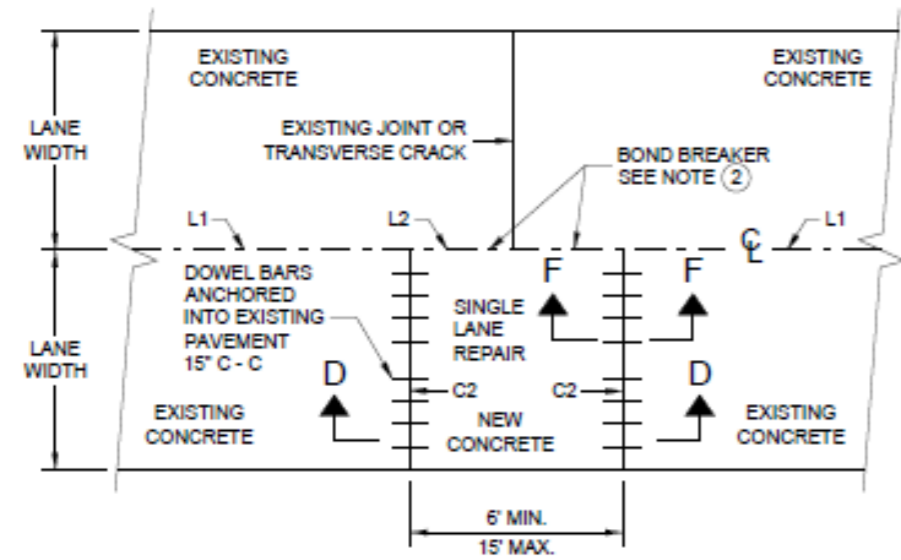


GENERAL NOTES

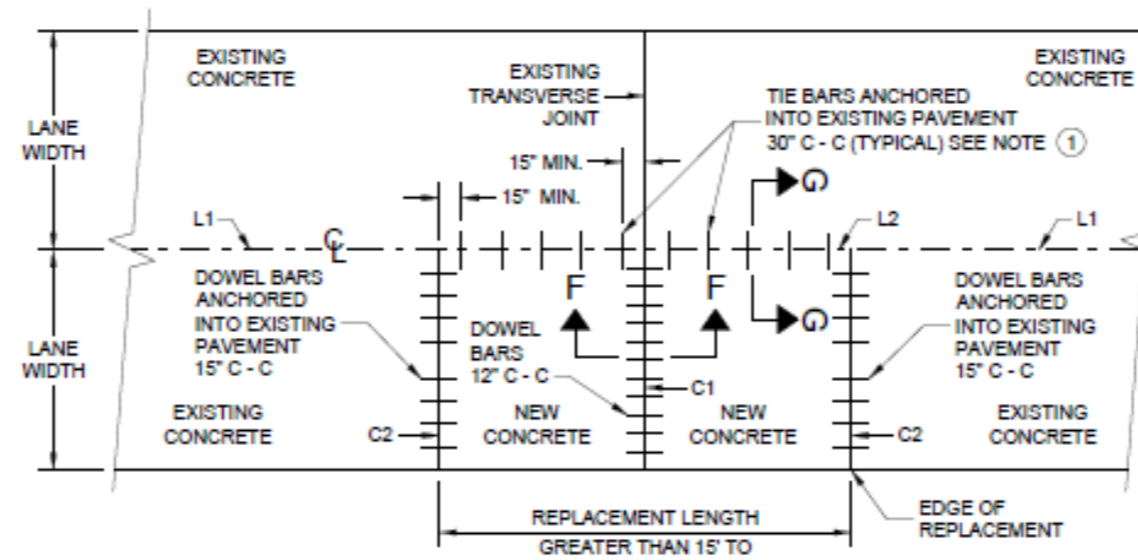
- WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES IN A HOLE OF SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- USE AN ENGINEER APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.
- ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



**SECTION G - G
TIE BARS ANCHORED INTO EXISTING PAVEMENT**



**PLAN VIEW
SINGLE LANE CONCRETE PAVEMENT REPAIR**



**PLAN VIEW
SINGLE LANE CONCRETE PAVEMENT REPLACEMENT**

**CONCRETE REPAIR
AND REPLACEMENT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 DATE /S/ Peter Kemp P.E. PAVEMENT SUPERVISOR

6

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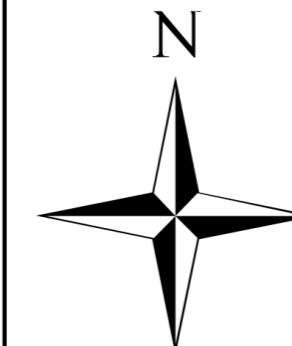
SDD 13C09 - 17C

SDD 13C09 - 17C

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ENTRUST COMMS

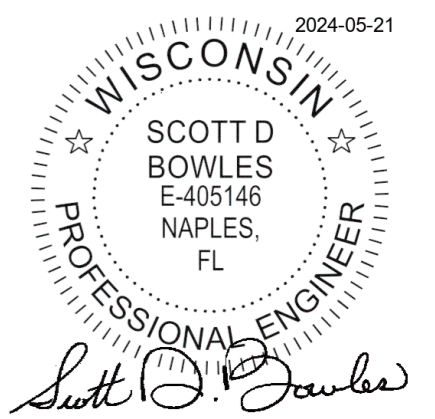
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-04

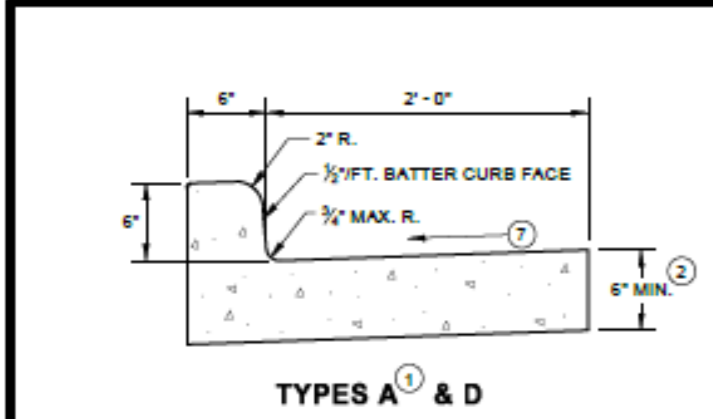
ENGINEER: SM
DRAWN BY: JW
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REVISIONS

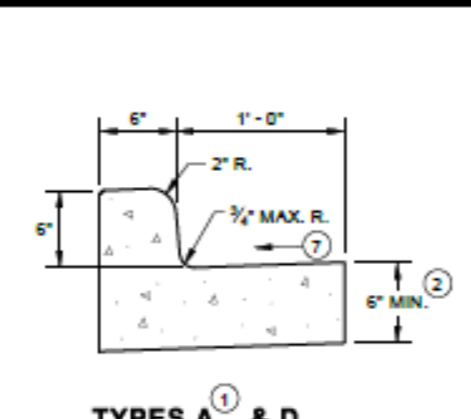
DATE	DESCRIPTION	BY
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DATE: 5/21/2024
SHEET: 36 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

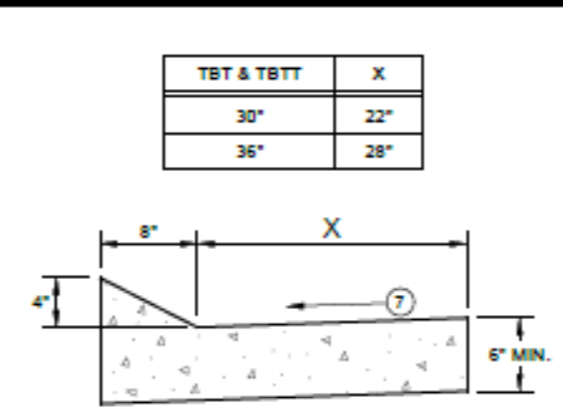


TYPES A¹ & D



TYPES A¹ & D

CONCRETE CURB AND GUTTER 18"



TYPES TBT & TBTT¹

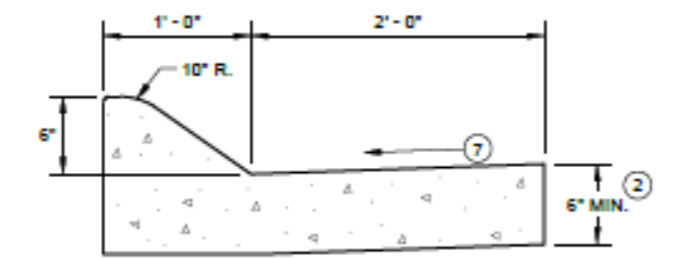
CONCRETE CURB AND GUTTER

GENERAL NOTES

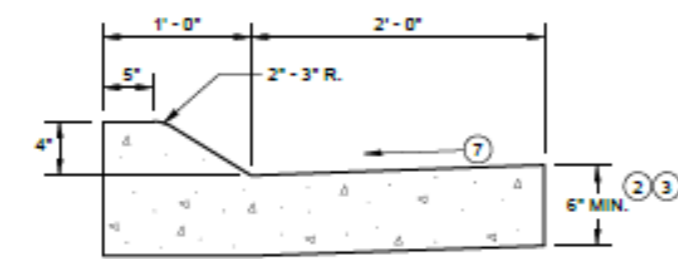
- DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.
- PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.
- INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.
- UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.
- TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBT.
 - THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
 - USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
 - THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
 - UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
 - WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
 - USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
 - INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWS.
 - CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'

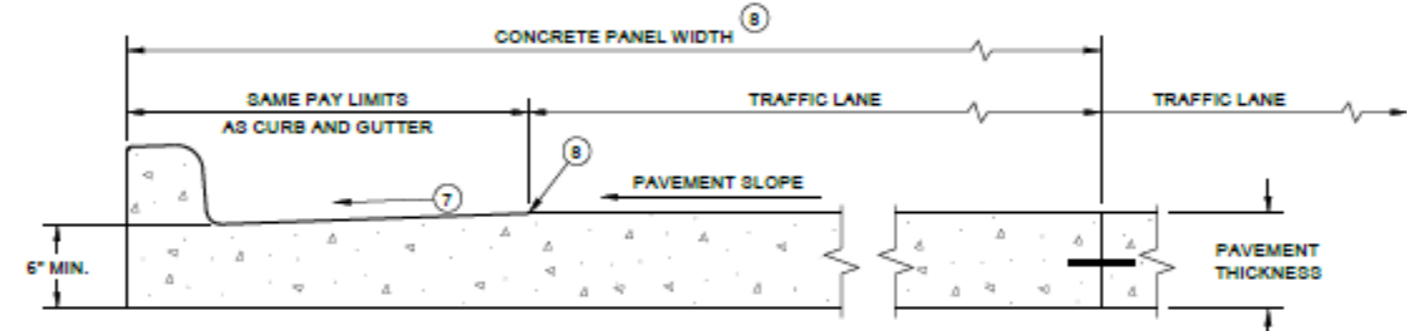


6" SLOPED CURB TYPES A¹ & D



4" SLOPED CURB TYPES A¹ & D

CONCRETE CURB AND GUTTER 36"



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

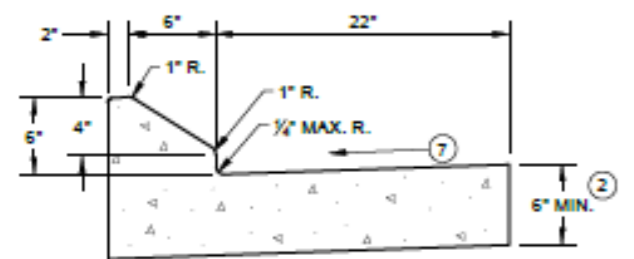
*BIKE LANE IS NOT SHOWN



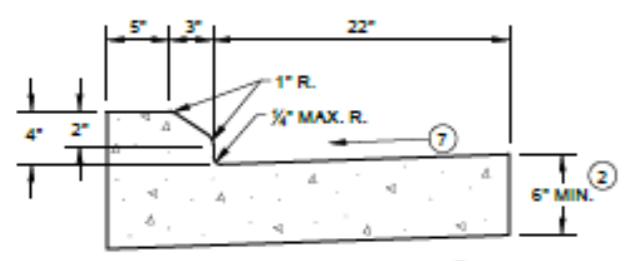
REVERSE SLOPE GUTTER⁵
(TYPICAL FOR ALL CURB & GUTTER TYPES)

CONCRETE CURB AND GUTTER

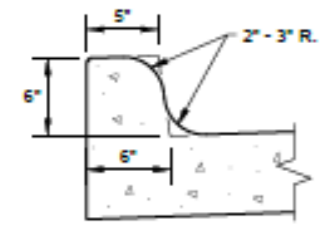
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



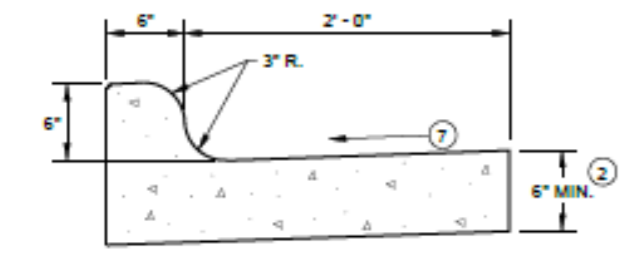
6" SLOPED CURB TYPES G¹ & J



4" SLOPED CURB TYPES G¹ & J

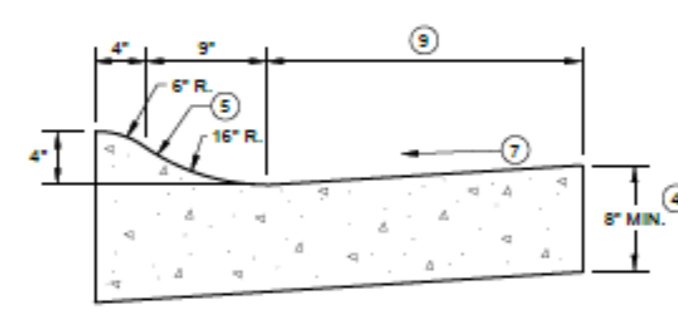


TYPES K¹ & L
(OPTIONAL CURB SHAPE)



TYPES K¹ & L

CONCRETE CURB AND GUTTER 30"



4" SLOPED CURB TYPES R¹ & T

6

6

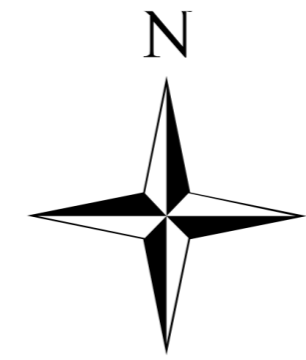
SDD 08D01 - 22a

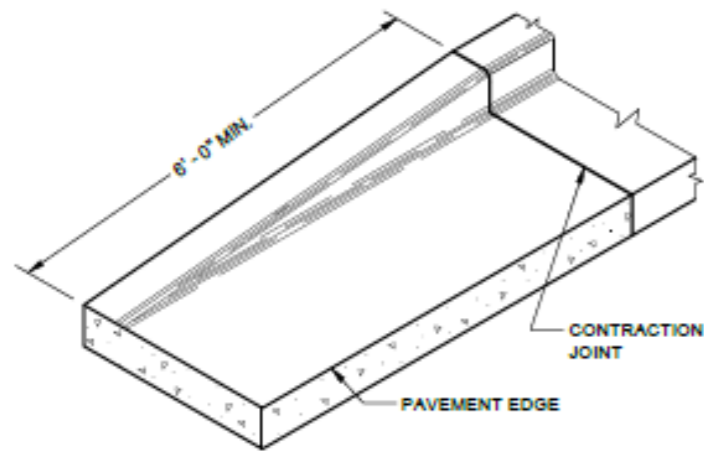
SDD 08D01 - 22a

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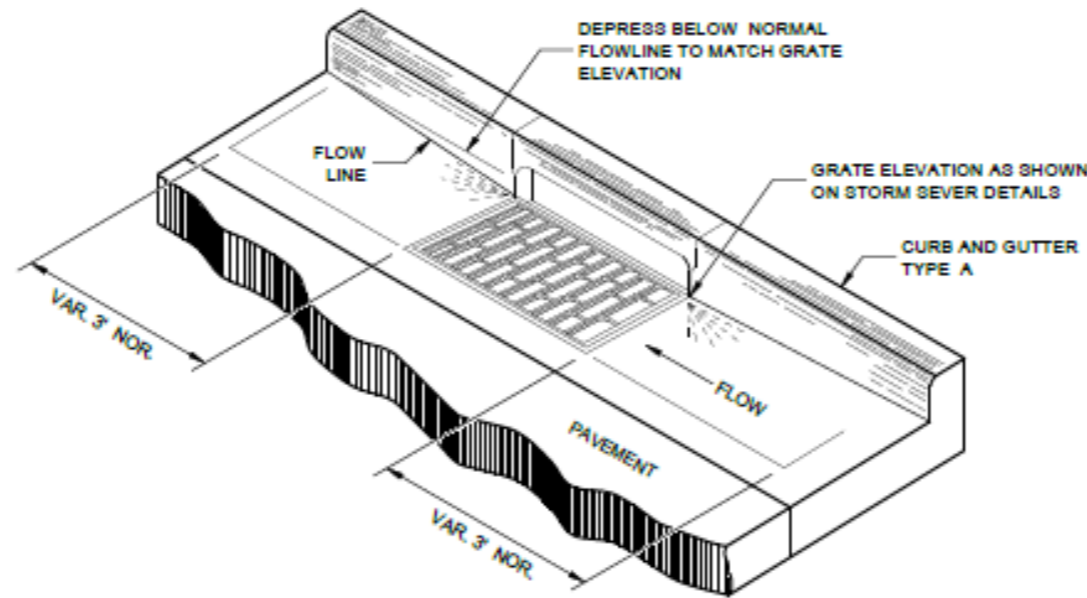
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END SECTION CURB AND GUTTER

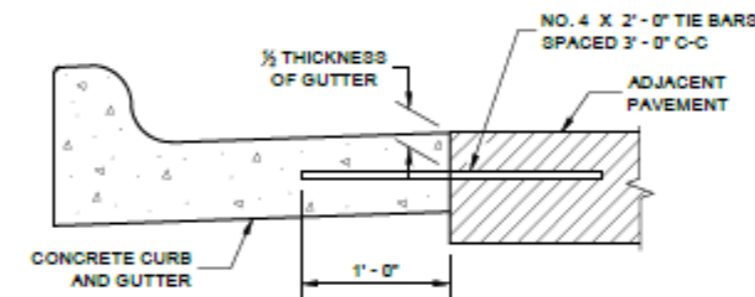


DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

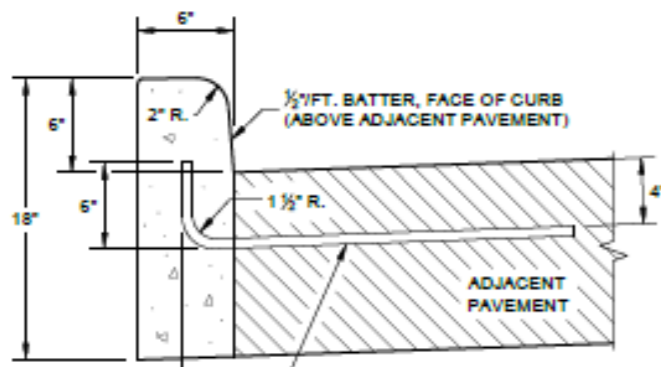
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.26.2 OF THE STANDARD SPECIFICATIONS.
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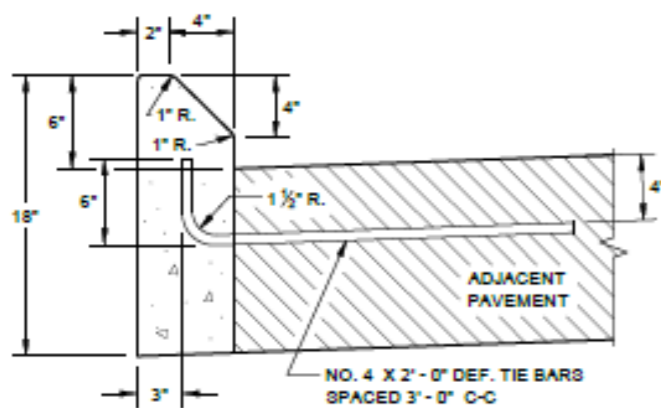
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

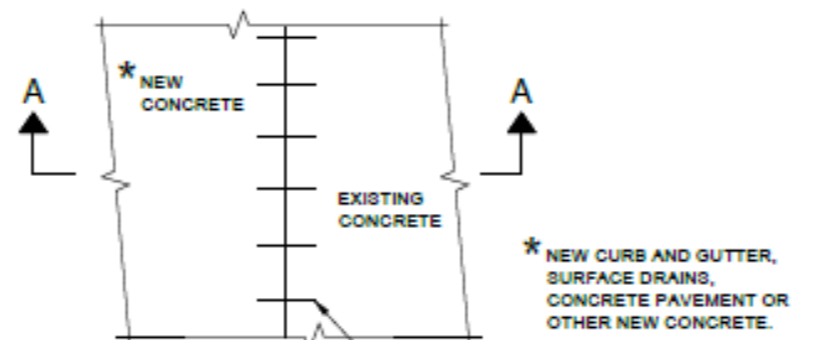


TYPES A ① & D

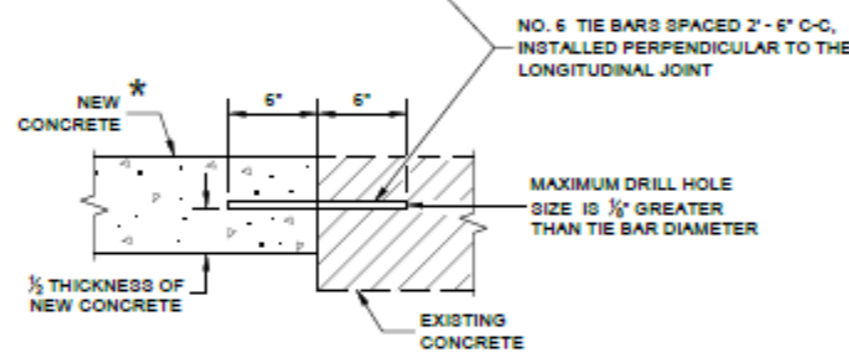


TYPES G ① & J

CONCRETE CURB

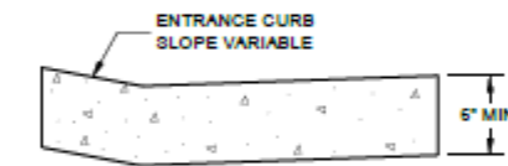


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB ③
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /R/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

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SDD 08D01 - 22b

SDD 08D01 - 22b

ENTRUST COMMS

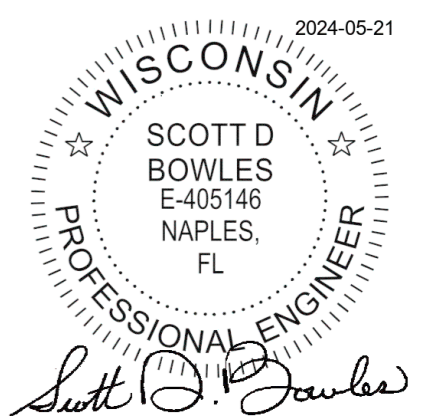
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-05

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



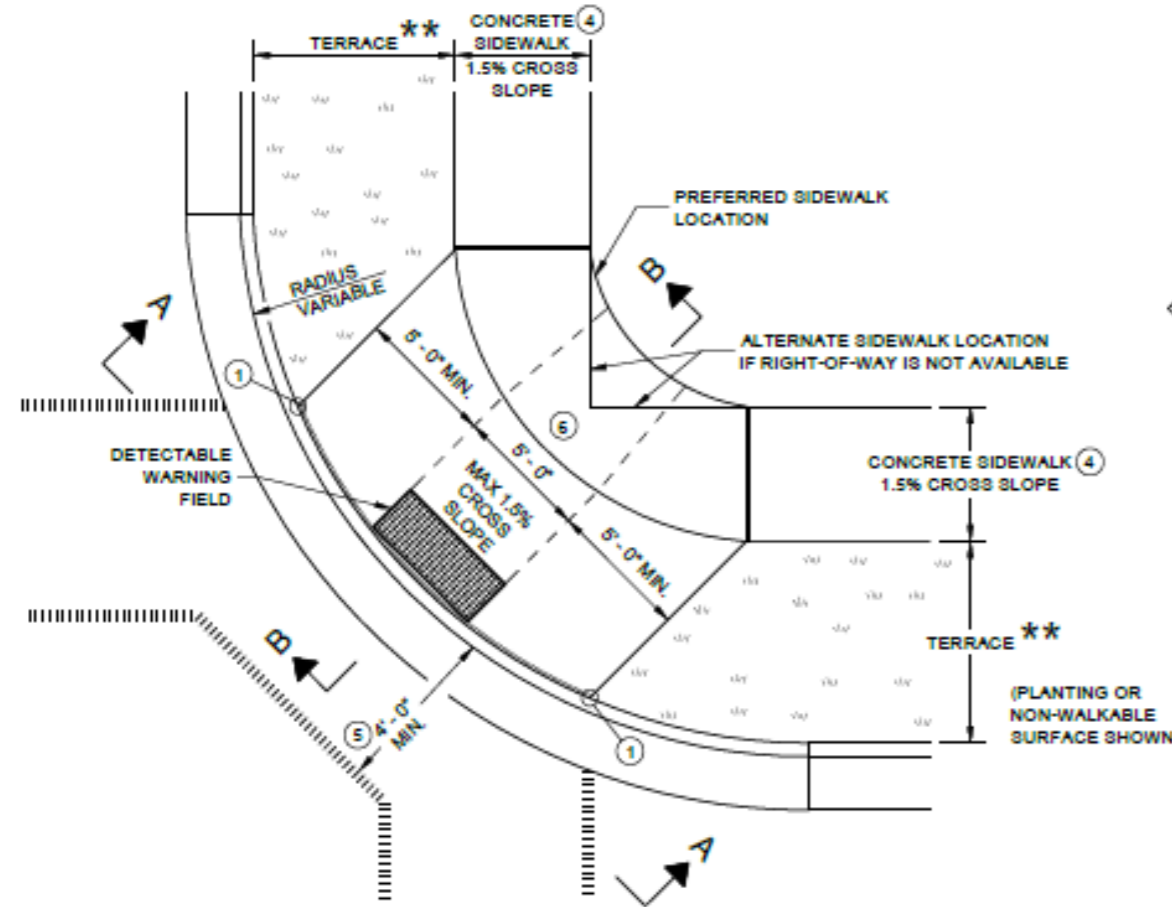
DATE: 5/21/2024
SHEET: 37 OF 69
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf

SOME UTILITIES SHOWN ON THESE PLANS ARE PER GIS DATA. LOCATIONS AND DEPTHS FOR ALL UTILITIES ARE APPROXIMATE. NOT ALL UTILITIES ARE SHOWN ON PLANS

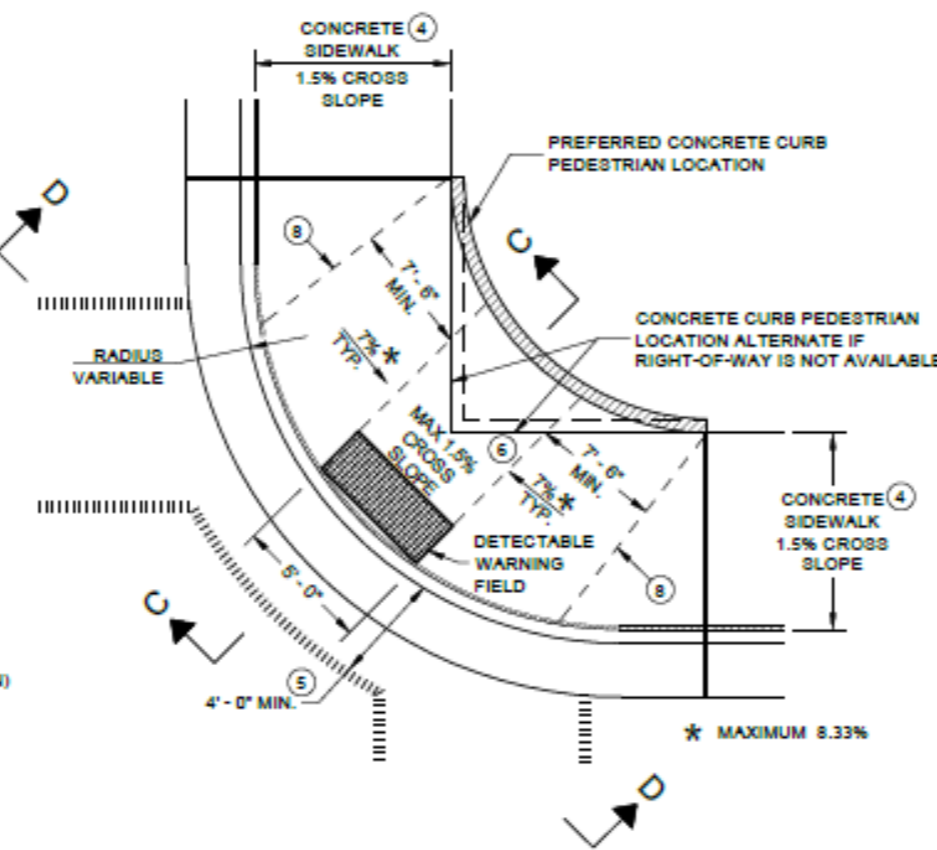
PRIOR TO CONSTRUCTION CALL call811.org (TOLL FREE) AT 1-800-242-8511 OR 811 FOR LOCATION OF UNDERGROUND UTILITIES

NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED





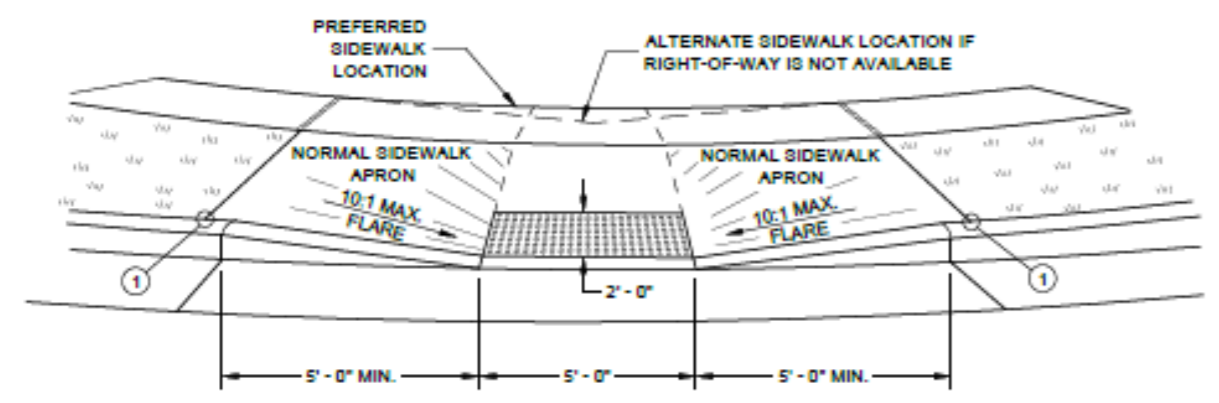
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



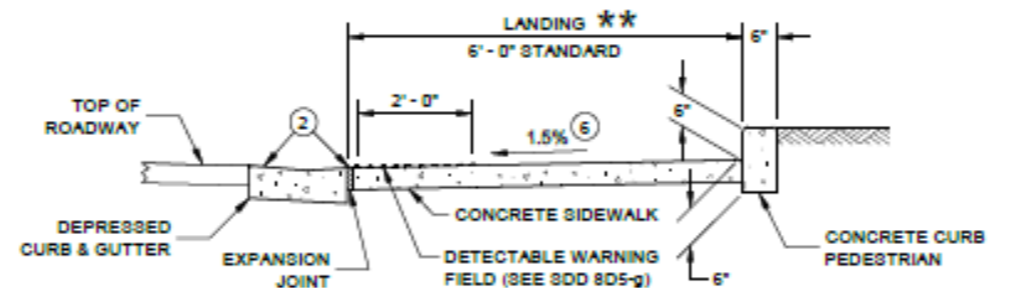
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**

GENERAL NOTES

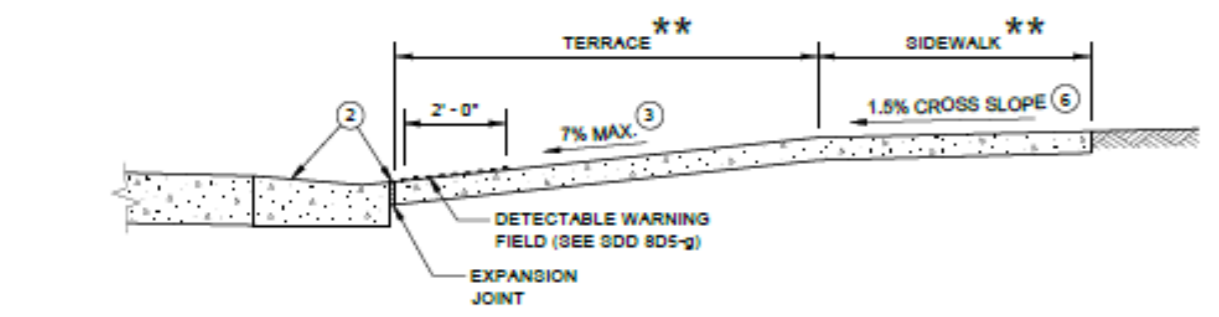
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMP SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- 1 THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 5 PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.



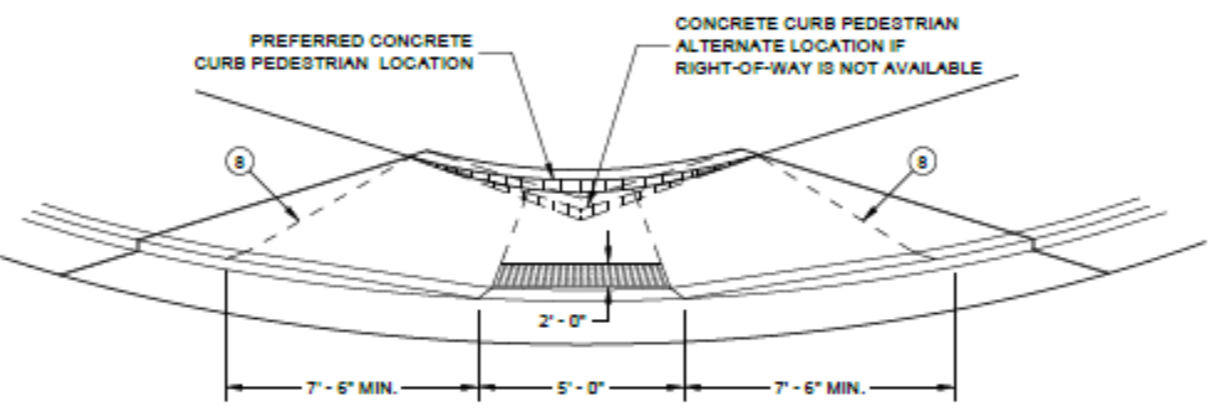
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMP TYPE 1 AND 1-A
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 08D05 - 20a

SDD 08D05 - 20a

ENTRUST COMMS

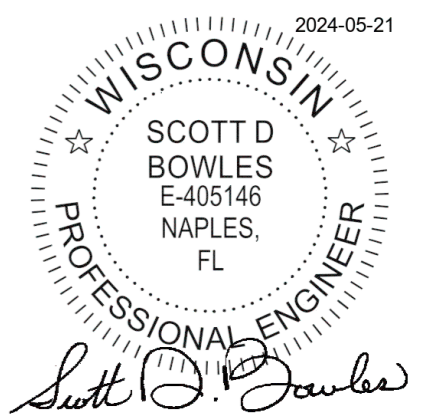
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-06

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

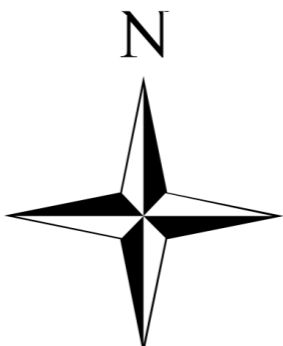
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



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DATE: 5/21/2024
SHEET: 38 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

ENTRUST COMMS

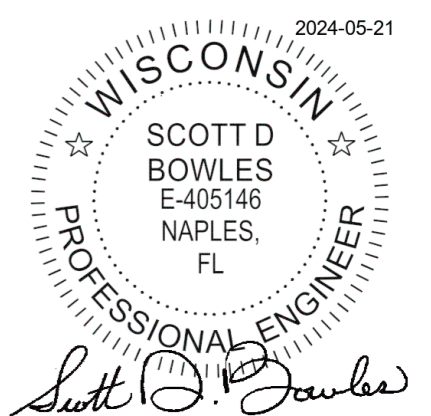
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-07

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



DATE: 5/21/2024
SHEET: 39 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- WHEN DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

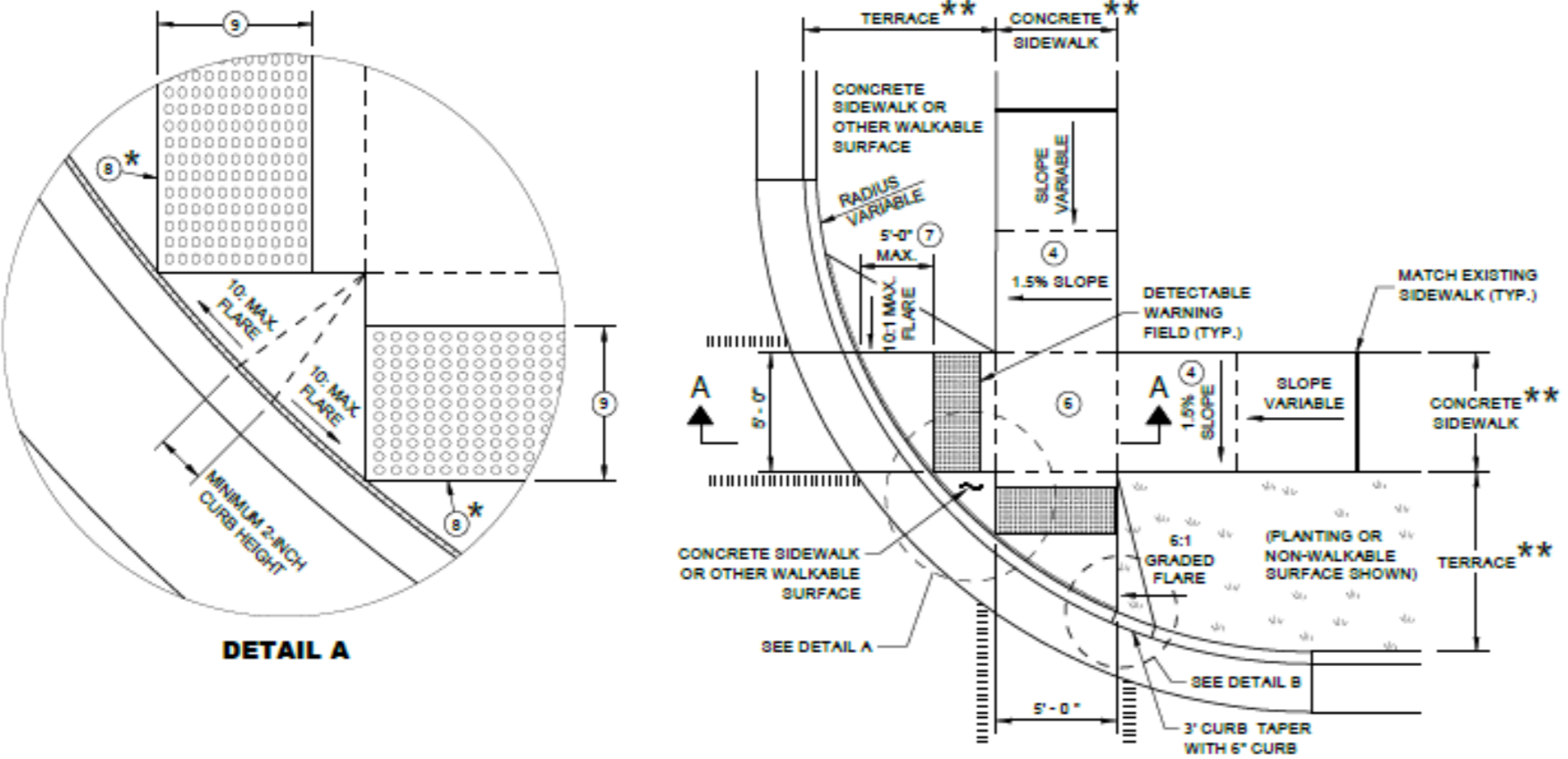
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

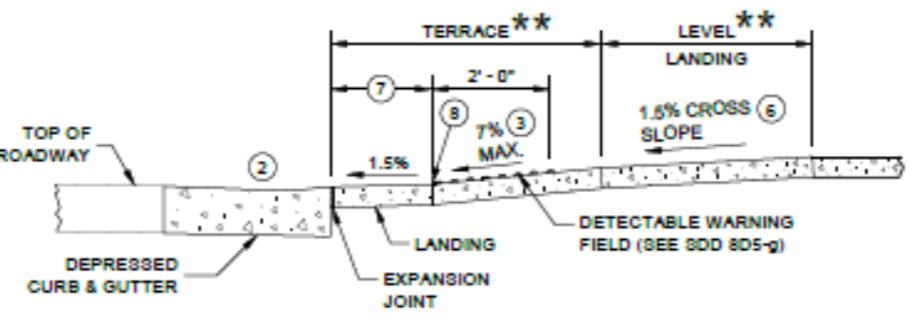
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

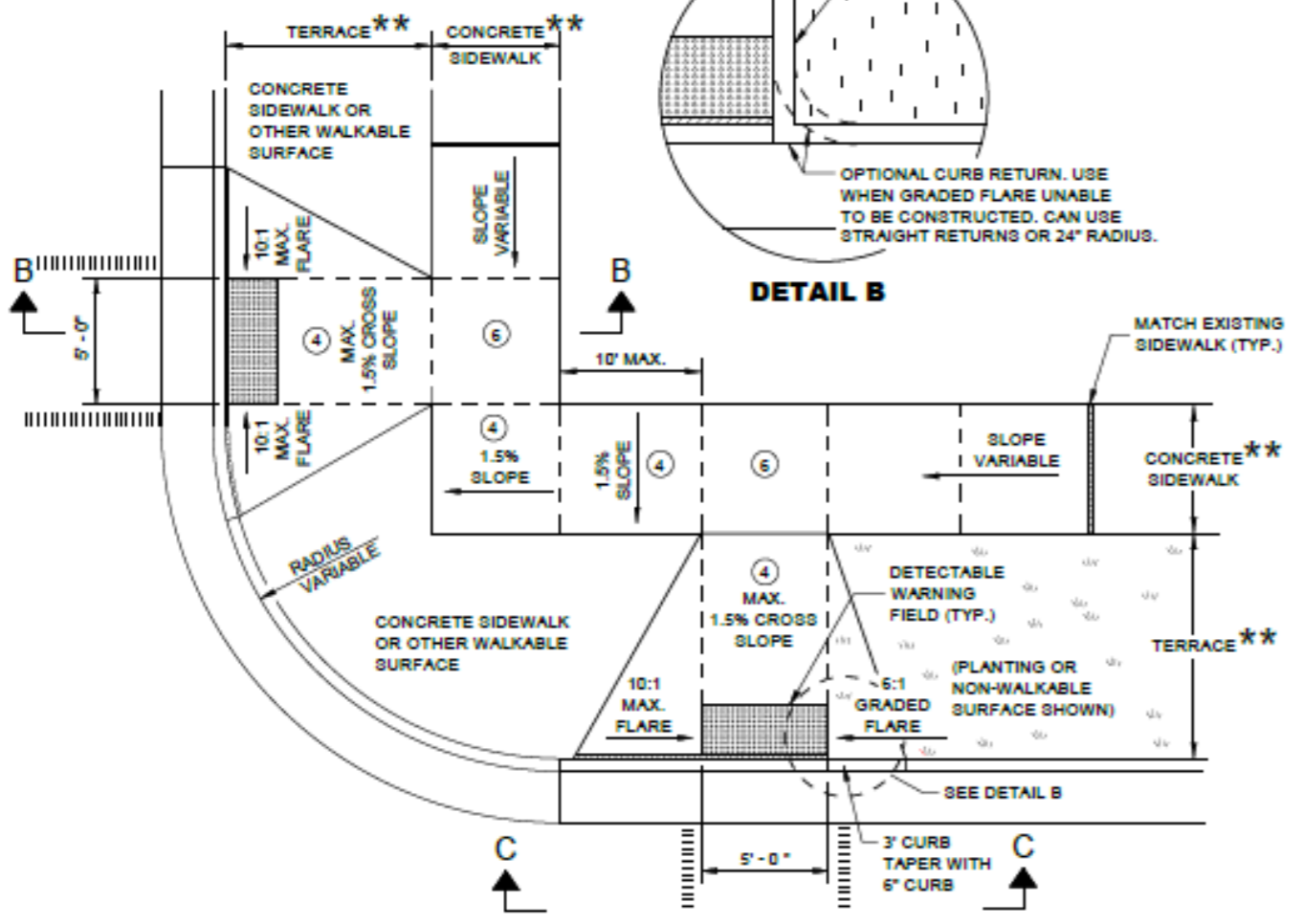
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



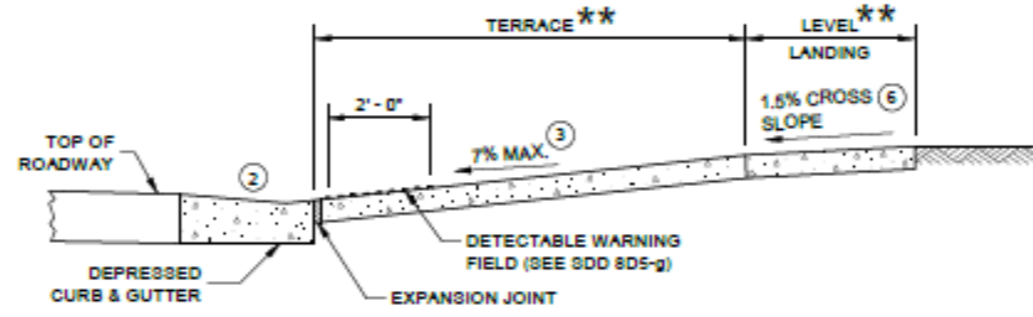
PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



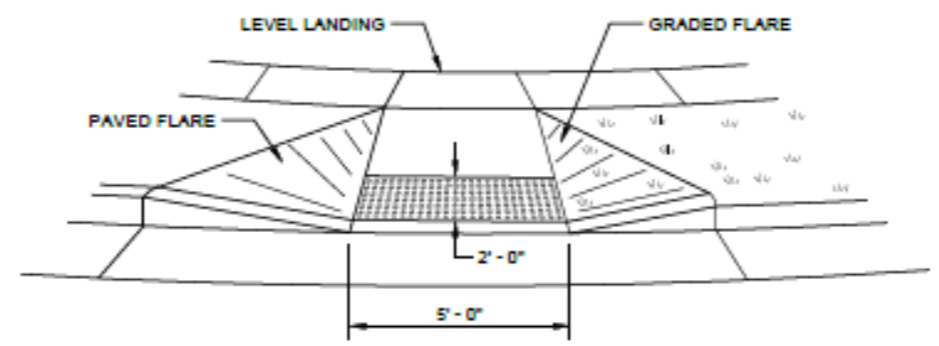
SECTION A - A FOR TYPE 2



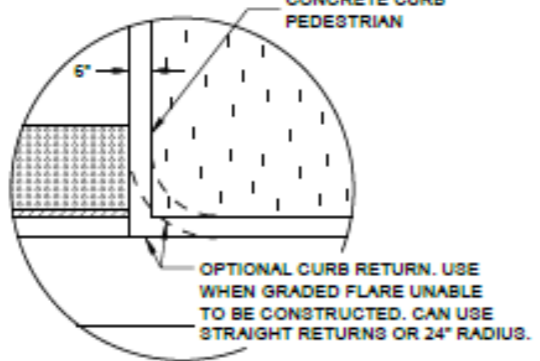
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3



DETAIL B

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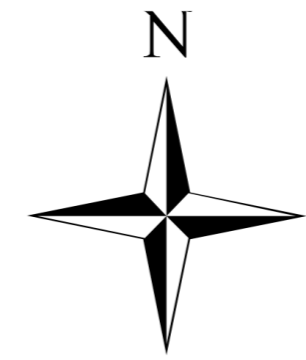
SDD 08D05 - 20b

SDD 08D05 - 20b

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ENTRUST COMMS

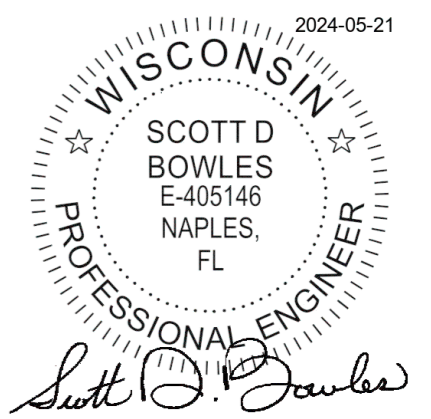
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-08

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



DATE: 5/21/2024
SHEET: 40 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

GENERAL NOTES

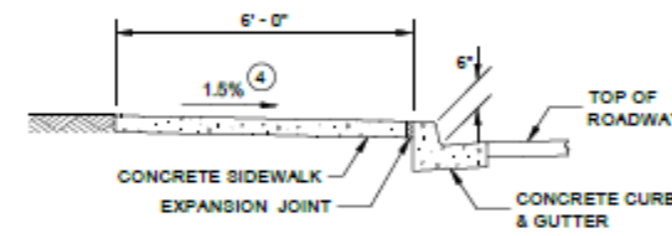
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- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

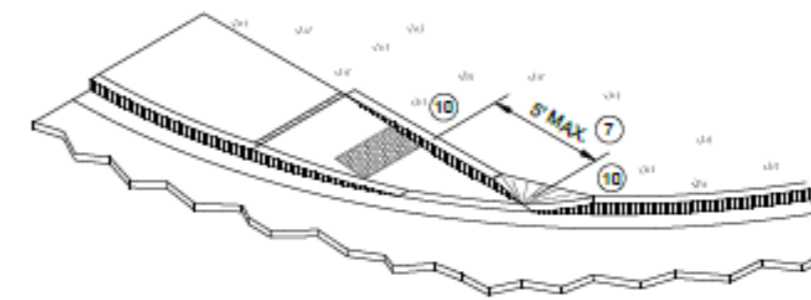
- EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS (AT CURB FACE)	X
10 FEET	4'-7"
15 FEET	6'-5 1/2"

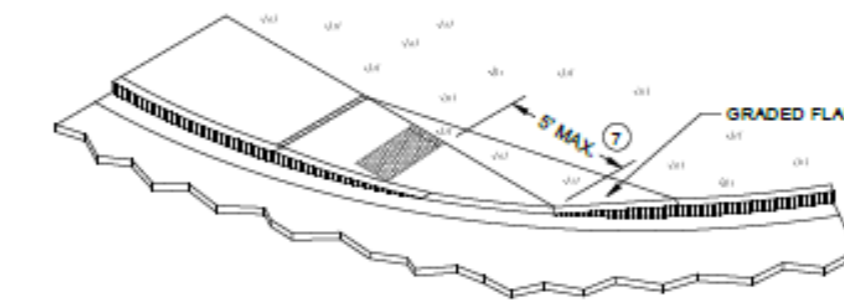
INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A - A FOR TYPE 4A



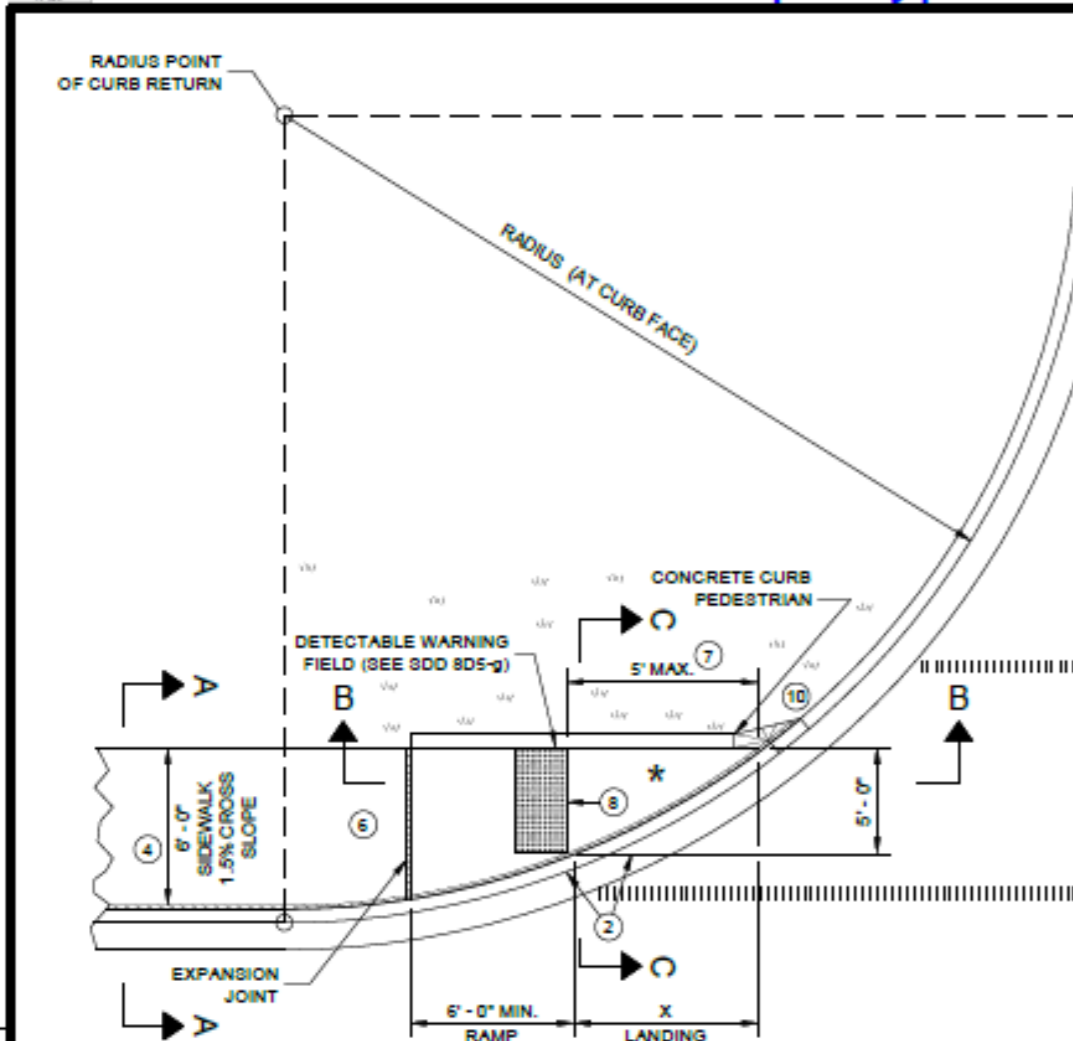
ISOMETRIC VIEW FOR TYPE 4A



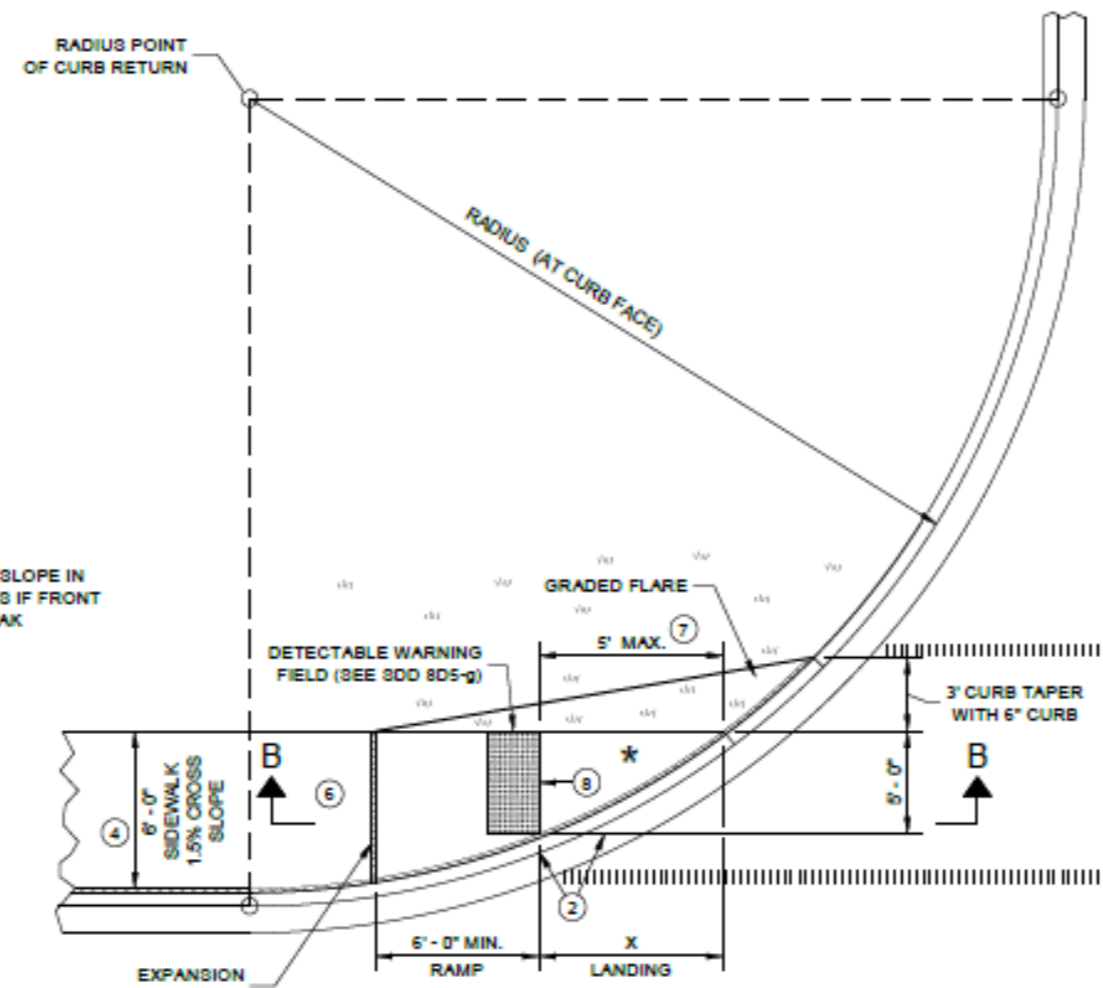
ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMP TYPE 4A AND 4A1

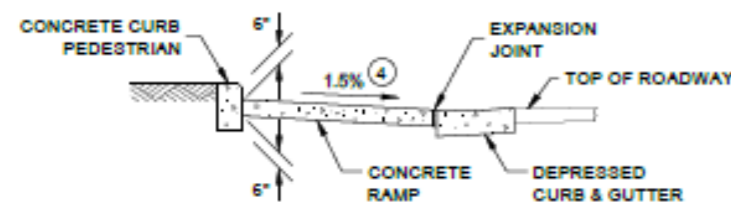
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW
CURB RAMP TYPE 4A

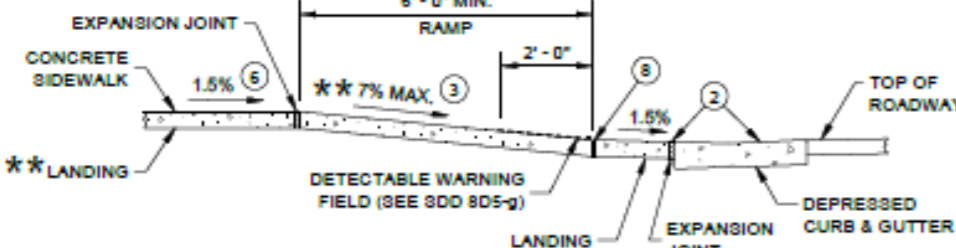


PLAN VIEW
CURB RAMP TYPE 4A1



SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



SECTION B - B FOR
TYPE 4A AND TYPE 4A1

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

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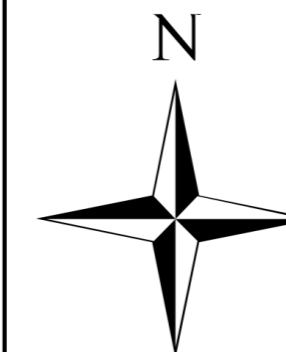
SDD 08D05 - 20c

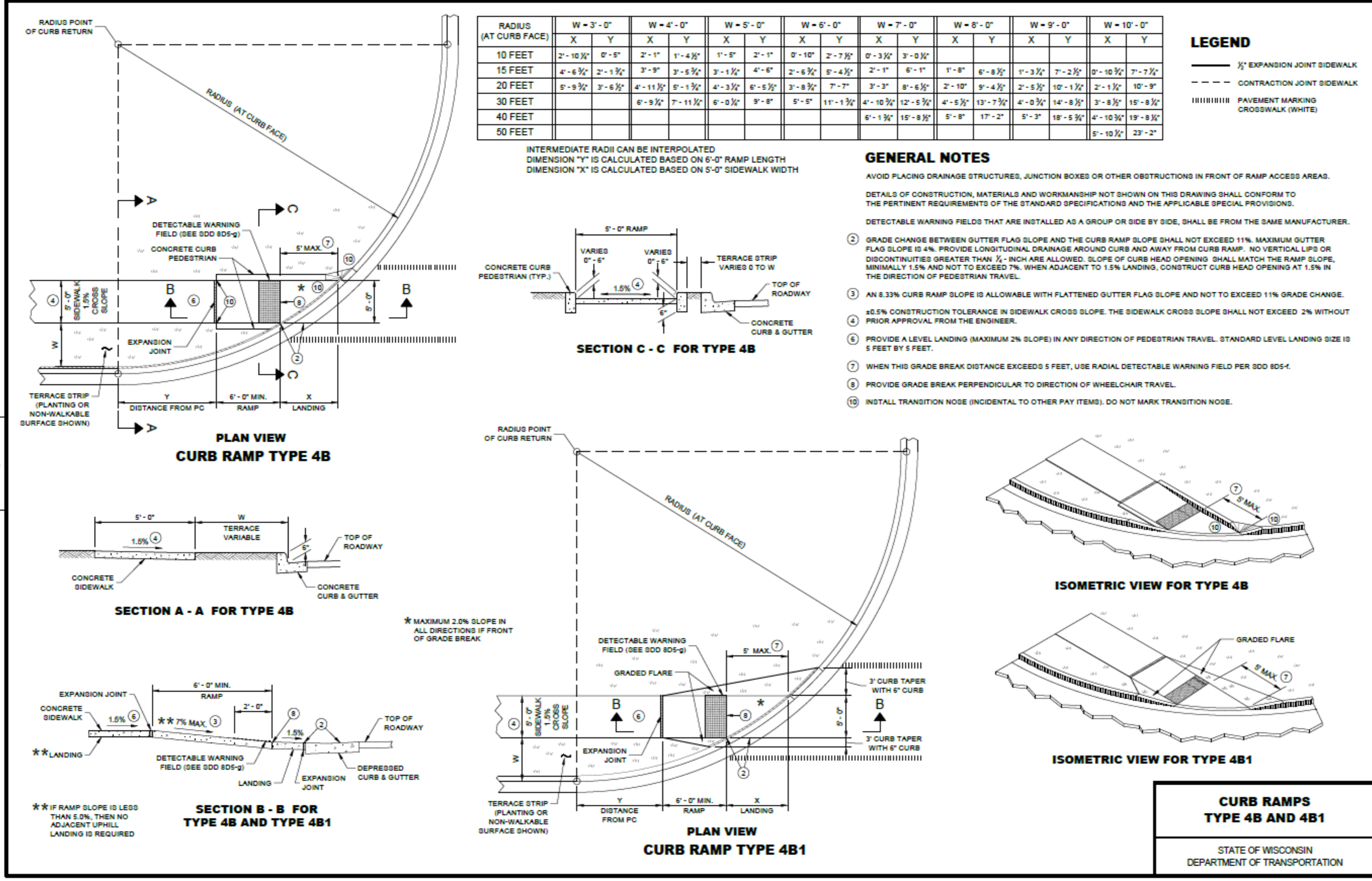
SDD 08D05 - 20c

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SDD 08D05 - 20d

SDD 08D05 - 20d

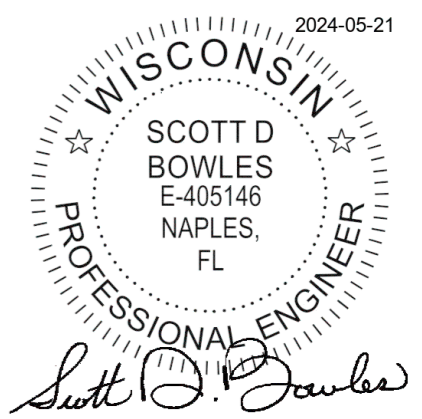
ENTRUST COMMS

CITY OF SUPERIOR, WI
 FIBER OPTIC NETWORK
 TYPICAL PACKAGE
 PRELIMINARY - FOR REVIEW

SHEET-COSR-09

ENGINEER: SM
 DRAWN BY: JW
 CHECKED BY: JW

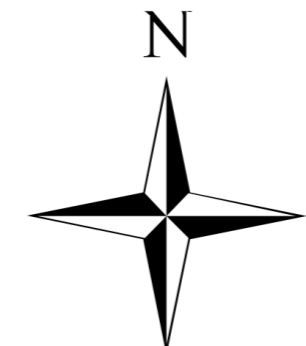
REVISIONS		
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



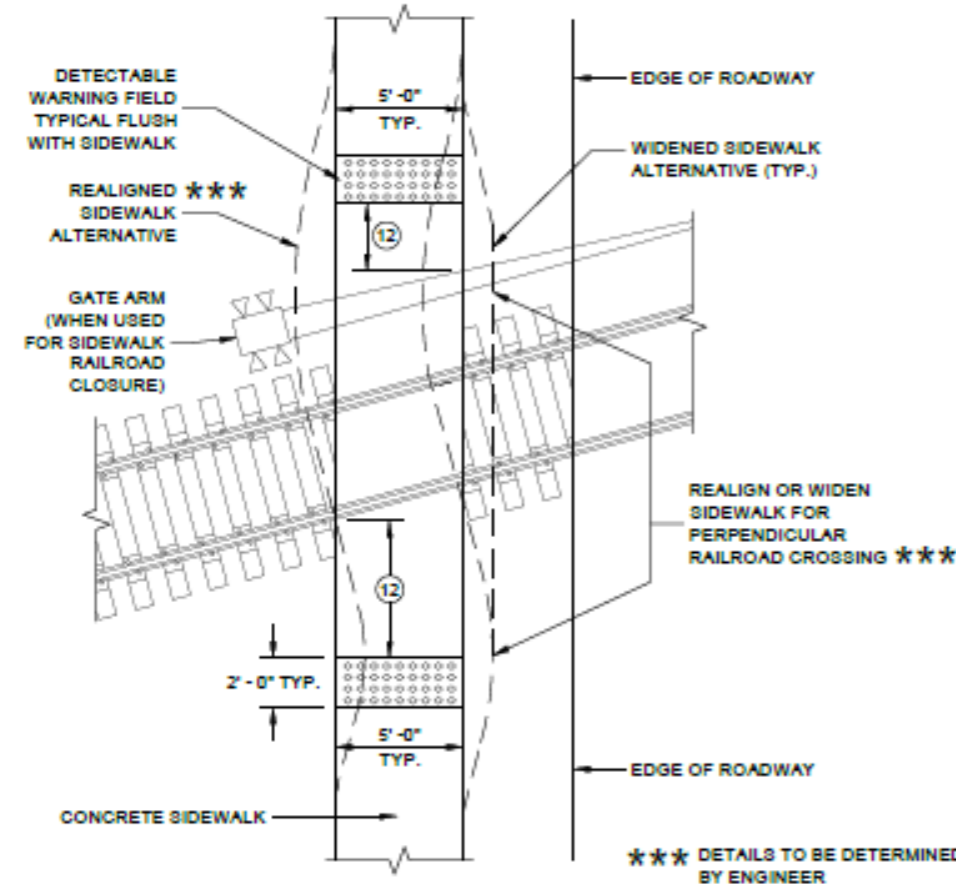
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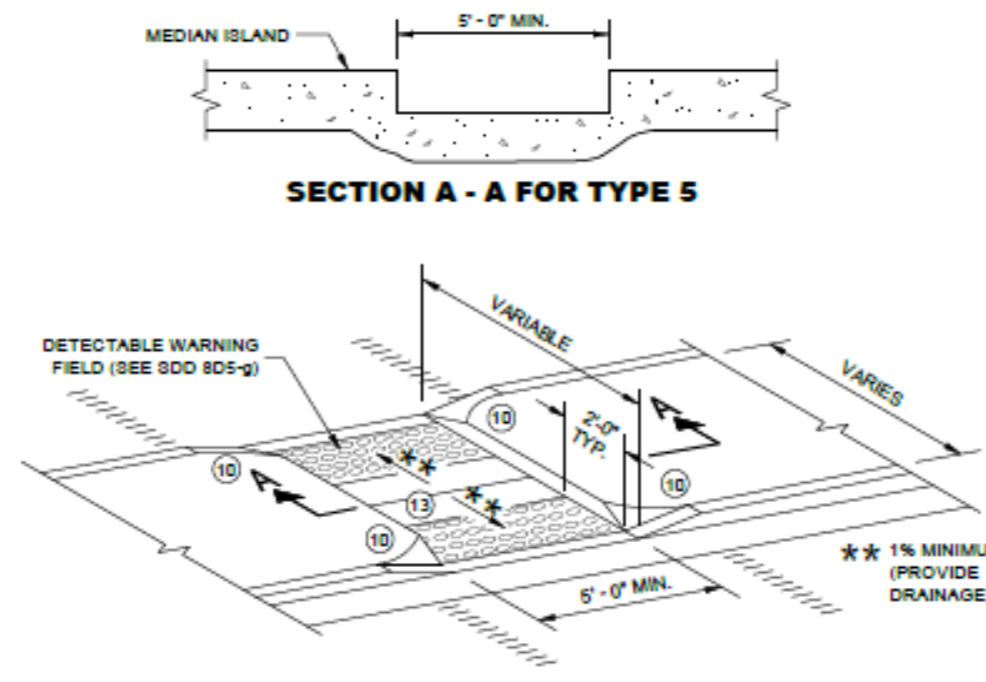
NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED



DATE: 5/21/2024
 SHEET: 41 OF 69
 FILE: City of Superior, WI Construction Standards v4_05202024.pdf



**CURB RAMP TYPE 8
DETECTABLE WARNINGS
AT RAILROAD CROSSING**



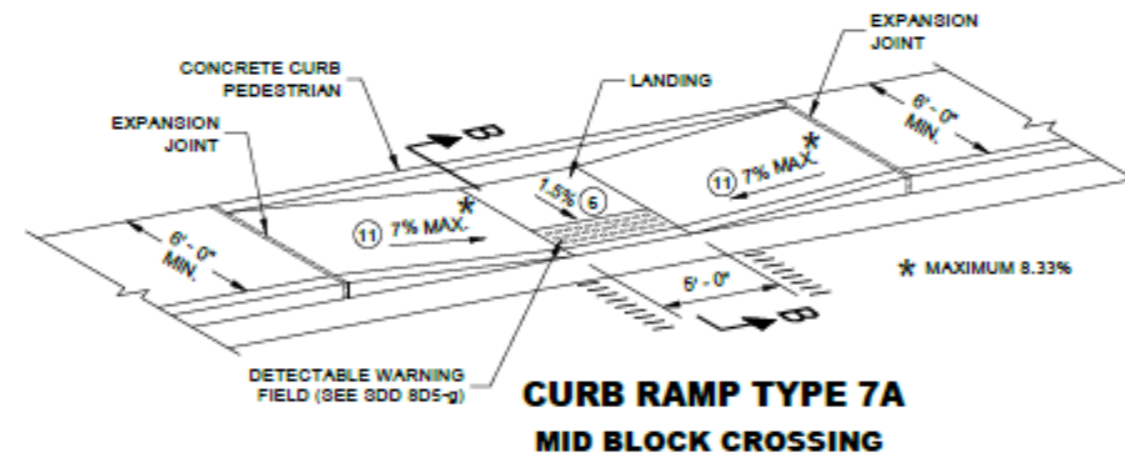
**CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**

GENERAL NOTES

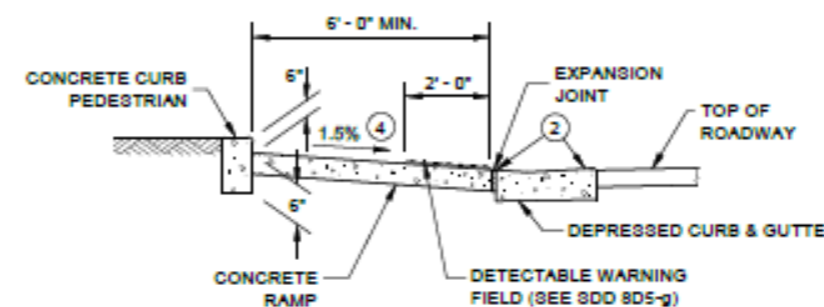
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 5 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 10 INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- 11 SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- 12 THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- 13 DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

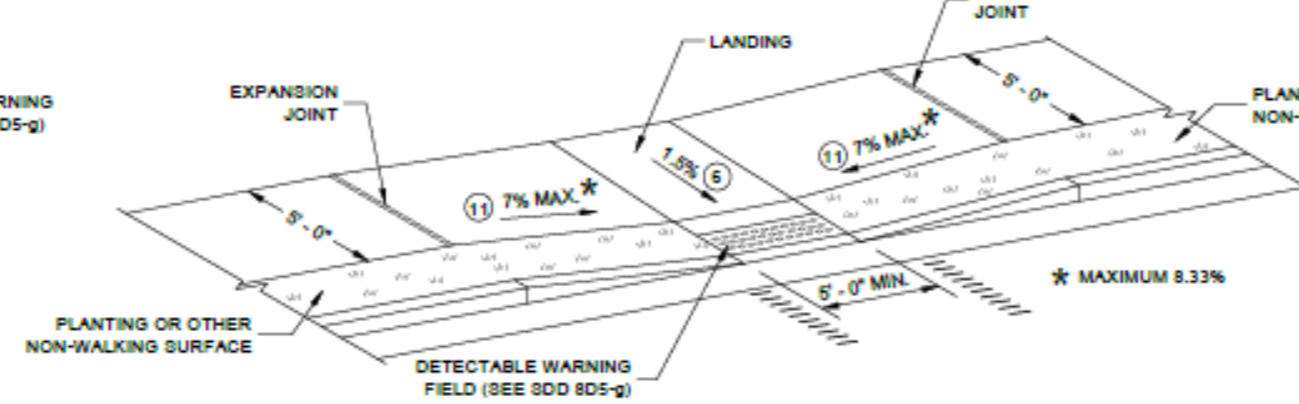
- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



**CURB RAMP TYPE 7A
MID BLOCK CROSSING**



SECTION B - B FOR TYPE 7A



**CURB RAMP TYPE 7B
MID BLOCK CROSSING**

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

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SDD 08D05 - 20e

SDD 08D05 - 20e

ENTRUST COMMS

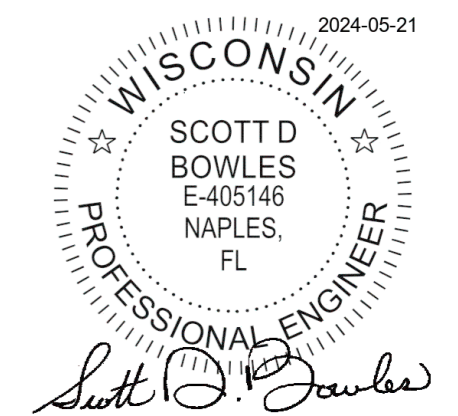
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-10

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

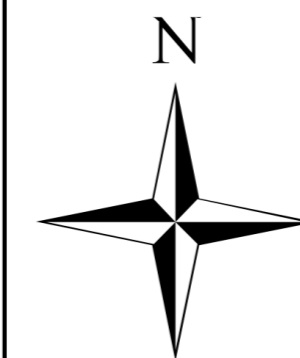
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



SOME UTILITIES SHOWN ON THESE PLANS ARE PER GIS DATA. LOCATIONS AND DEPTHS FOR ALL UTILITIES ARE APPROXIMATE. NOT ALL UTILITIES ARE SHOWN ON PLANS

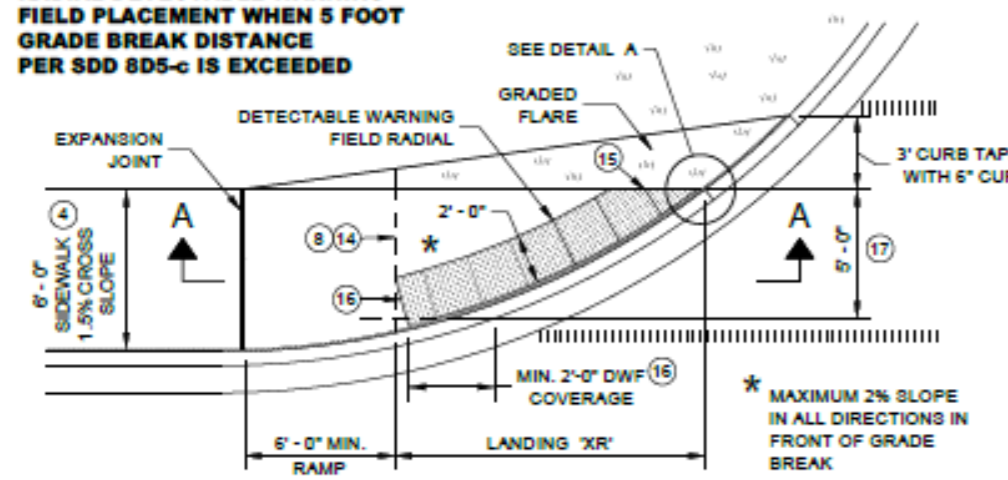
PRIOR TO CONSTRUCTION CALL call811.org (TOLL FREE) AT 1-800-242-8511 OR 811 FOR LOCATION OF UNDERGROUND UTILITIES

NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED

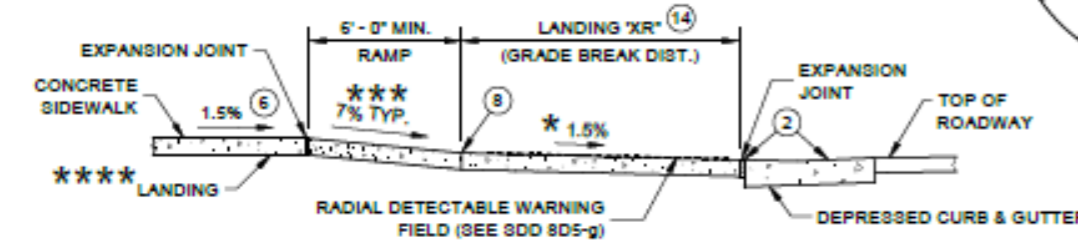


DATE: 5/21/2024
SHEET: 42 OF 69
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED

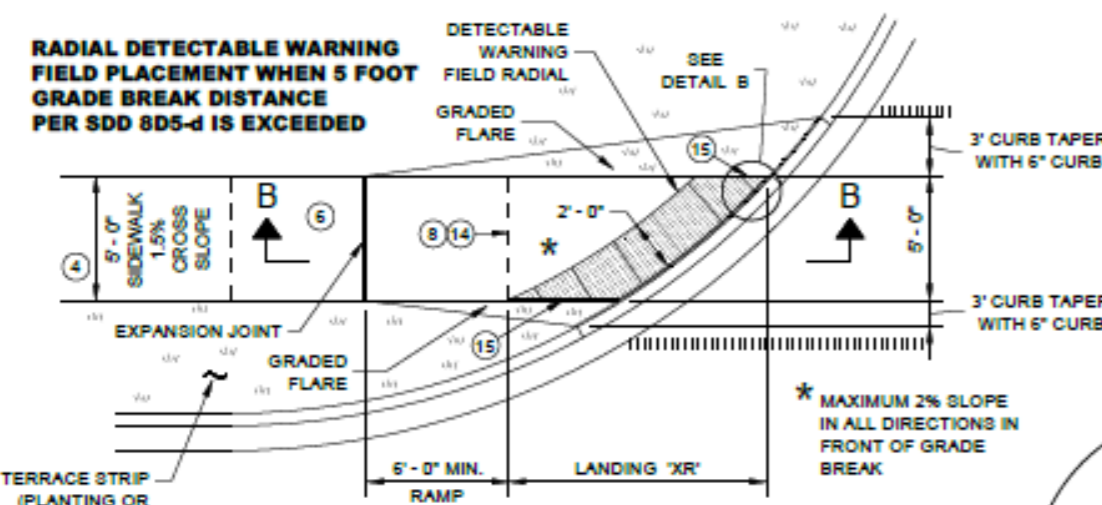


PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

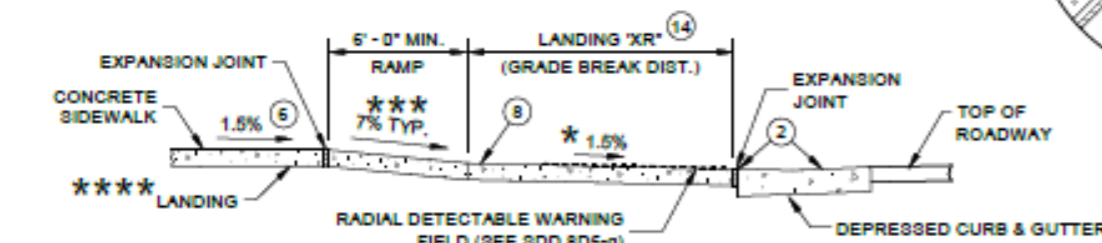


SECTION A - A FOR TYPE 4A1

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



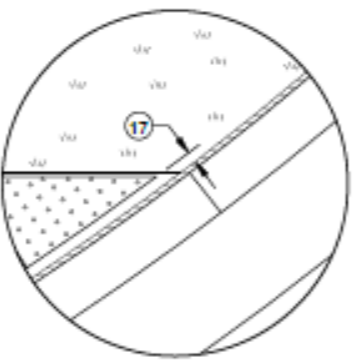
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



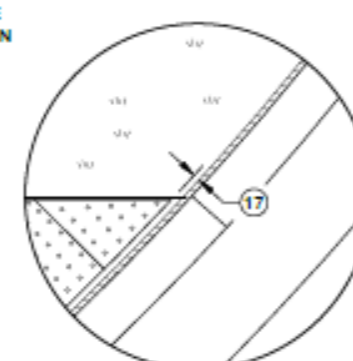
SECTION B - B FOR TYPE 4B1

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



DETAIL A

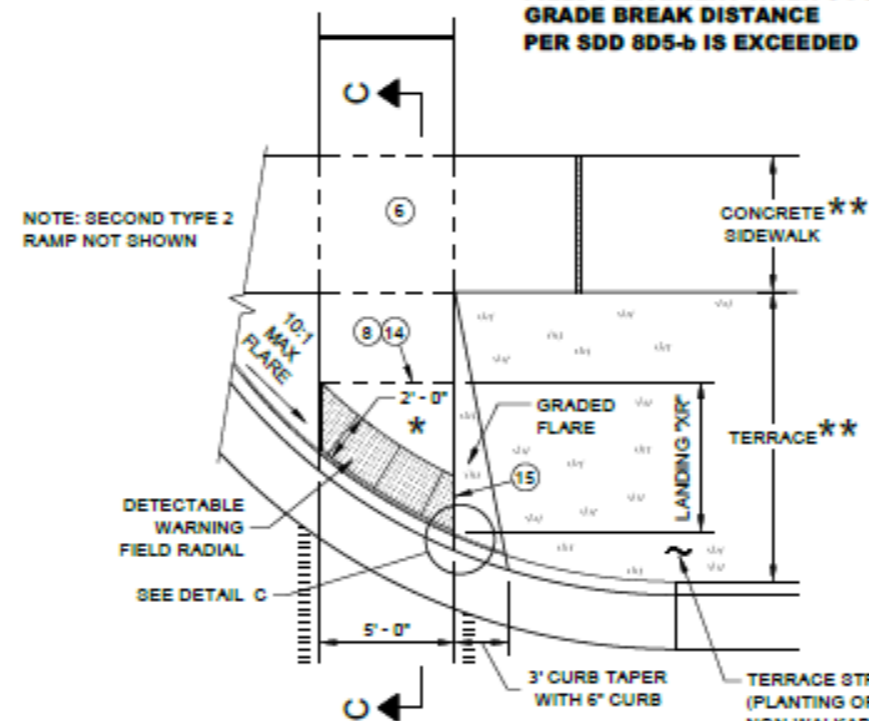


DETAIL B

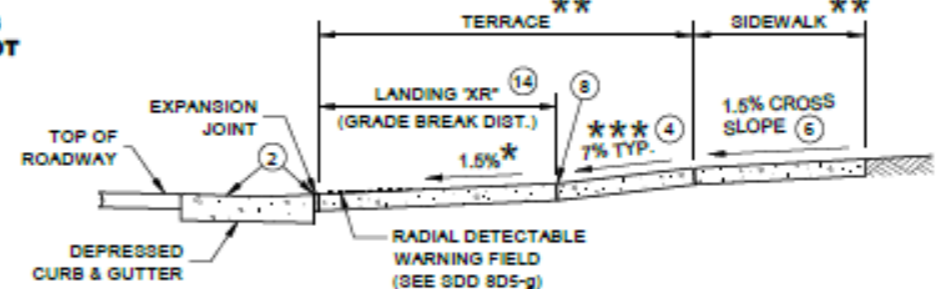
GENERAL NOTES

1. AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
2. DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
3. DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
4. APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMP AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMP. TYPE 4A AND 4B CURB RAMP ARE NOT SHOWN.
5. REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
6. FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
7. DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
8. GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
9. AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
10. ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
11. PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
12. PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
13. CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
14. FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
15. USE 1' X 2' RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2'-0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
16. A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

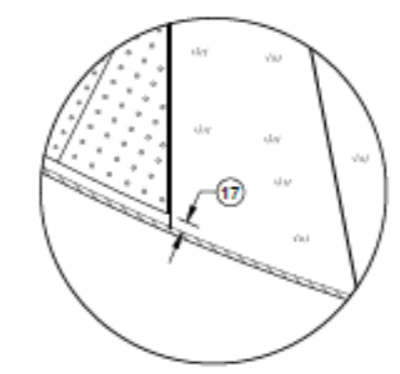
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (ON LINE WITH SIDEWALK)



SECTION C - C FOR TYPE 2



DETAIL C

CURB RAMP RADIAL DETECTABLE WARNING FIELD APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 08D05 - 20f

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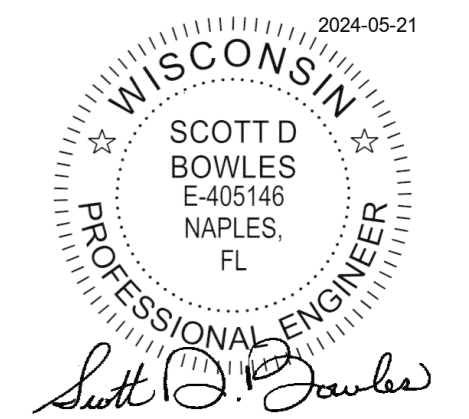
ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-11

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

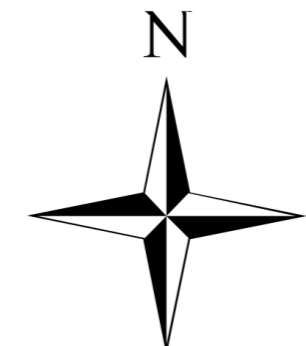
REVISIONS		
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



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NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED

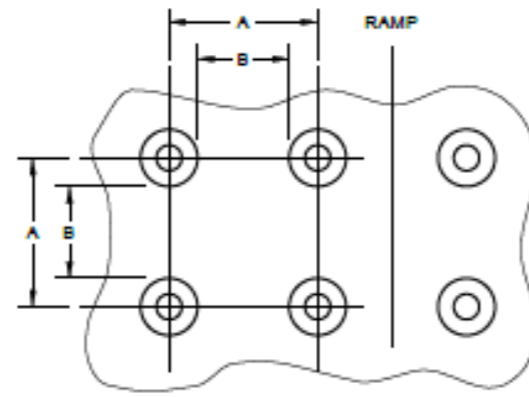


DATE:	5/21/2024
SHEET:	43 OF 69
FILE:	City of Superior, WI Construction Standards v4_05202024.pdf

SDD 08D05-g: Curb Ramps Rectangular and Radial Detectable Warning Plates

	MIN.	MAX.
A	1.5"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 85% OF THE D DIMENSION.

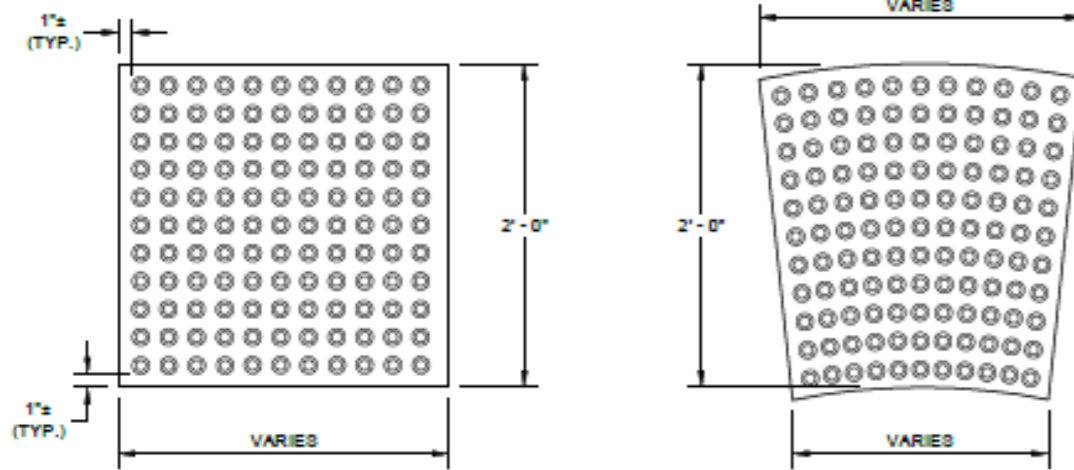


PLAN VIEW



ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**



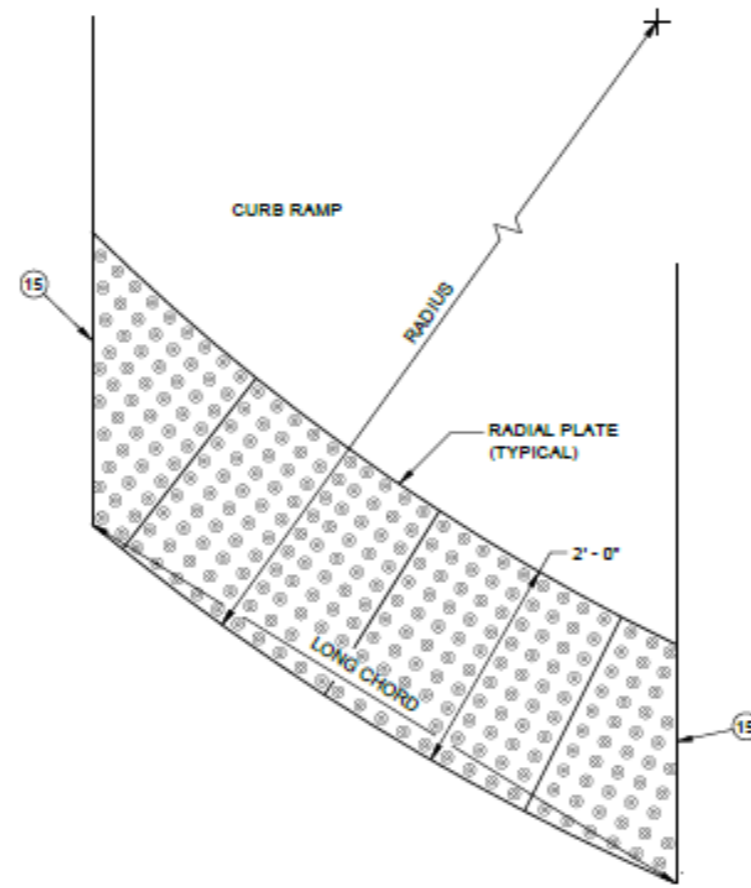
**RECTANGULAR
PLATES**

**RADIAL
PLATES**

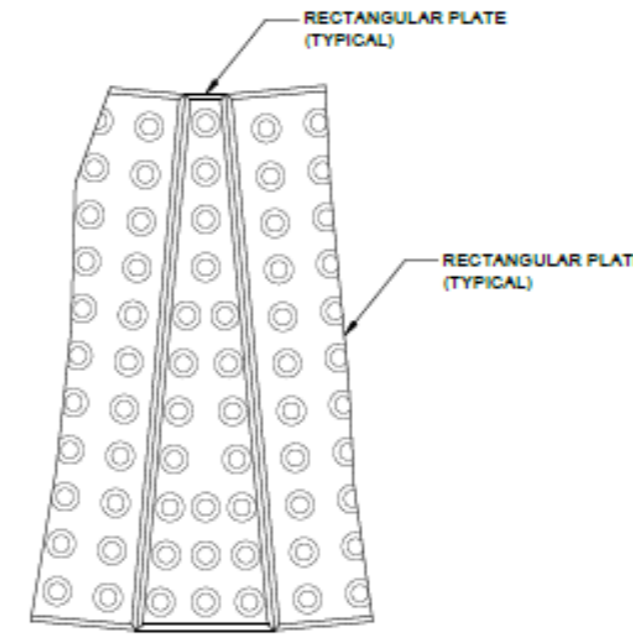
**PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)**

GENERAL NOTES

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.
- FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**



**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

**CURB RAMP
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

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SDD 08D05 - 20g

SDD 08D05 - 20g

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-12

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

2024-05-21

WISCONSIN
SCOTT D BOWLES
E-405146
NAPLES, FL
PROFESSIONAL ENGINEER

Scott D. Bowles

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DATE: 5/21/2024
SHEET: 44 OF 69
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-13

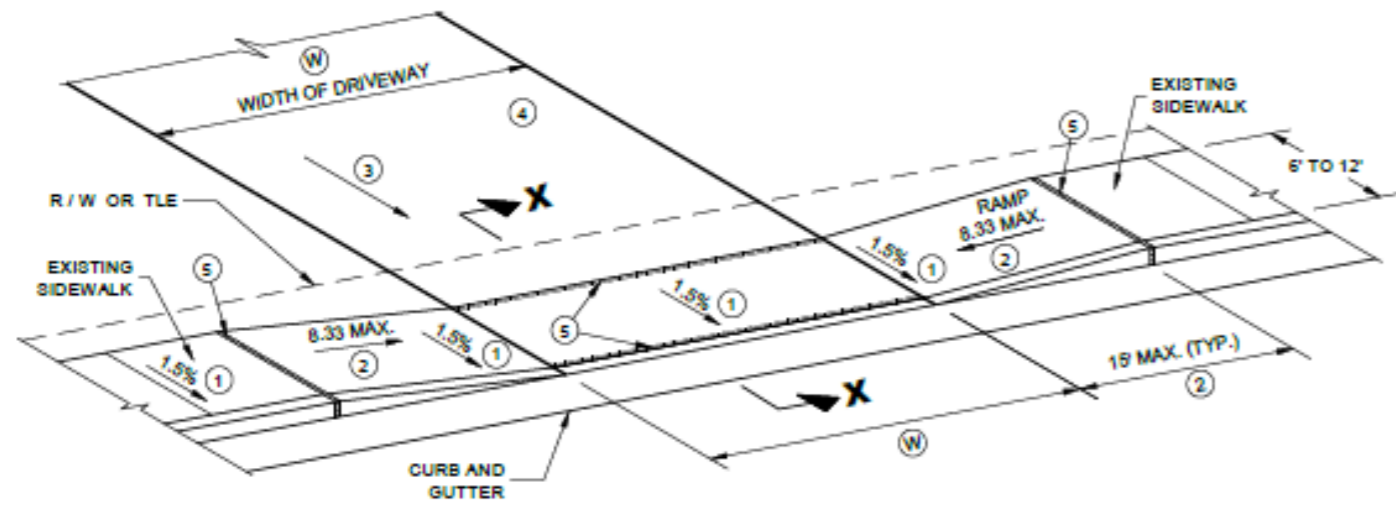
ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

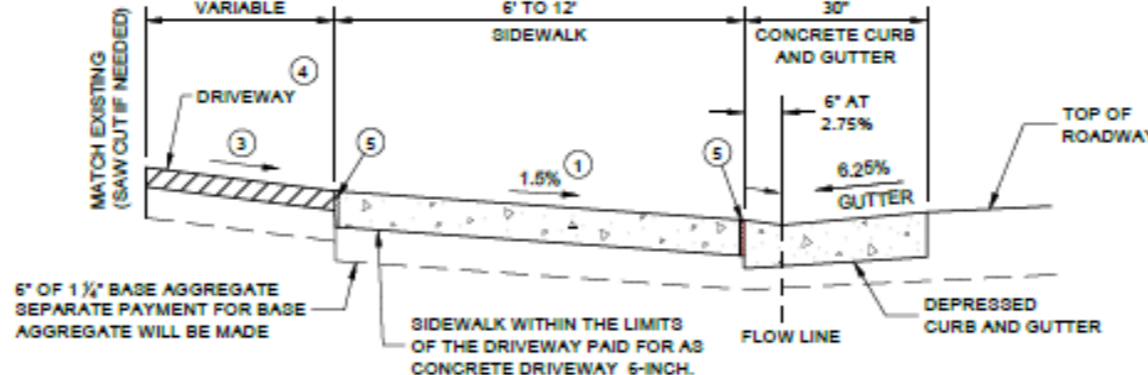
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

2024-05-21
WISCONSIN PROFESSIONAL ENGINEER
SCOTT D BOWLES
E-405146
NAPLES, FL
Scott D. Bowles

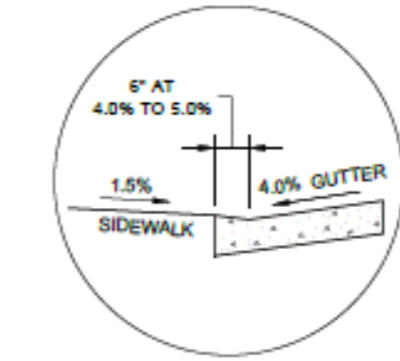
DATE: 5/21/2024
SHEET: 45 OF 69
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf



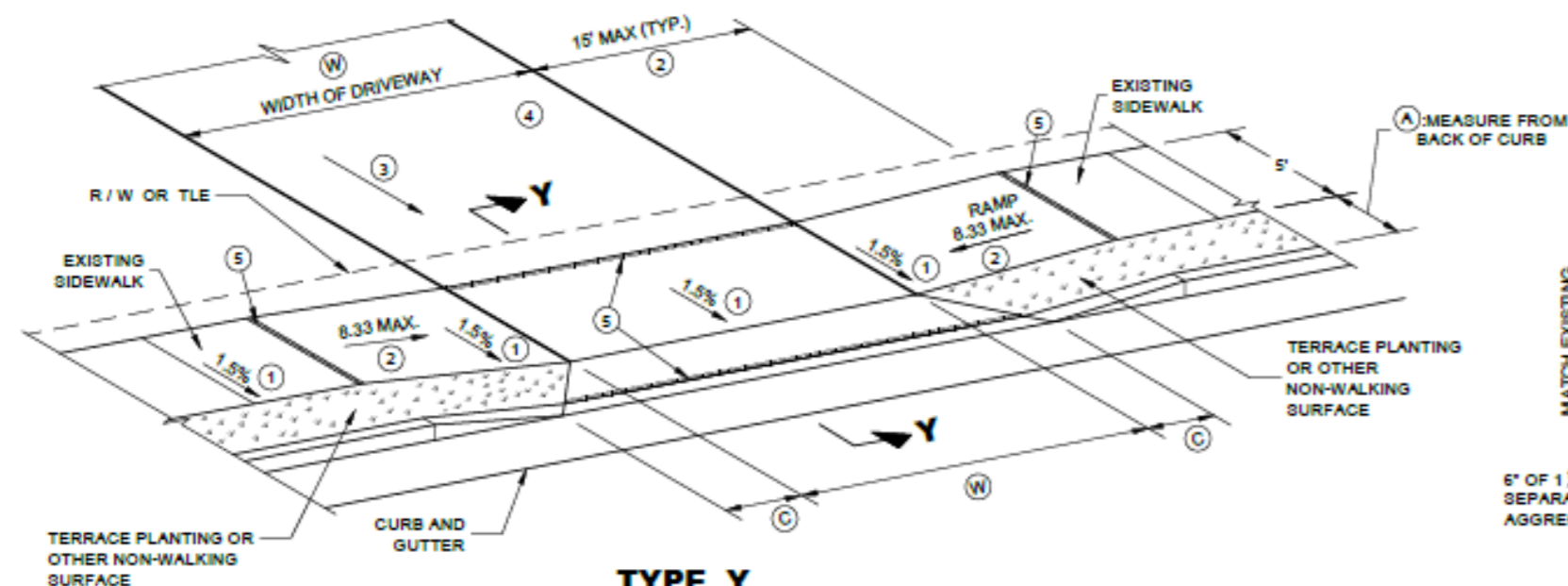
TYPE X
SIDEWALK ABUTS CURB AND GUTTER
TERRACE VARIES 0 TO 3 FEET



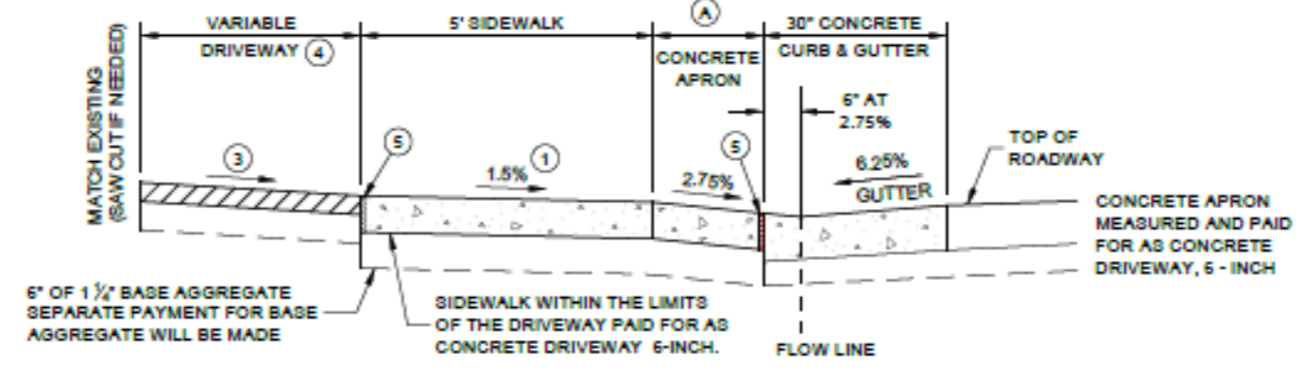
SECTION X - X
4% GUTTER SLOPE



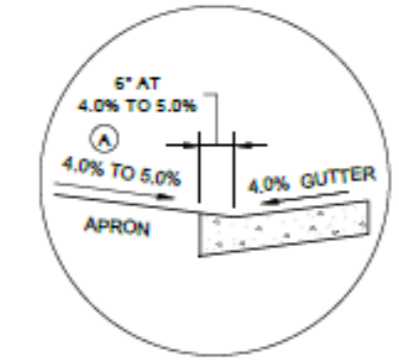
SECTION X - X
4% GUTTER SLOPE



TYPE Y
SIDEWALK WITH NARROWER TERRACE
TERRACE VARIES 4 TO 6 FEET



SECTION Y - Y
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)



SECTION Y - Y
4% GUTTER SLOPE

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)

TABLE Y

(A) FEET	(C) FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'

GENERAL NOTES

- PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.
- (W) IS SHOWN ON PLAN AND PROFILE SHEETS.
- OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.
- CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

- DRIVEWAY SLOPES: DESIRABLE MAXIMUM 10.5% UP AWAY FROM SIDEWALK (SAG) 8.5% DOWN AWAY FROM SIDEWALK (CREST) ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- DRIVEWAY TYPES
 - 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 - 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 - 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
- 1/2" EXPANSION JOINT FILLER

DRIVEWAY AND SIDEWALK RAMPS TYPES X AND Y

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED February 2022 /S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER

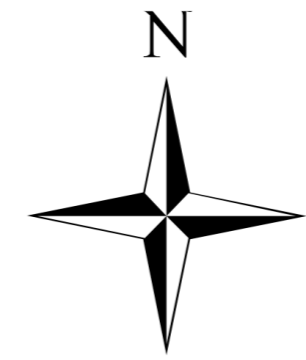
SDD 08D18 - 03

SDD 08D18 - 03

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ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-14

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

2024-05-21
WISCONSIN
SCOTT D BOWLES
E-405146
NAPLES, FL
PROFESSIONAL ENGINEER
Scott D. Bowles

DATE: 5/21/2024
SHEET: 46 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

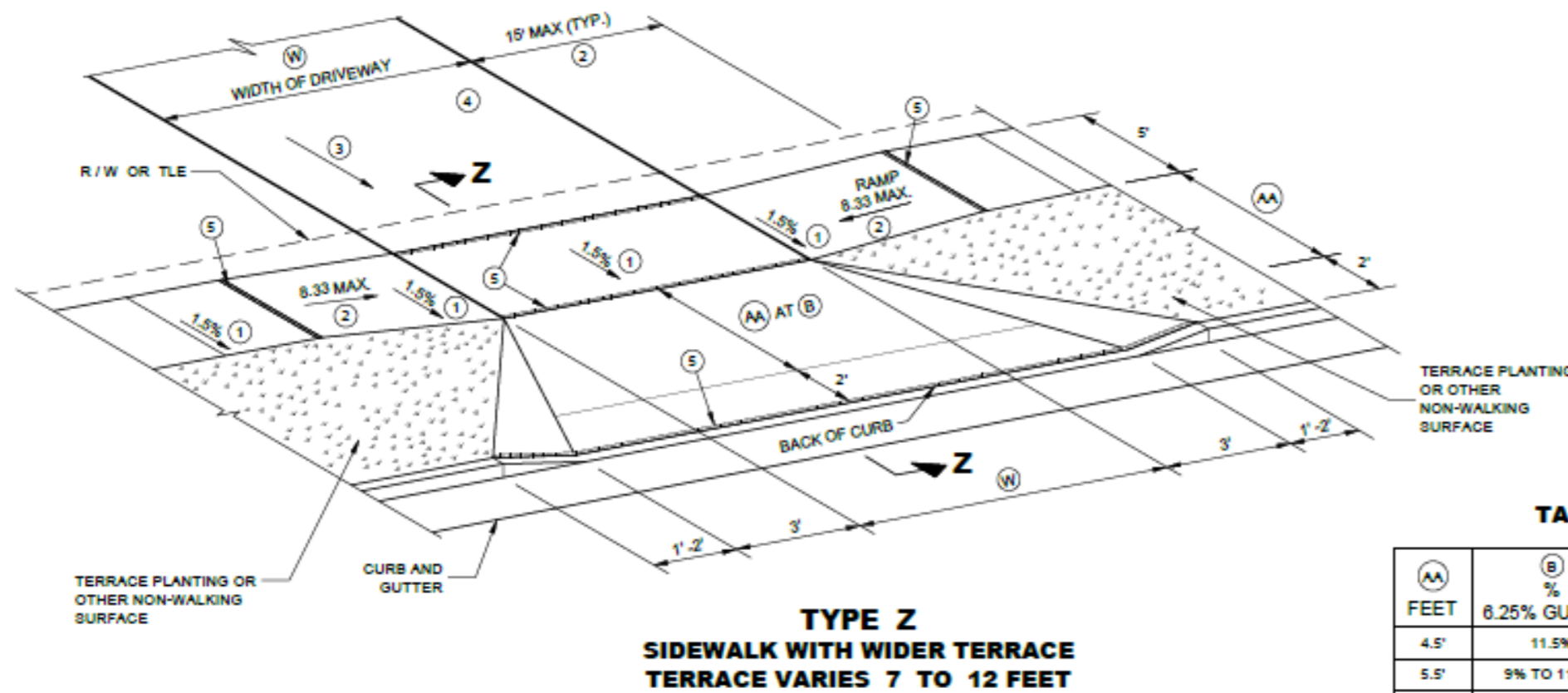
OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
- DRIVEWAY SLOPES: DESIRABLE MAXIMUM
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- DRIVEWAY TYPES
- 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
- 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
- 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
- ½" EXPANSION JOINT FILLER.

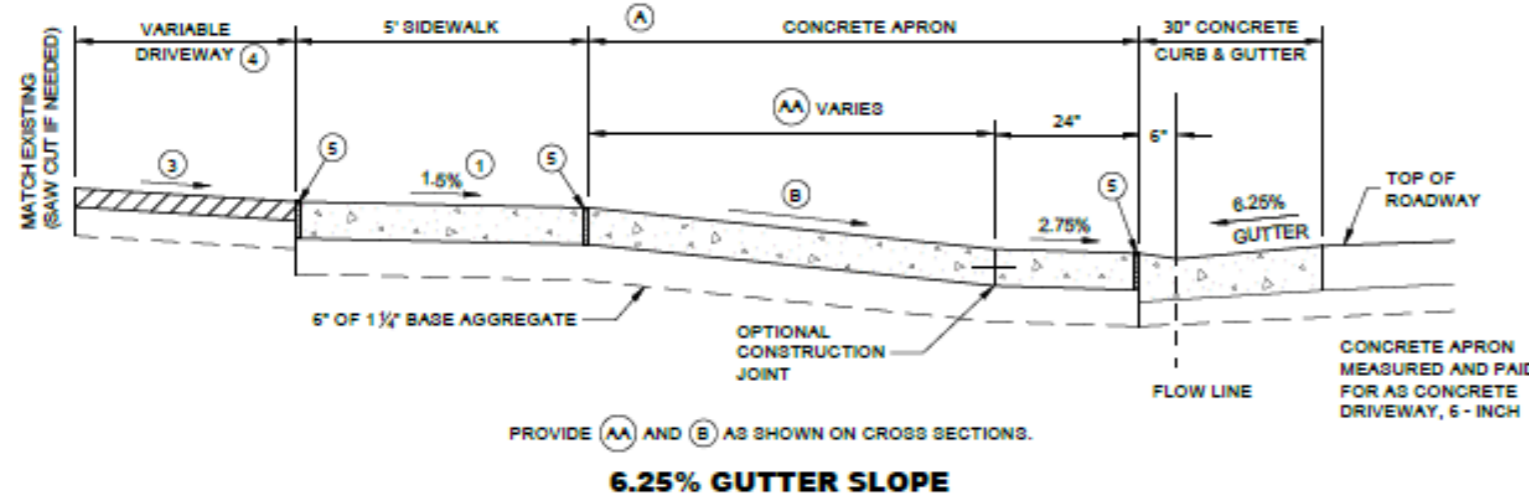
TABLE Z

(AA) FEET	(B) 6.25% GUTTER	(B) 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

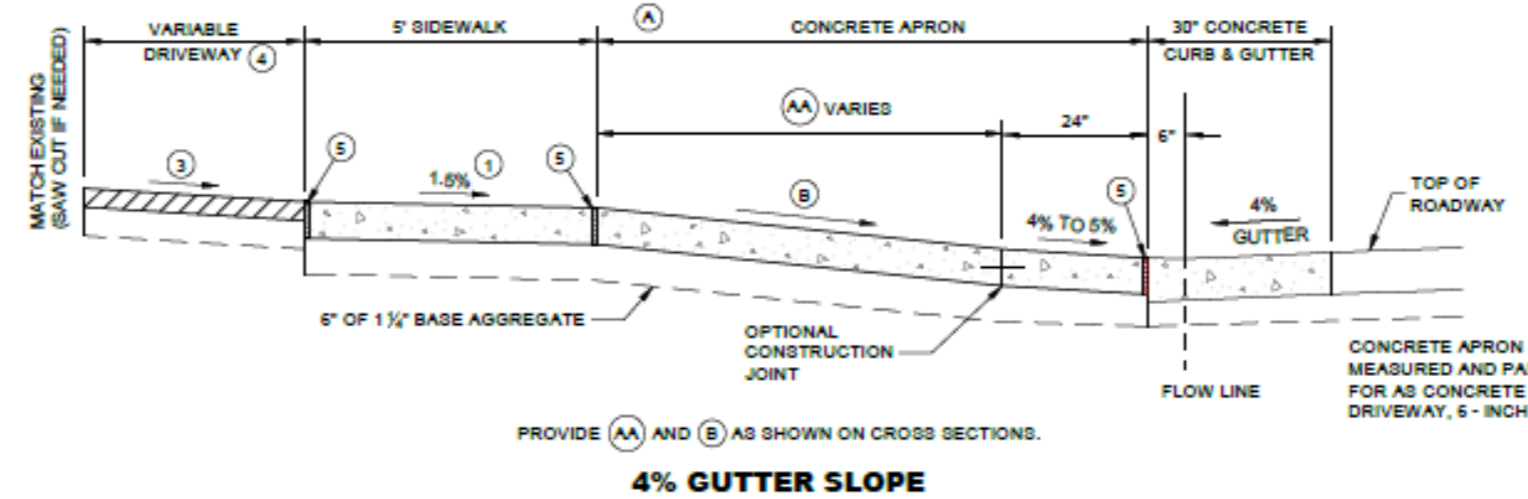
(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET



6.25% GUTTER SLOPE



4% GUTTER SLOPE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.
SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE.

SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)

DRIVEWAY AND
SIDEWALK RAMPS
TYPE Z

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

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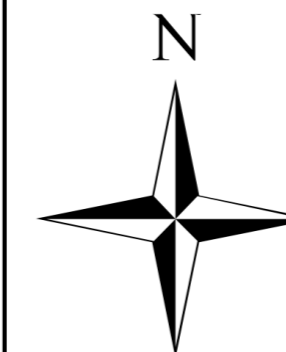
SDD 08D19 - 03

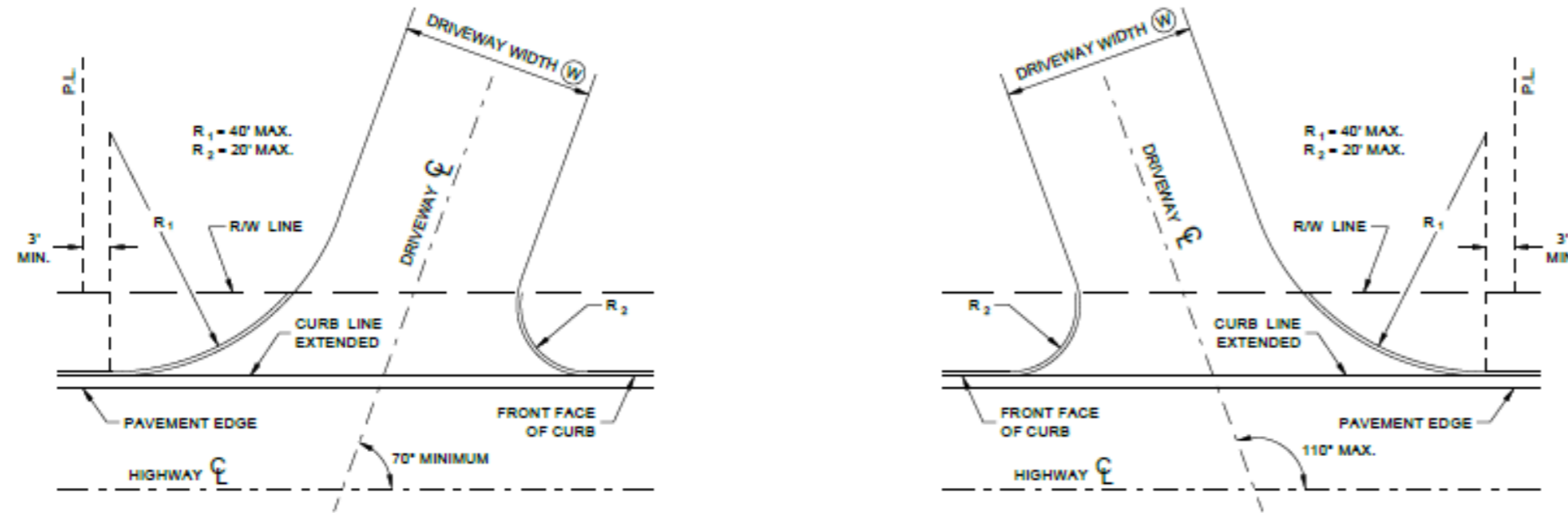
SDD 08D19 - 03

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NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED





GENERAL NOTES

A MAXIMUM RADIUS OF 10 FEET SHALL BE USED FOR NON-COMMERCIAL PRIVATE ENTRANCES. RADII FOR COMMERCIAL DRIVEWAYS SHALL BE DETERMINED BY THE ENGINEER BASED ON TRAFFIC AND DRIVEWAY PERMIT RESTRICTIONS.

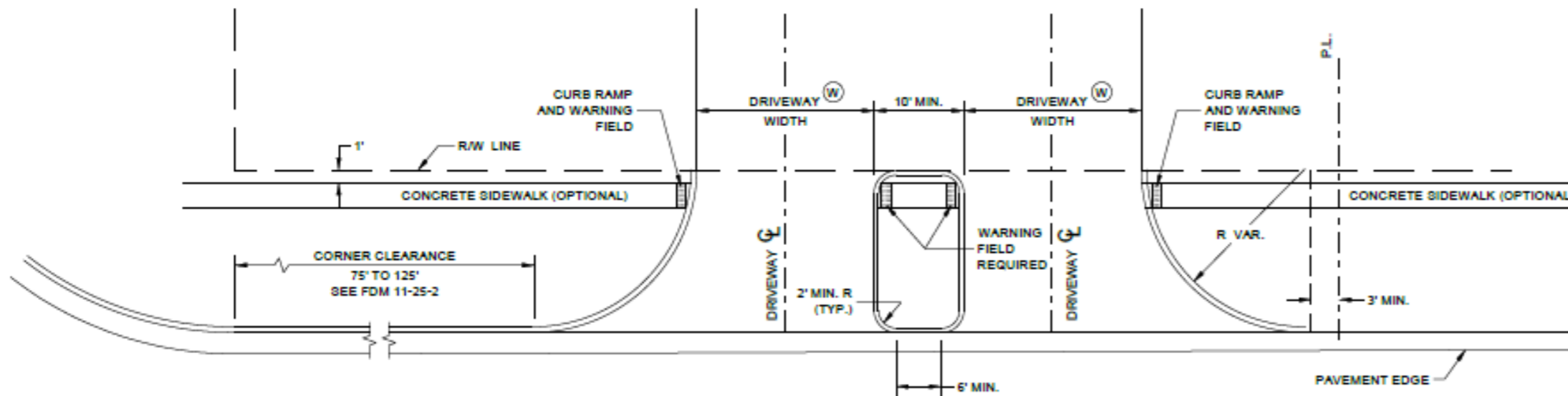
THE MINIMUM ANGLE OF INTERSECTION BETWEEN THE DRIVEWAY AND HIGHWAY CENTERLINES SHALL BE 70°.

ALL CURVILINEAR PRIVATE ENTRANCE OUTLINES SHALL BE CONTAINED WITHIN THE HIGHWAY R/W.

NO DRIVEWAY SHALL BE BUILT WITHIN 3 FEET OF THE PROPERTY LINE EXCEPT FOR EXISTING JOINT DRIVEWAY SHARED BY TWO OWNERS.

W: 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)

**SKewed DRIVEWAY DETAILS
(COMMERCIAL AND NON-COMMERCIAL)
SIDEWALK NOT SHOWN**



**DRIVEWAY LOCATION AND SPACING DETAILS
SIDEWALK SHOWN**

DRIVEWAYS WITH CURB AND GUTTER RETURNS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2015 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

ENTRUST COMMS

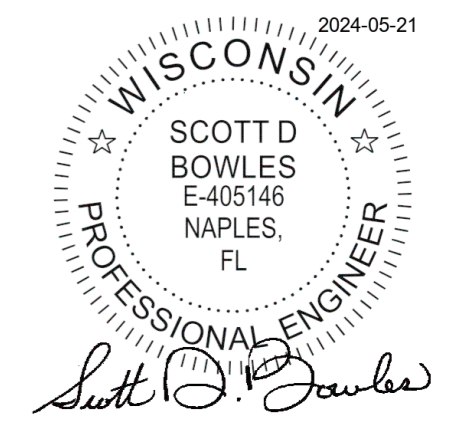
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-15

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



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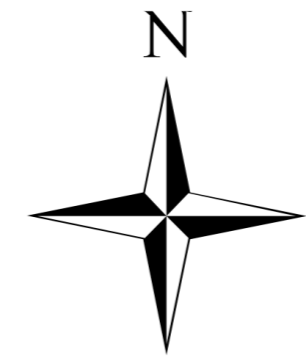
SDD 08D20 - 01

SDD 08D20 - 01

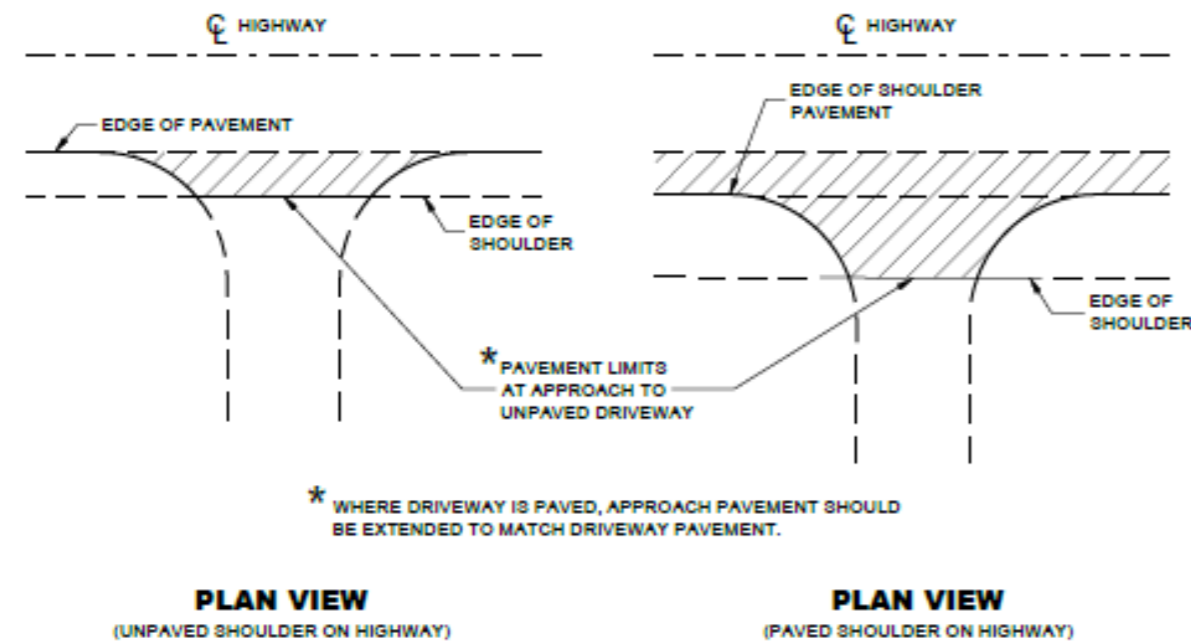
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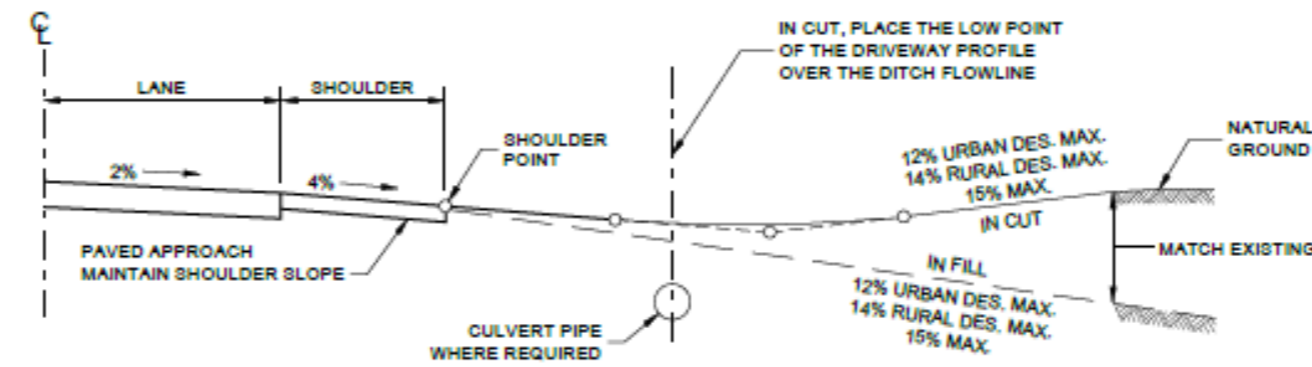


DATE: 5/21/2024
SHEET: 47 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

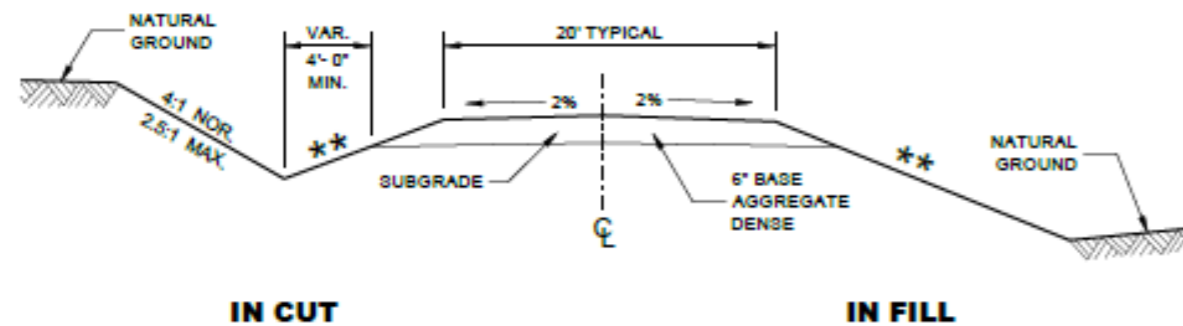


PLAN VIEW
(UNPAVED SHOULDER ON HIGHWAY) **PLAN VIEW**
(PAVED SHOULDER ON HIGHWAY)

**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



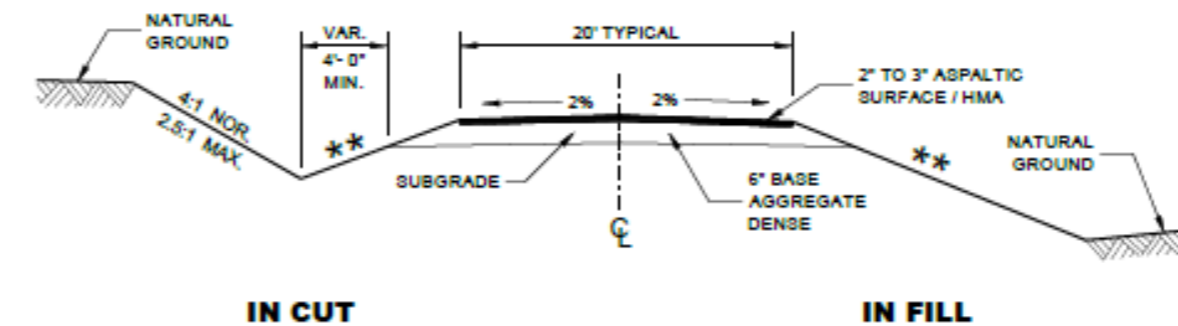
TYPICAL DRIVEWAY PROFILES



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥ 60	10:1



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

DRIVEWAYS WITHOUT CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2017
DATE

/s/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

6

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SDD 08D21 - 01

SDD 08D21 - 01

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-16

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS		
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

2024-05-21

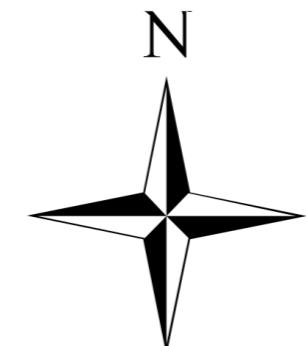
WISCONSIN
SCOTT D BOWLES
E-405146
NAPLES, FL
PROFESSIONAL ENGINEER

Scott D. Bowles

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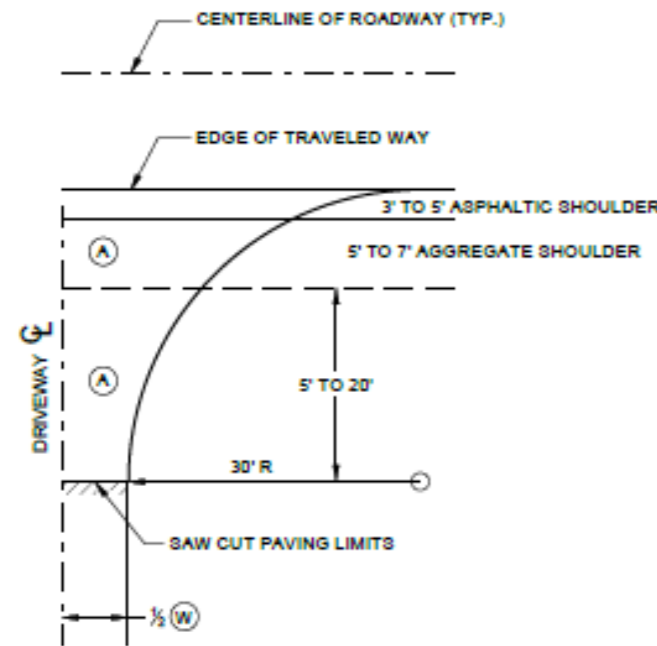
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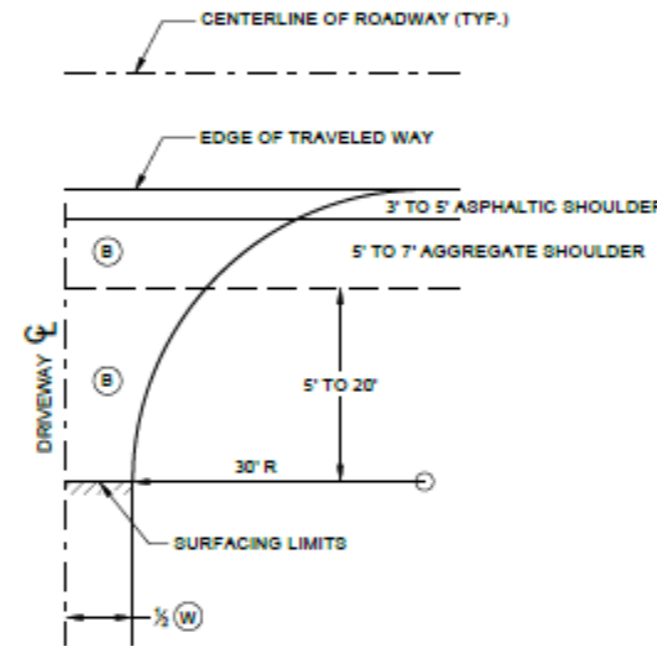


GENERAL NOTES

① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

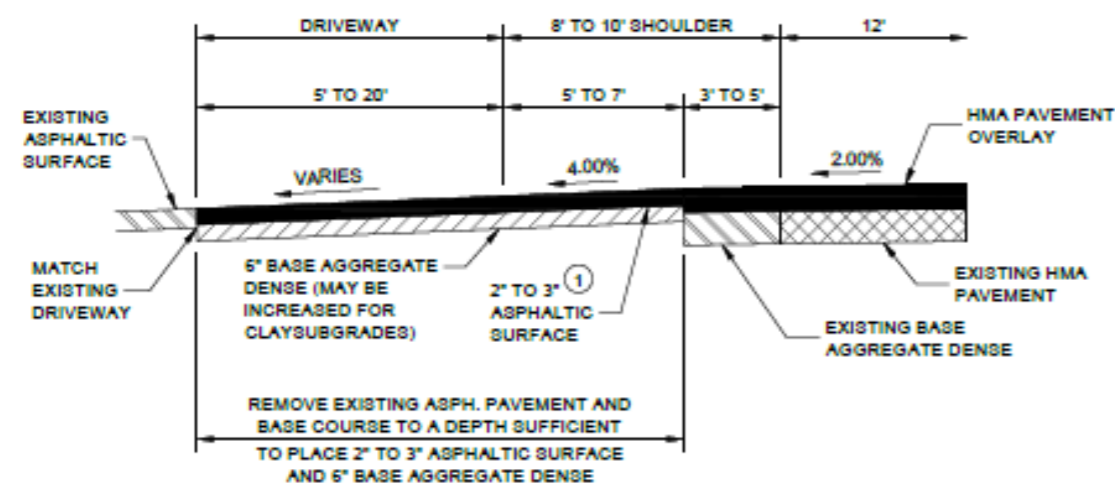


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/2" (TON)
- Ⓜ : DRIVEWAY WIDTH 15' MIN. - 24' MAX.

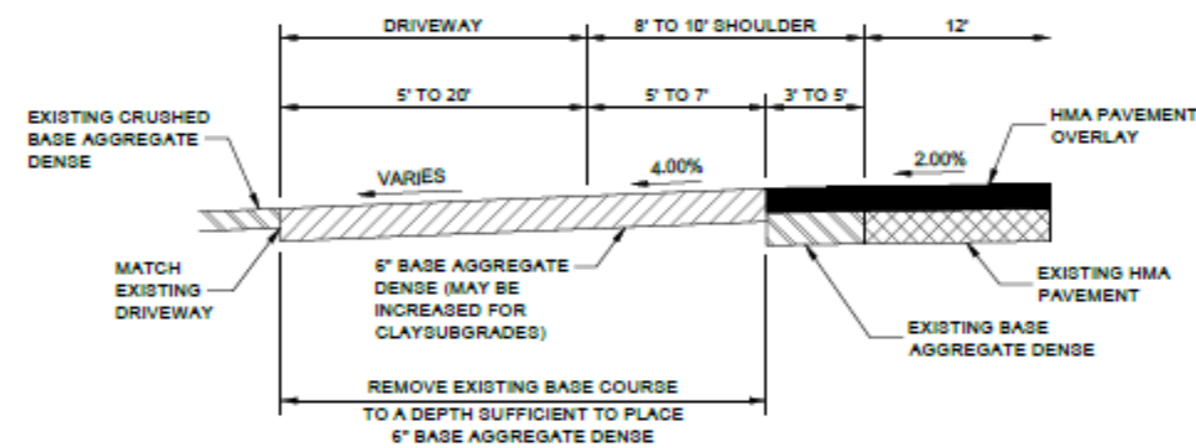


**PLAN VIEW
HALF SECTION**

**PLAN VIEW
HALF SECTION**



**PROFILE VIEW
RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS**



**PROFILE VIEW
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS**

**DRIVEWAYS WITHOUT CURB
AND GUTTER RESURFACING
PROJECTS RURAL**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

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SDD 08D22 - 01

SDD 08D22 - 01

ENTRUST COMMS

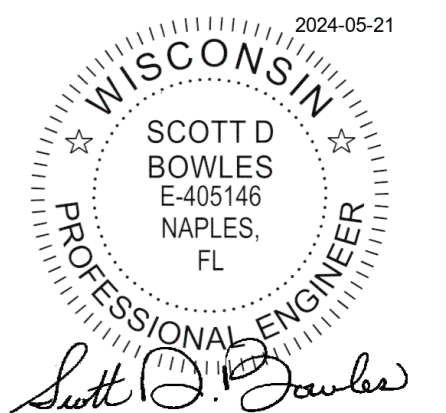
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-17

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

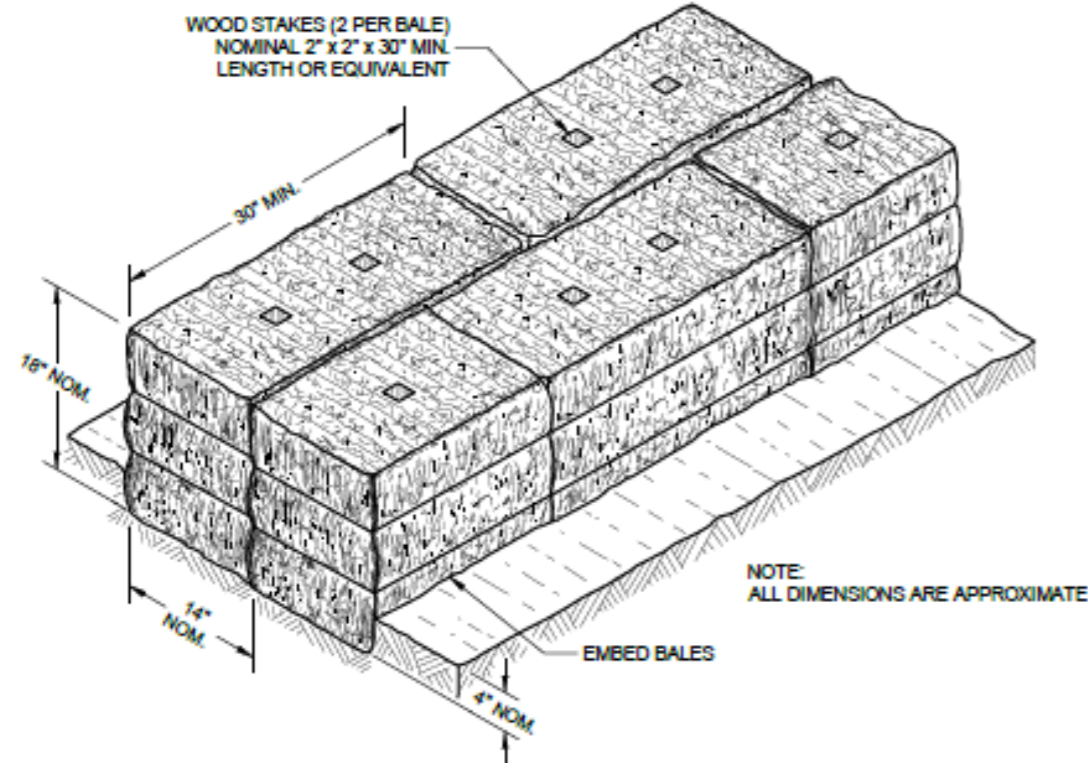


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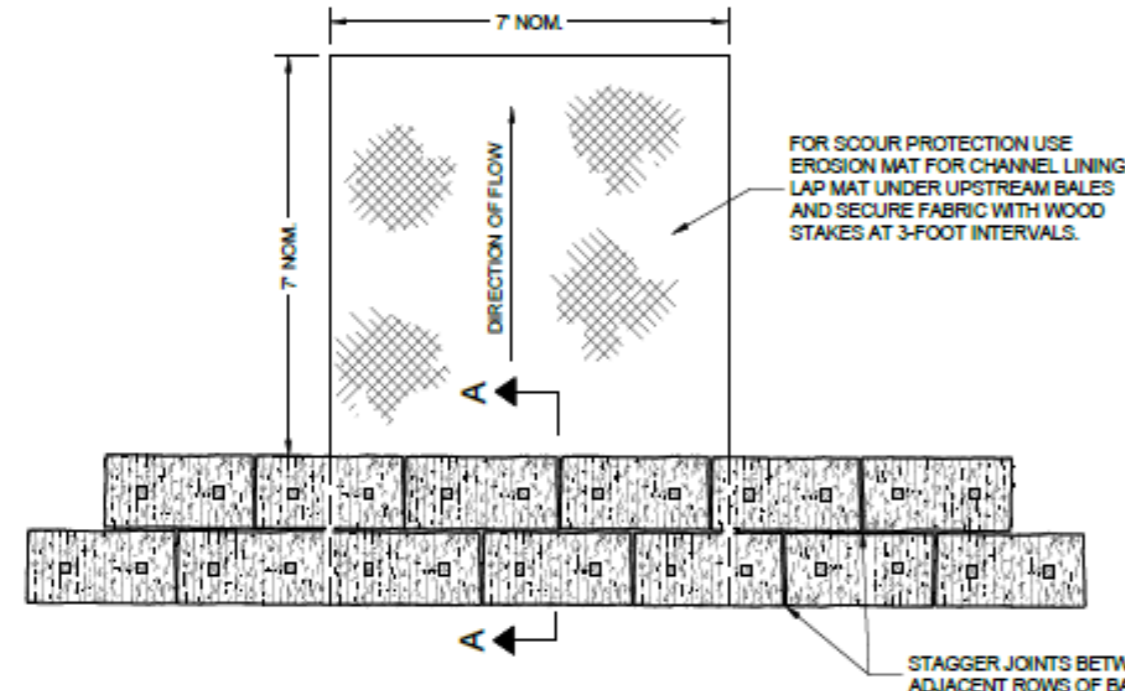
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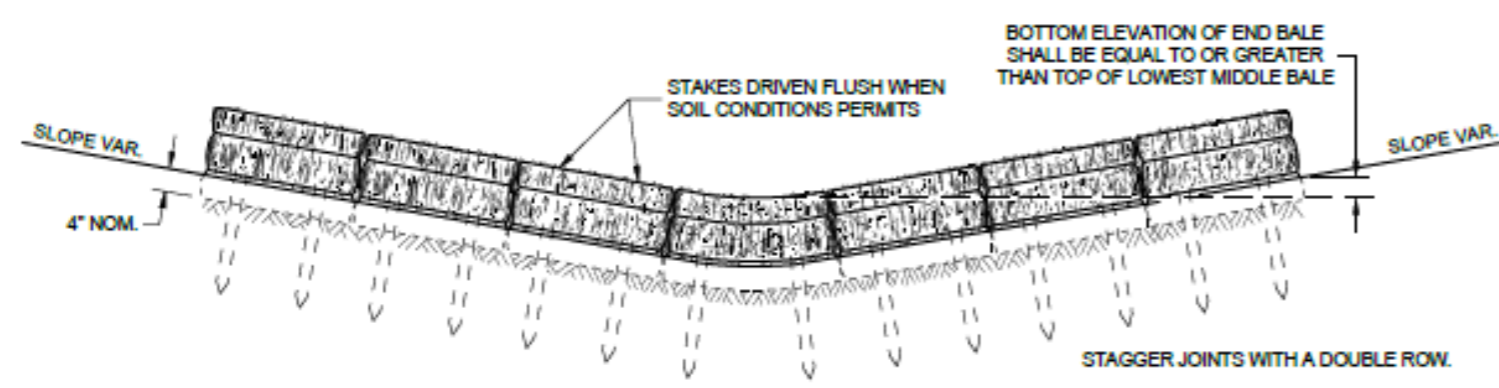




SECTION A - A



PLAN VIEW



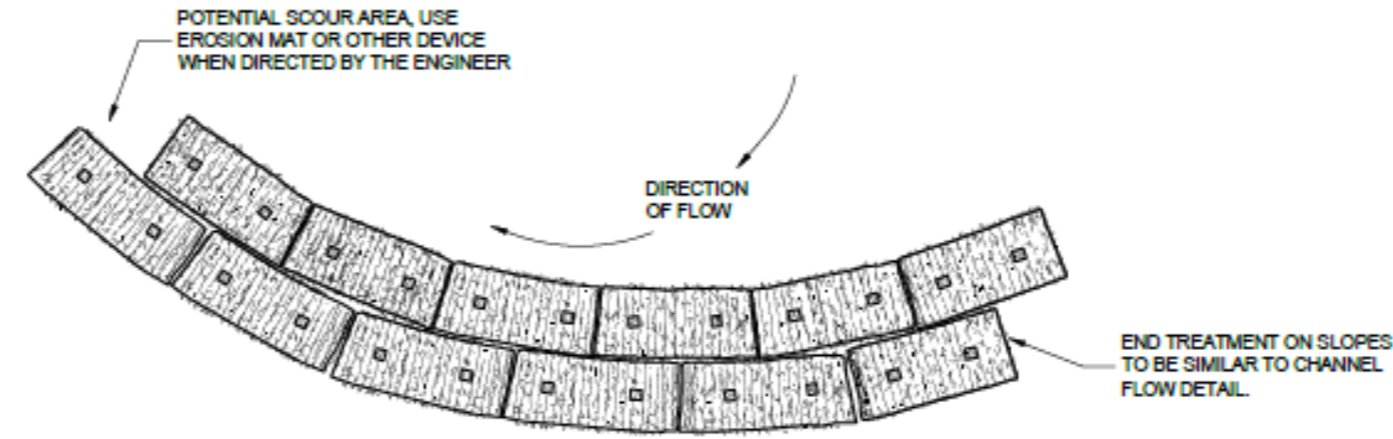
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES¹

GENERAL NOTES

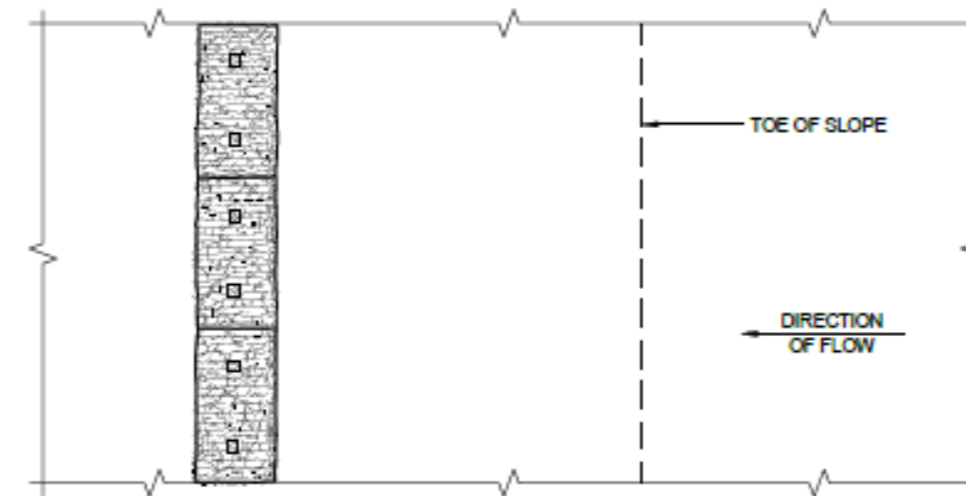
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

1 TEMPORARY DITCH CHECKS, EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

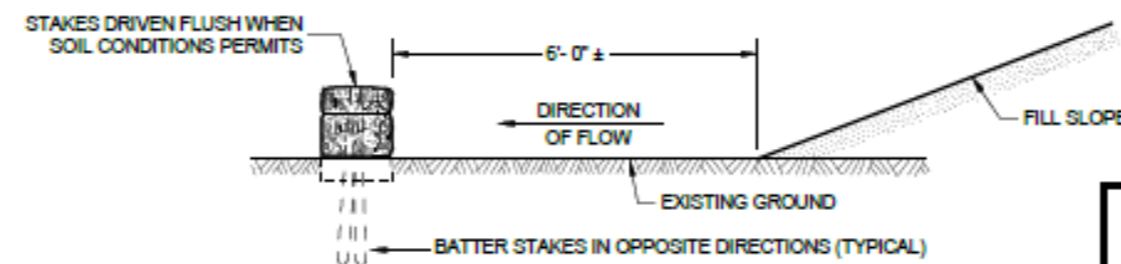


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN ELEVATION



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF EROSION BALES/TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02 DATE /B/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

6

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SDD 08E08 - 03

SDD 08E08 - 03

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-18

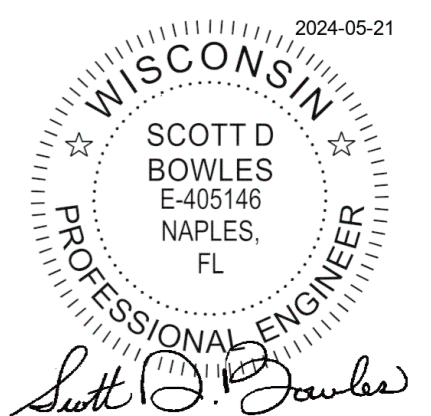
ENGINEER: SM

DRAWN BY: JW

CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



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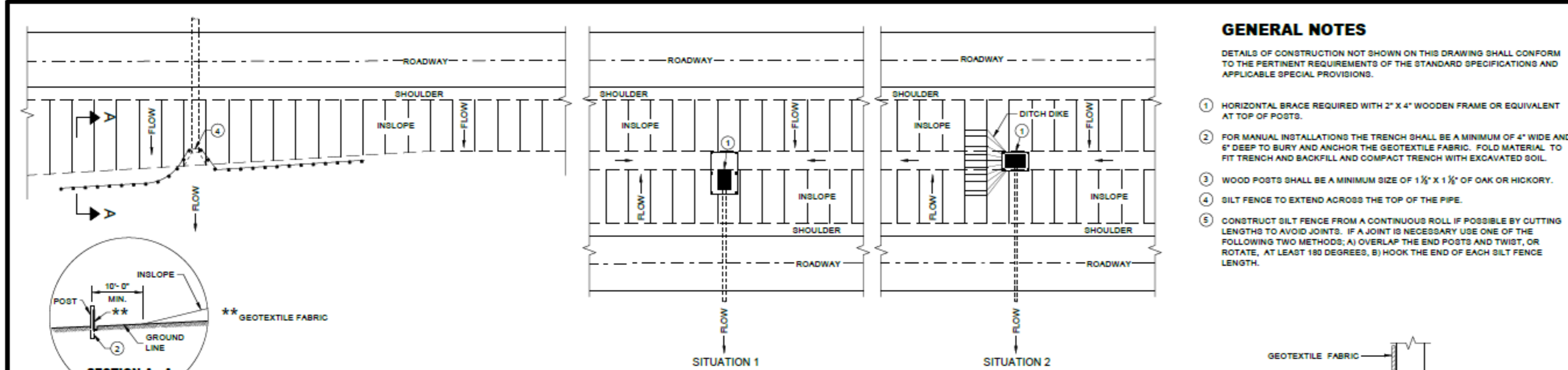
NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED



DATE: 5/21/2024

SHEET: 50 OF 69

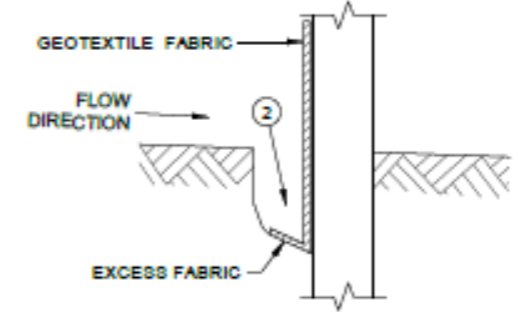
FILE: City of Superior, WI Construction Standards v4_05202024.pdf



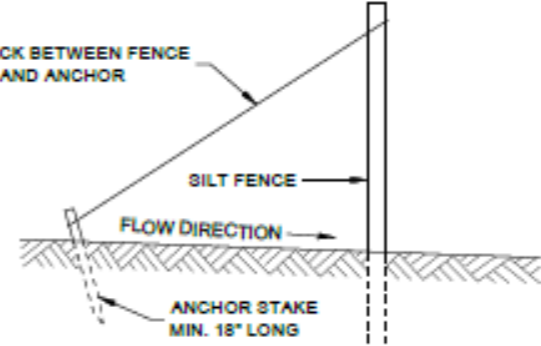
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE AND 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL AND COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/2" X 1 1/2" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

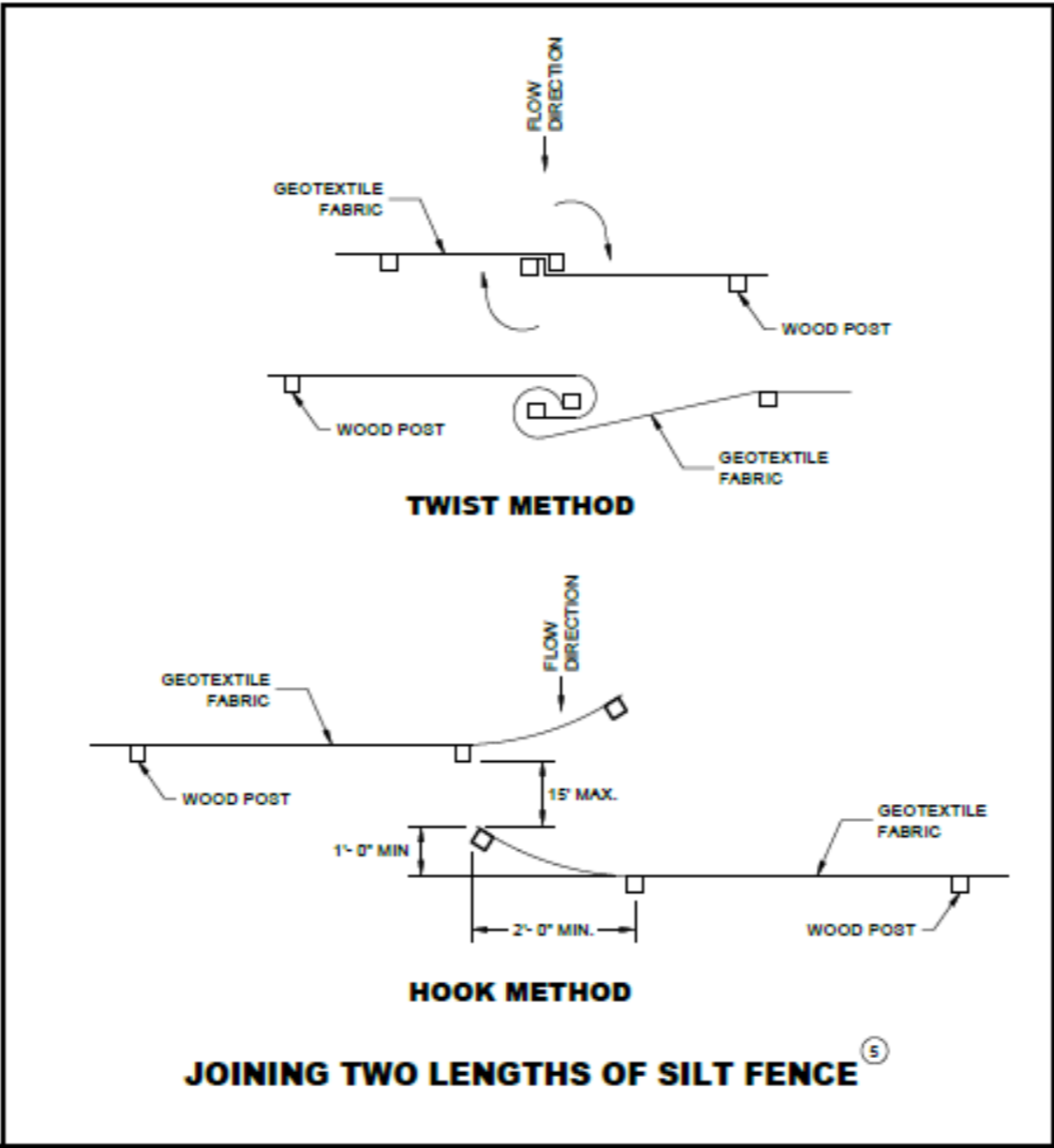


TRENCH DETAIL

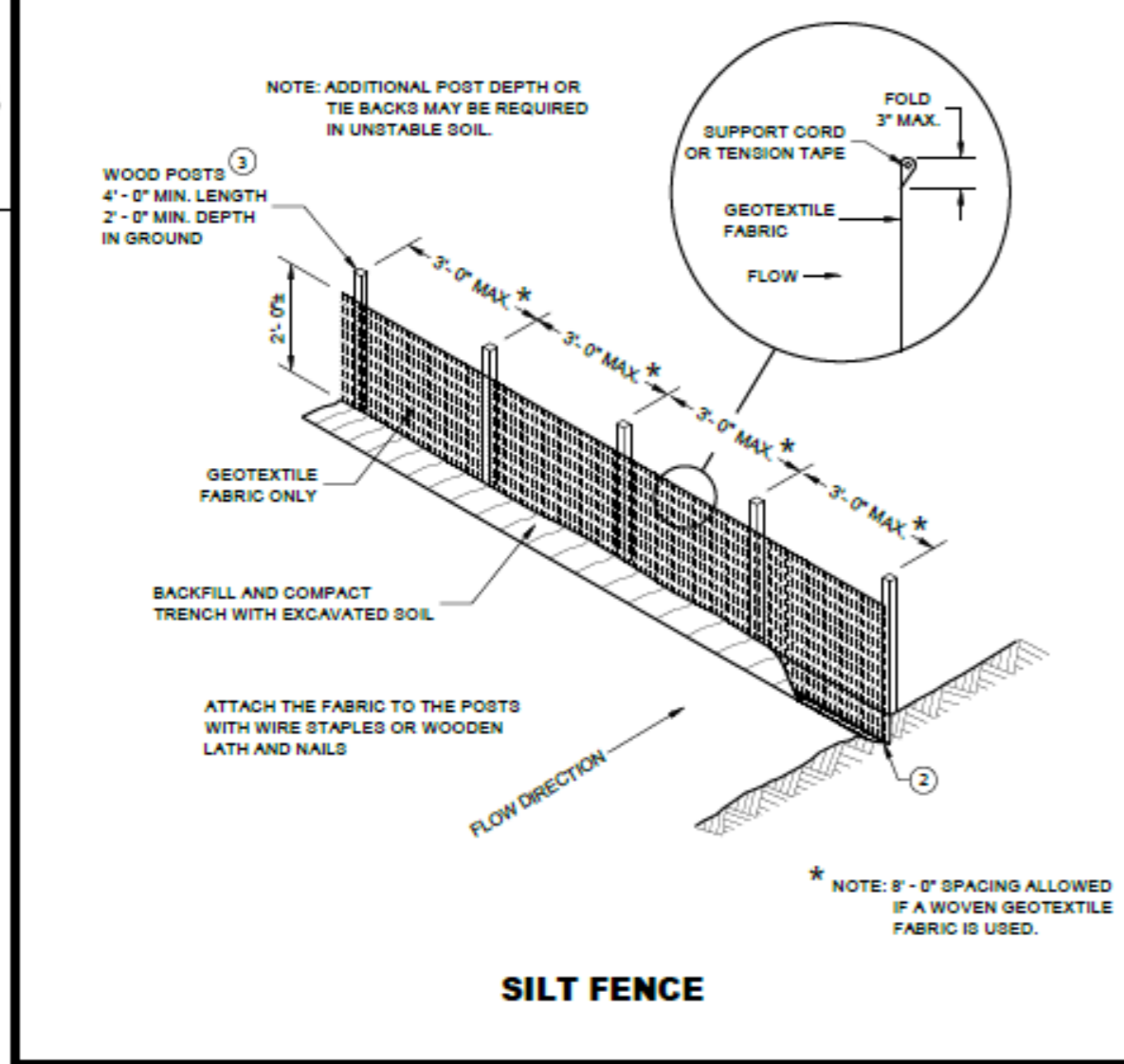


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS



JOINING TWO LENGTHS OF SILT FENCE



SILT FENCE

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4/25/05 DATE	/s/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER

ENTRUST COMMS

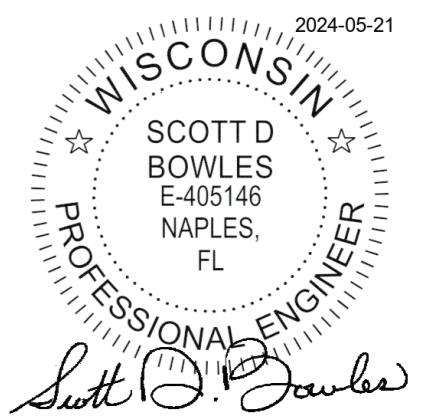
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-19

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

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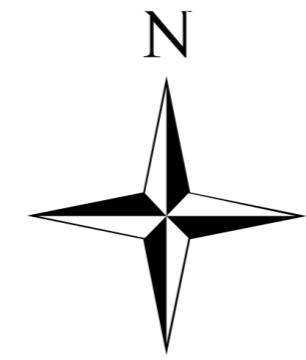
SDD 08E09 - 06

SDD 08E09 - 06

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ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-20

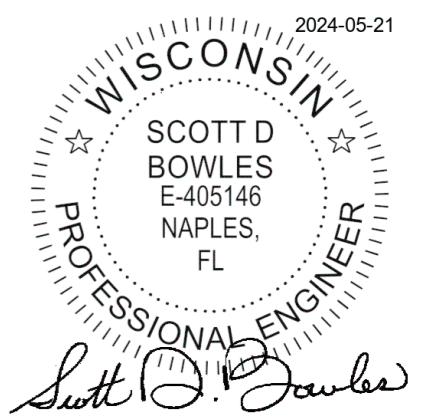
ENGINEER: SM

DRAWN BY: JW

CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

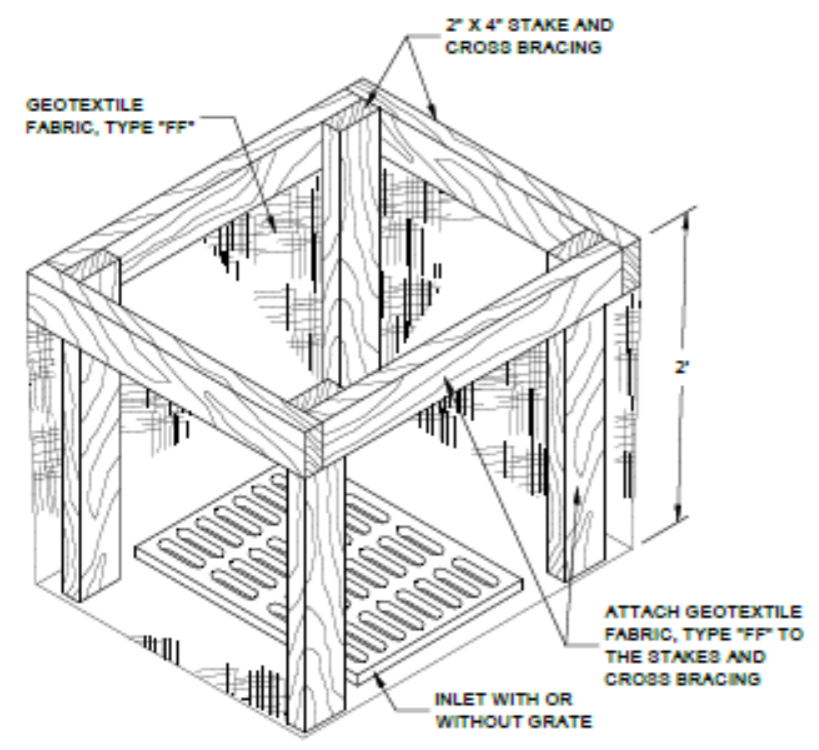
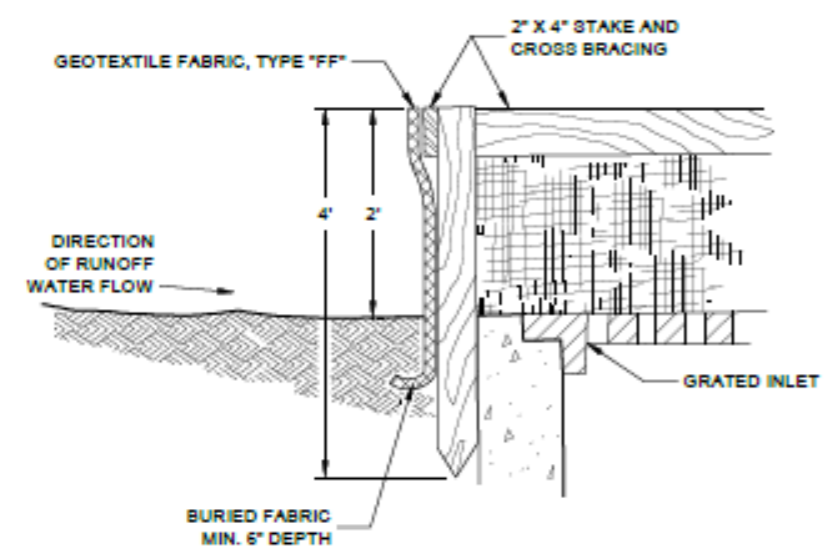


Scott D. Bowles

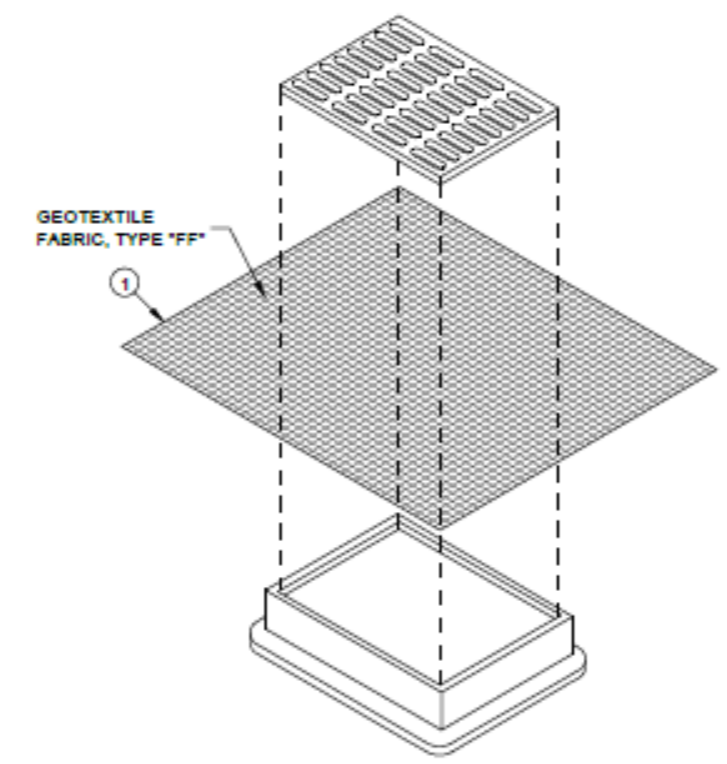
DATE: 5/21/2024

SHEET: 52 OF 69

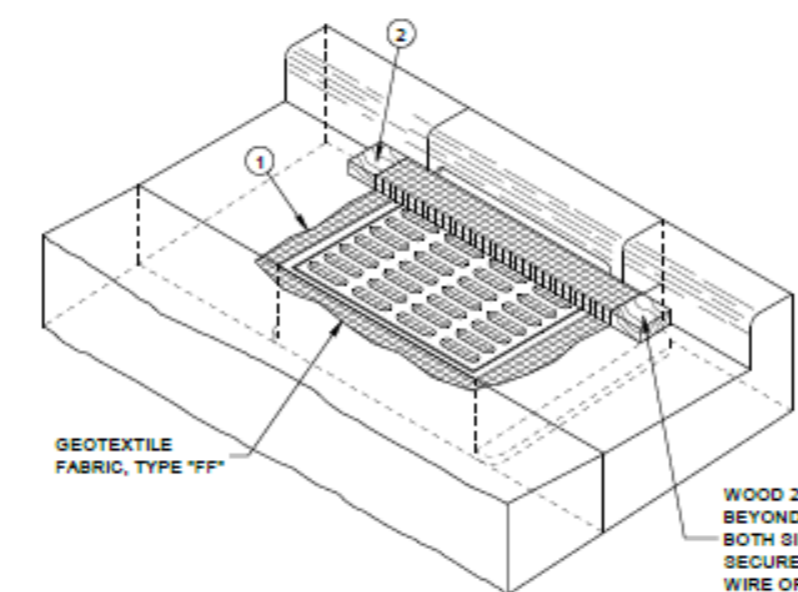
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf



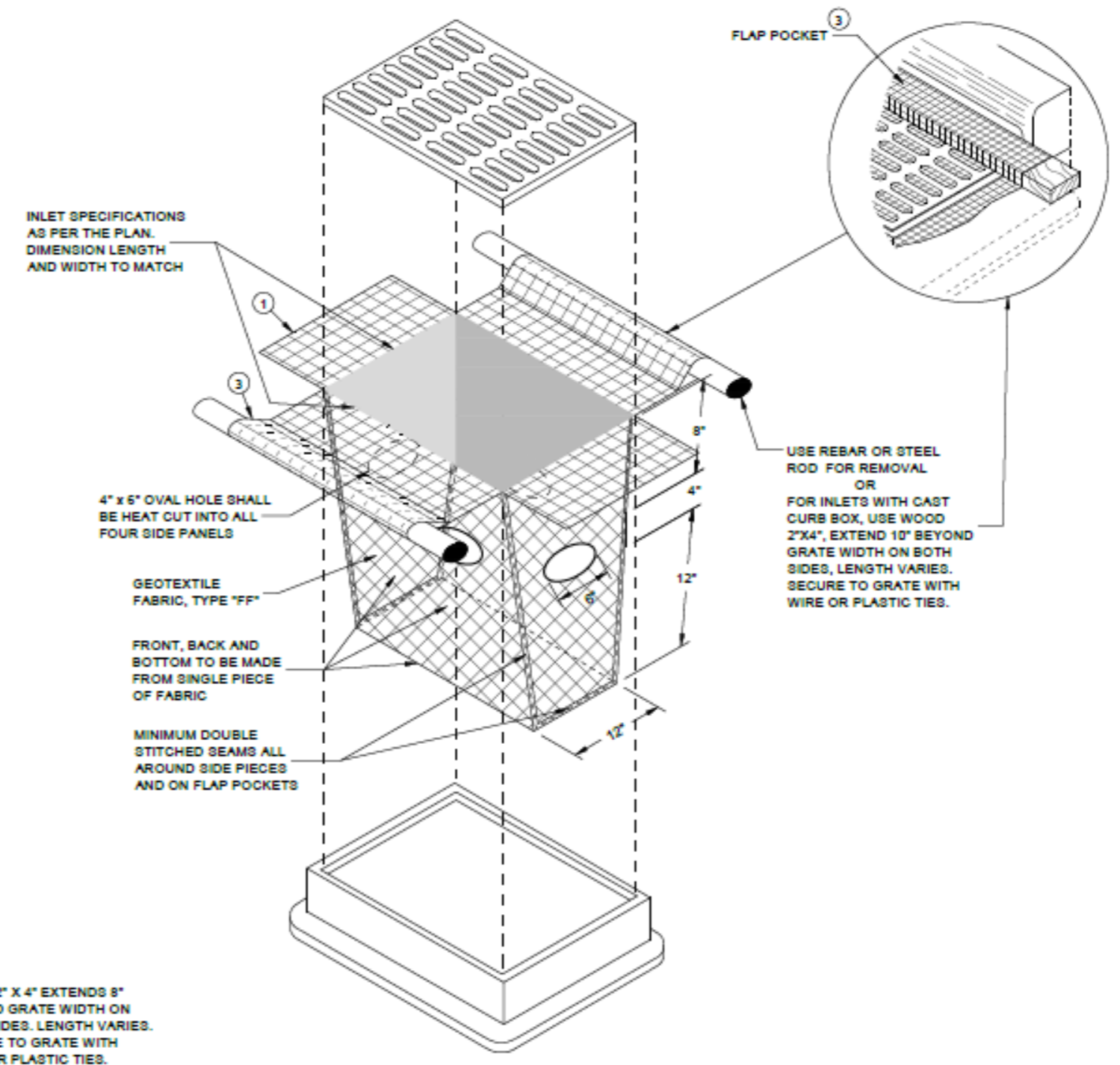
INLET PROTECTION, TYPE "A"



INLET PROTECTION, TYPE "B" (WITHOUT CURB BOX)
(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE "C" (WITH CURB BOX)



INLET PROTECTION, TYPE "D"
(CAN BE INSTALLED IN ANY INLET WITH OR WITHOUT A CURB BOX AS PER NOTE (2))

GENERAL NOTES

- INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.
- MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.
- WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.
- FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

- TYPES B & C**
TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.
- TYPE D**
TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.
DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION TYPES A, B, C AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/15/02 DATE
/s/ Beth Cannestra
ROADWAY STANDARDS DEVELOPMENT ENGINEER

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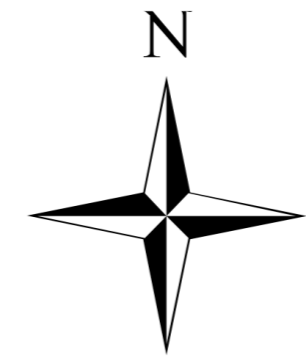
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SDD 08E10 - 02

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TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

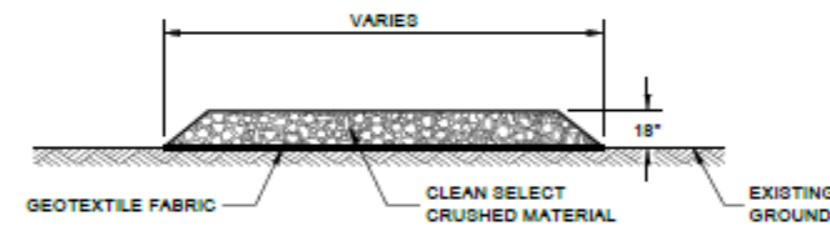
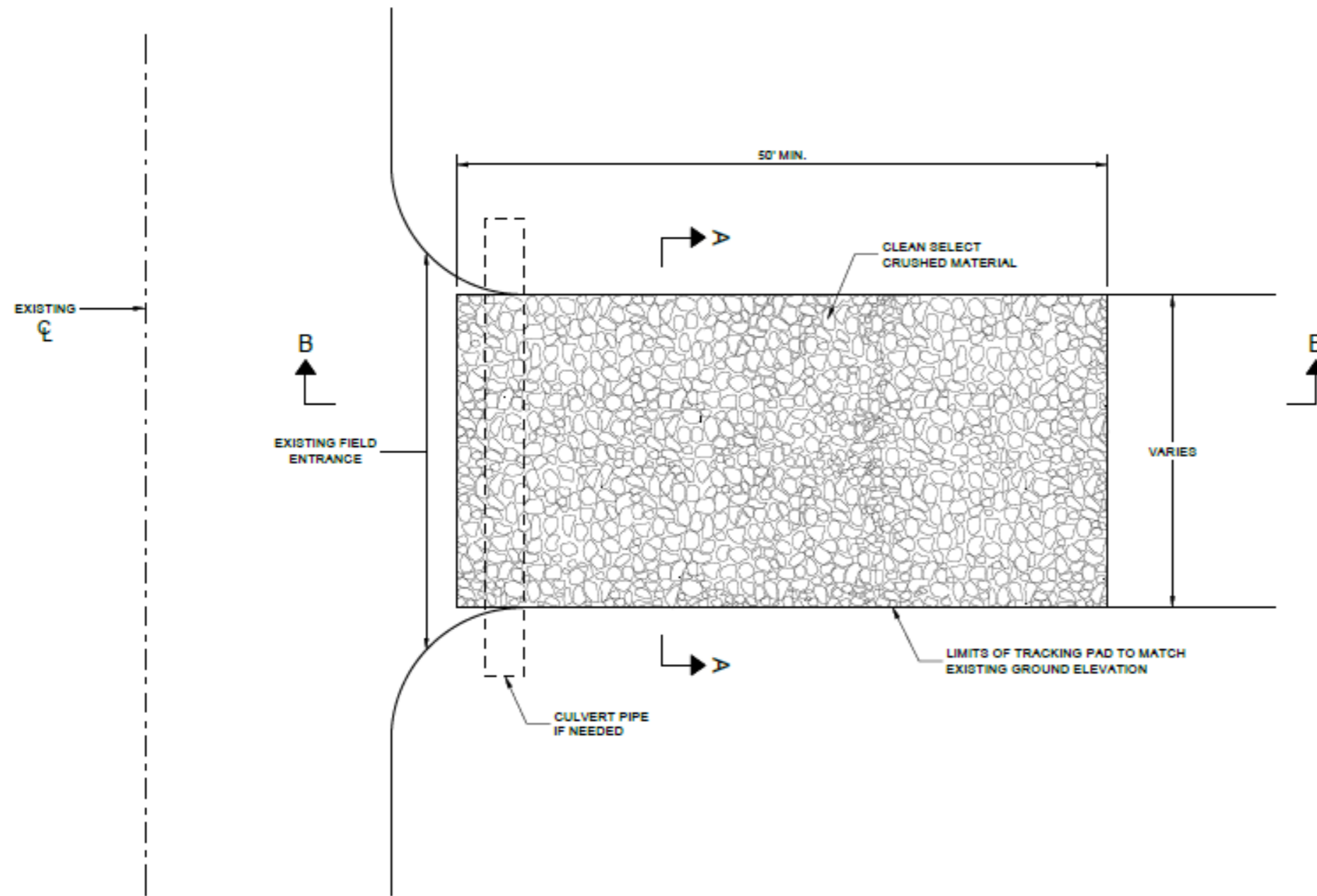
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

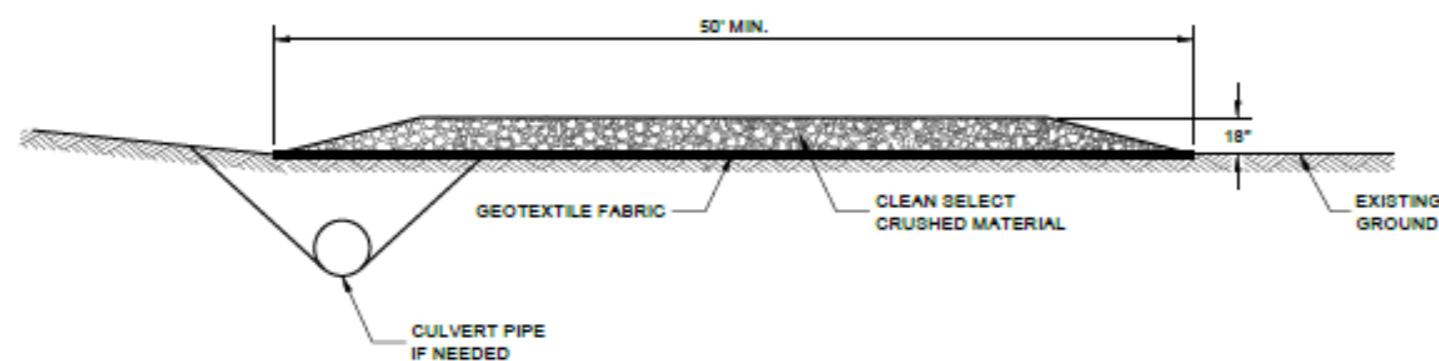
SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



SECTION A - A



SECTION B - B

TRACKING PAD

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
3/24/2011
DATE

/s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSR-21

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

2024-05-21

Scott D. Bowles

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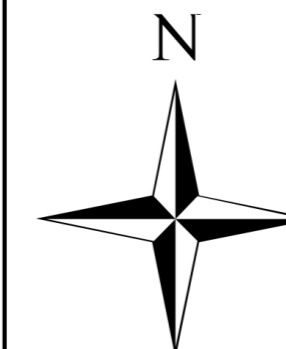
SDD 08E14 - 01

SDD 08E14 - 01

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PRIOR TO CONSTRUCTION CALL call811.org (TOLL FREE) AT 1-800-242-8511 OR 811 FOR LOCATION OF UNDERGROUND UTILITIES

NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED



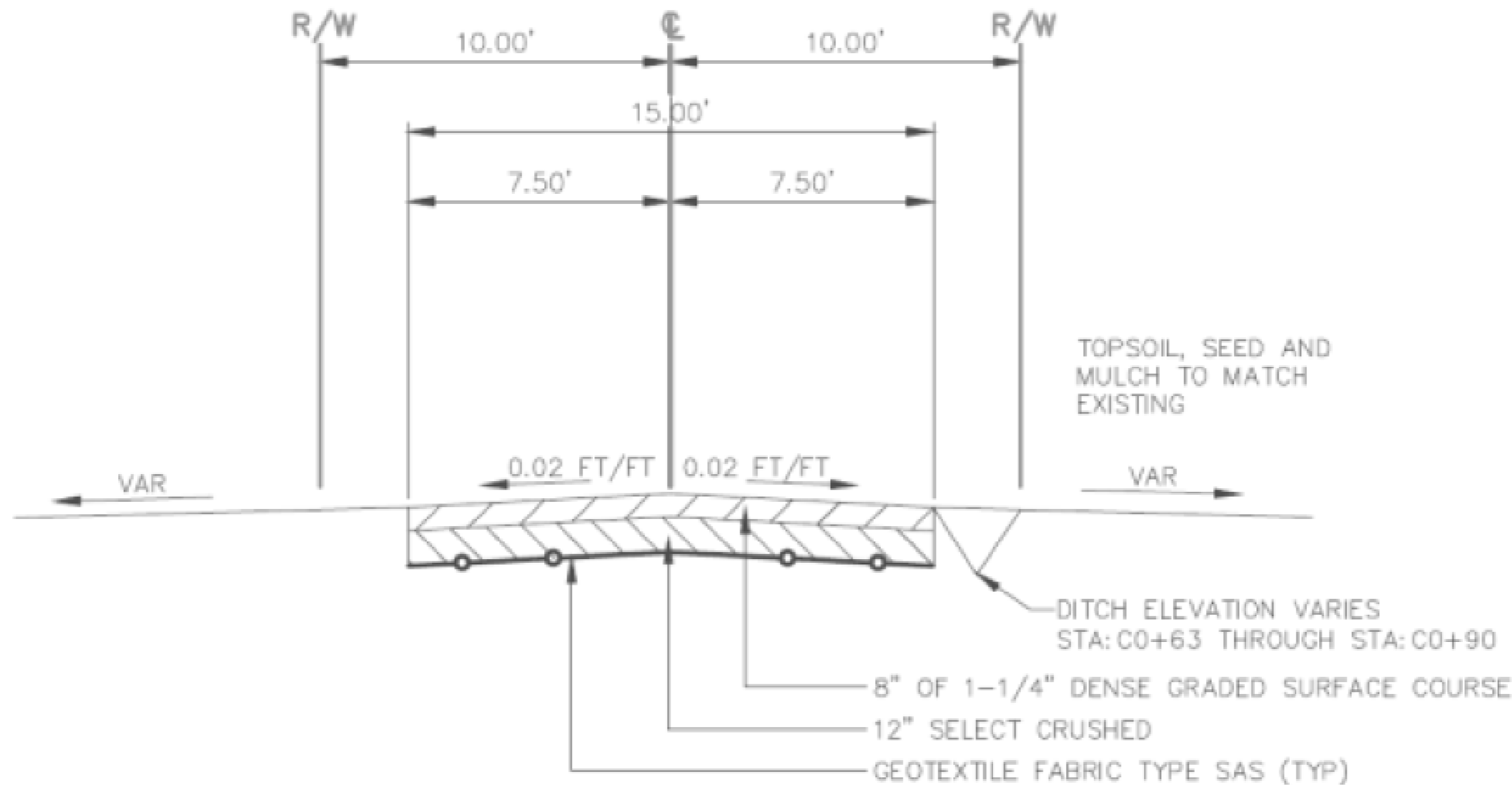
DATE: 5/21/2024

SHEET: 53 OF 69

FILE: City of Superior, WI Construction Standards_v4_05202024.pdf

City of Superior Alley Typical Section

The drawing shown is a gravel alley, for any blacktop alley, add 4 inches of blacktop to the surface. For any concrete alley, match existing concrete depths or more than 7 inches. If there is concrete and asphalt, then match the existing profile of each surface type.



ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-01

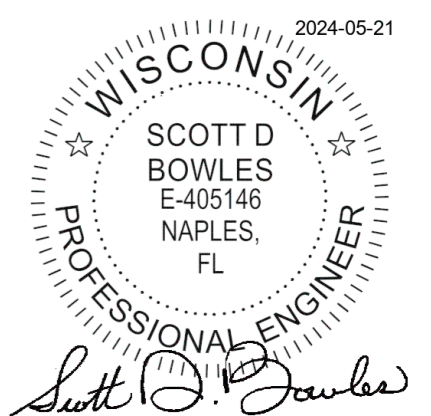
ENGINEER: SM

DRAWN BY: JW

CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



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DATE: 5/21/2024

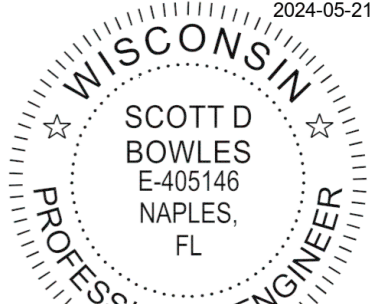
SHEET: 54 OF 69

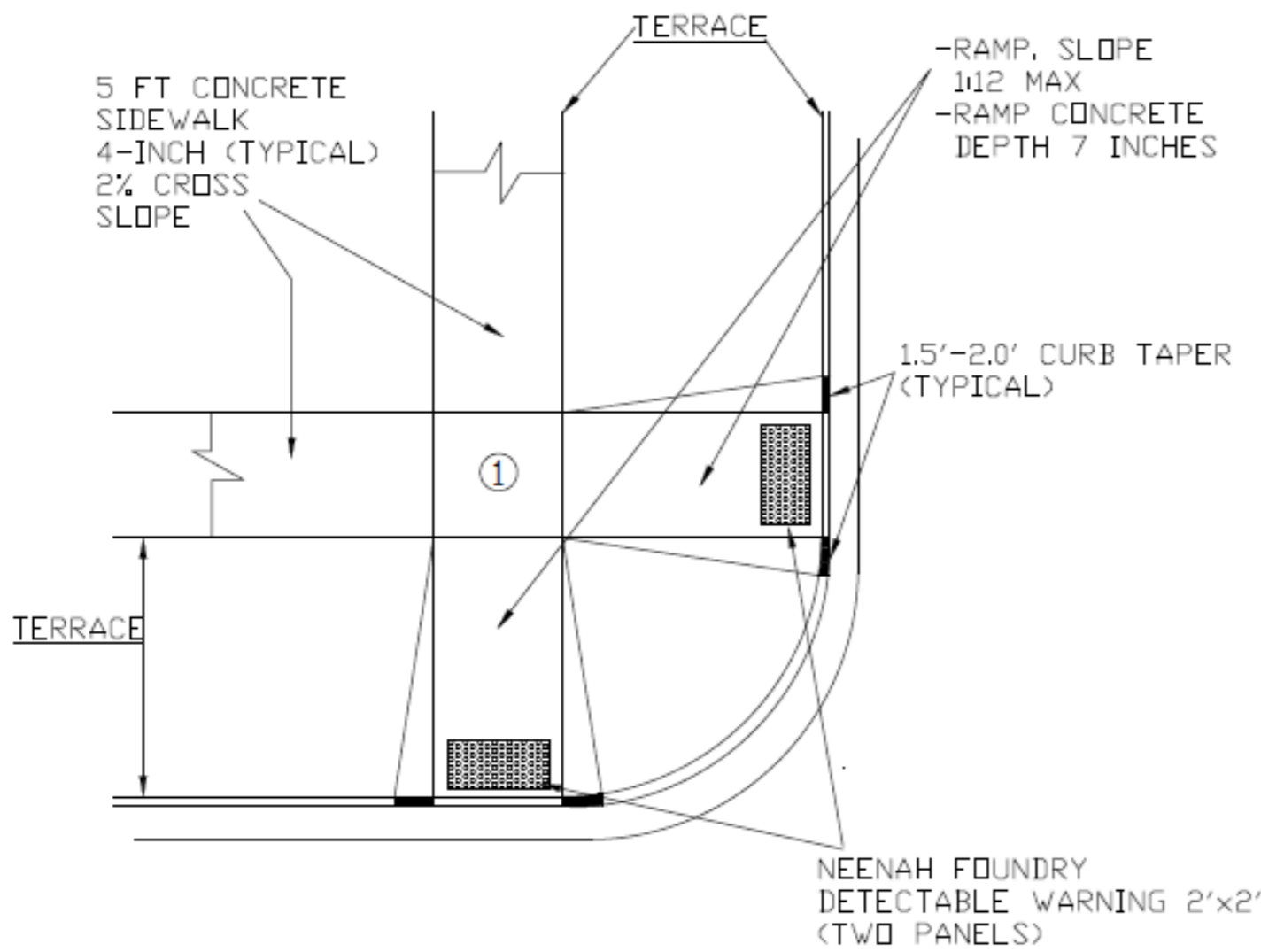
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

2024-05-21

 SCOTT D. BOWLES
 E-405146
 NAPLES, FL
 PROFESSIONAL ENGINEER
Scott D. Bowles

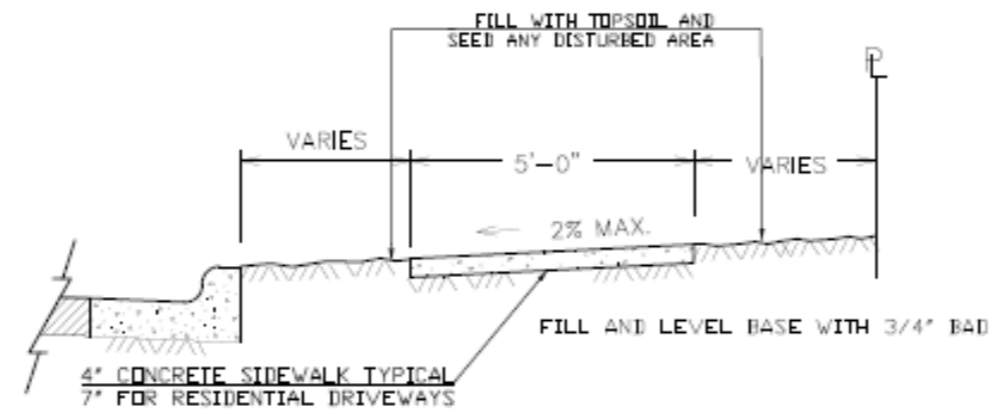


① **LANDING NOTES:**
 PROVIDE LANDING AT TOP OF RAMP WITH NO MORE THAN 2% SLOPE IN ANY DIRECTION. MINIMUM LANDING SIZE 5'x5', CONCRETE DEPTH SHALL BE 4 INCHES.

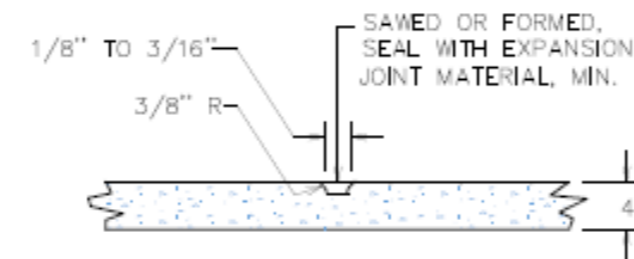
PED-RAMP OVERHEAD DETAIL
 (NO SCALE)

PEDESTRIAN RAMP REPLACEMENT NOTES:

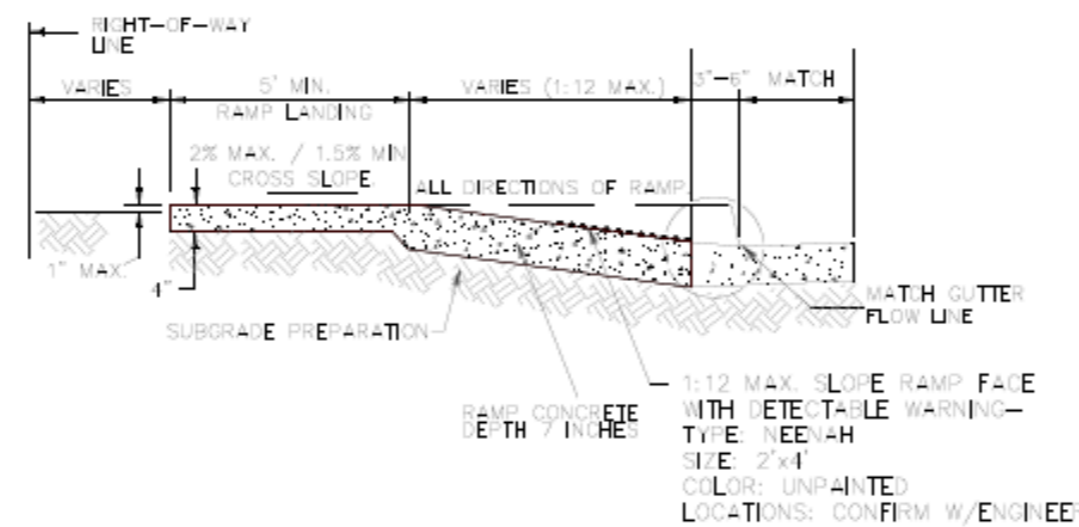
ALL PEDESTRIAN RAMPS ON PROJECT WILL BE REPLACED TO BRING THEM TO ADA SPECIFICATIONS.



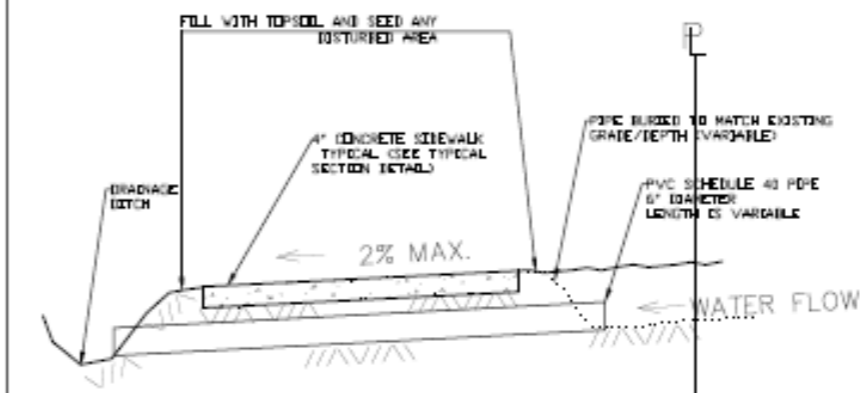
TYPICAL CONCRETE SIDEWALK SECTION



SIDEWALK CONTRACTION JOINT
 (NO SCALE)



PED-RAMP CROSS-SECTION
 (NO SCALE)



PVC PIPE INSTALLATION DETAIL
 (VARIABLE LENGTH)
 (NO SCALE)

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ENTRUST COMMS

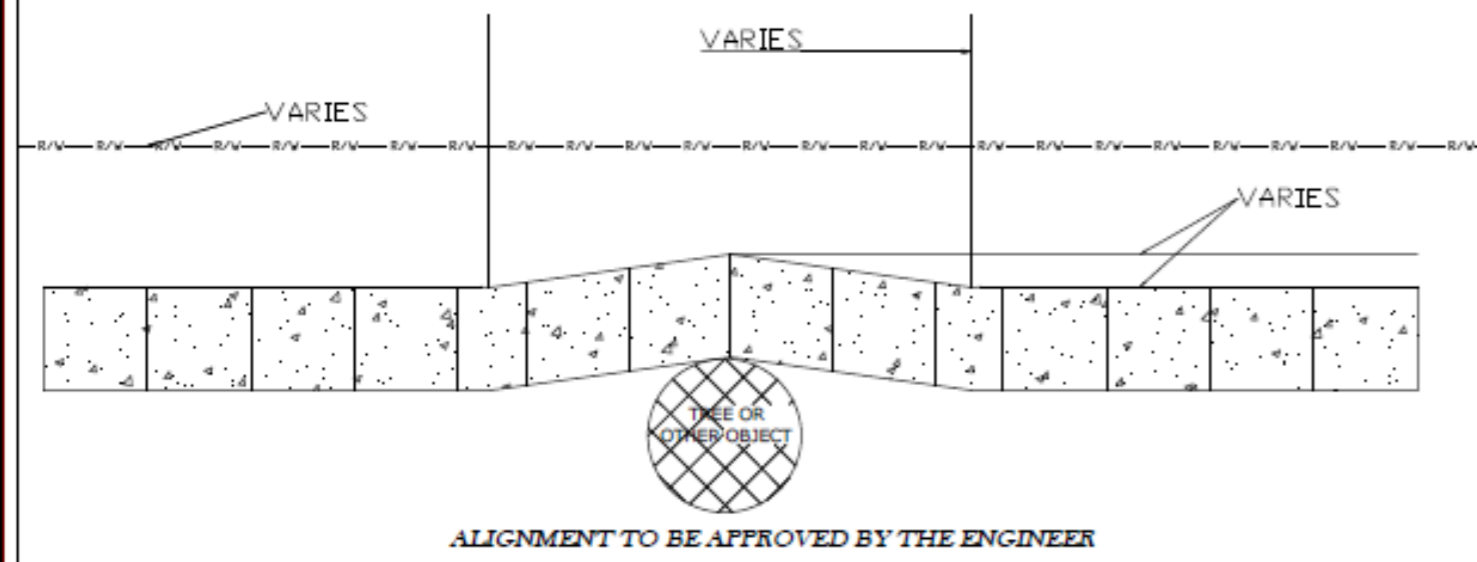
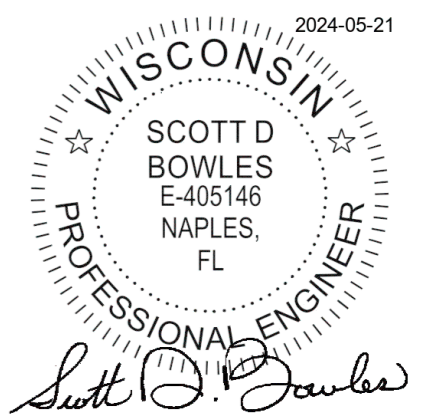
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-03

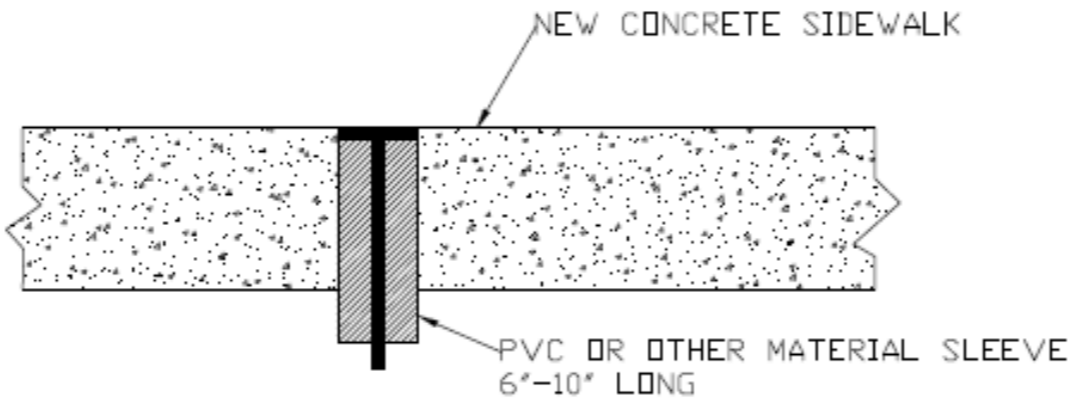
ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

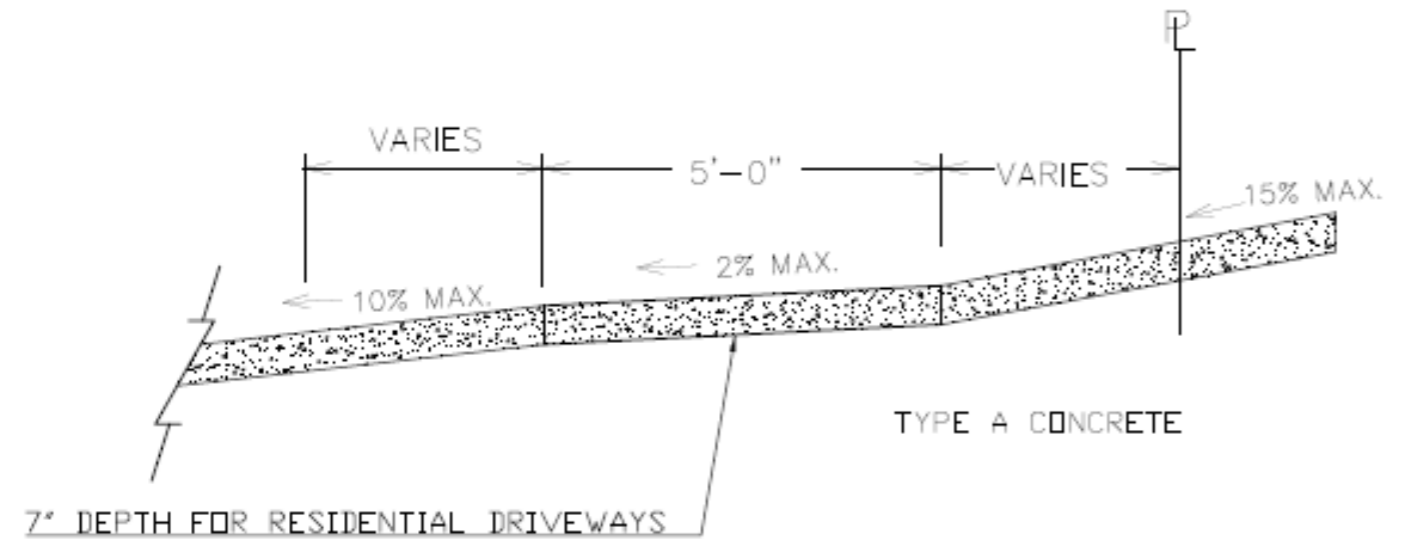


SIDEWALK BUMPOUT DETAIL



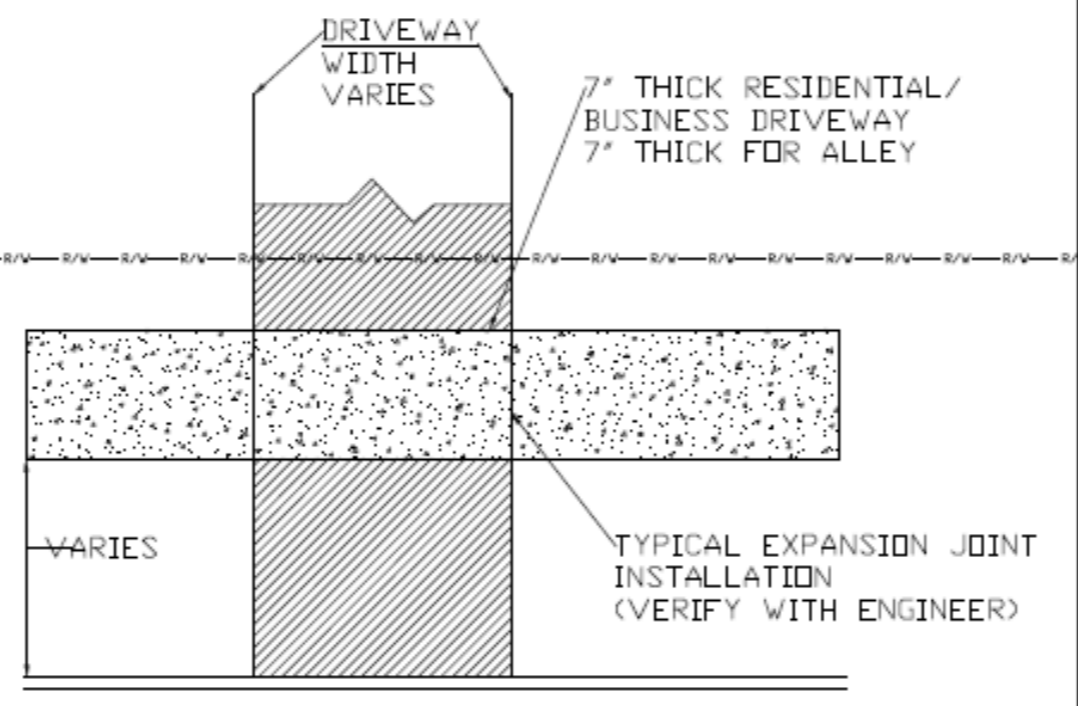
WATER STOPBOX REPAIR DETAIL

(NO SCALE)



TYPICAL CONCRETE DRIVEWAY SECTION

(NO SCALE)



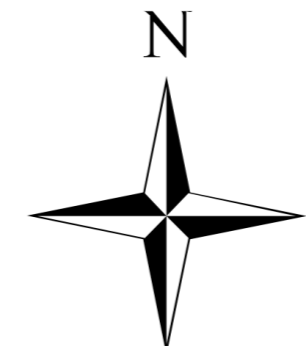
SIDEWALK THROUGH DRIVEWAY OR ALLEY

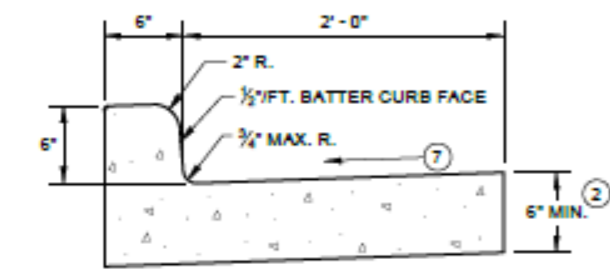
(NO SCALE)

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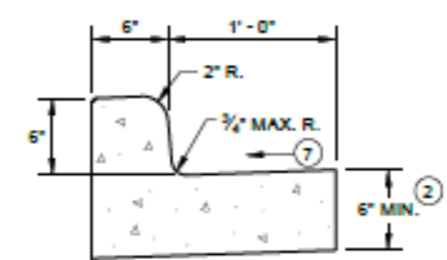
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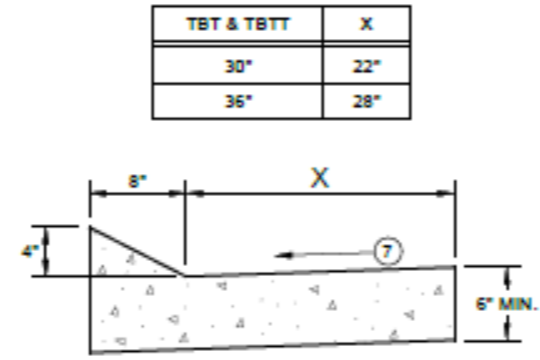


TYPES A¹ & D



TYPES A¹ & D

CONCRETE CURB AND GUTTER 18"



TYPES TBT & TBTT¹

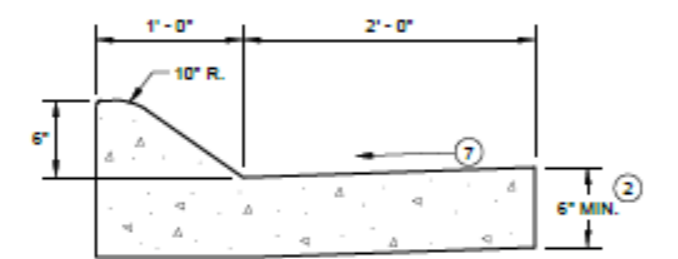
CONCRETE CURB AND GUTTER

GENERAL NOTES

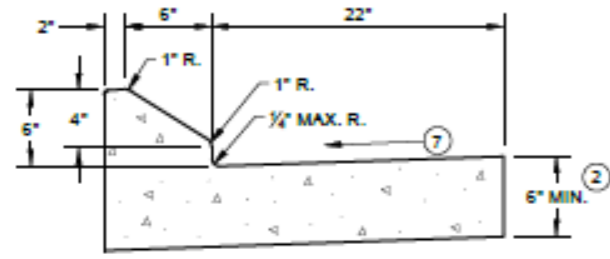
- DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.
- PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.
- INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.
- UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.
- TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBT.
 - THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
 - USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
 - THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
 - THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
 - WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
 - USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
 - INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWS.

PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

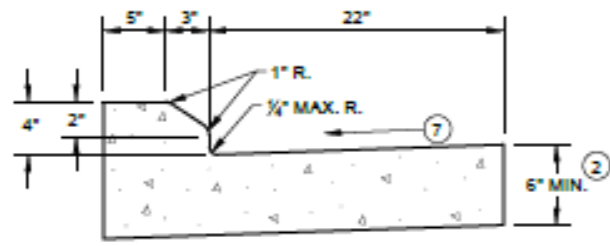
PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



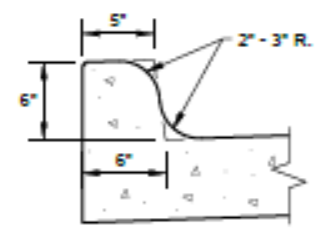
6" SLOPED CURB TYPES A¹ & D



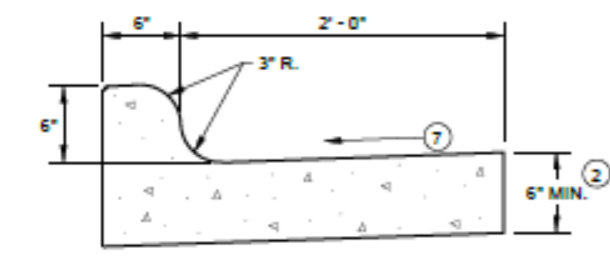
6" SLOPED CURB TYPES G¹ & J



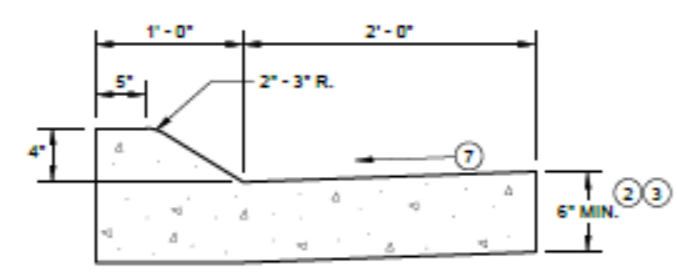
4" SLOPED CURB TYPES G¹ & J



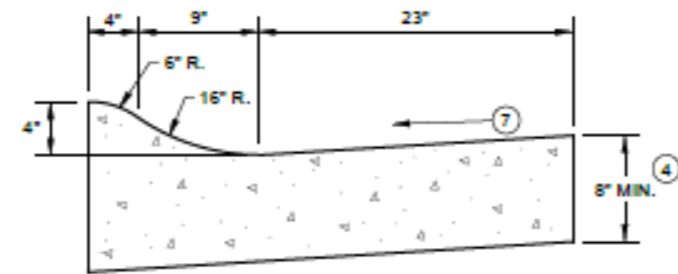
TYPES K¹ & L
(OPTIONAL CURB SHAPE)



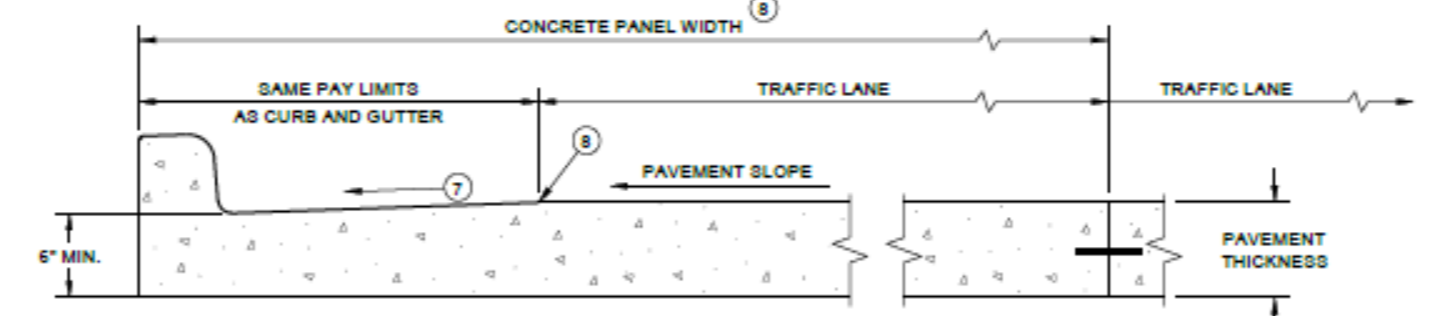
TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"



4" SLOPED CURB TYPES A¹ & D

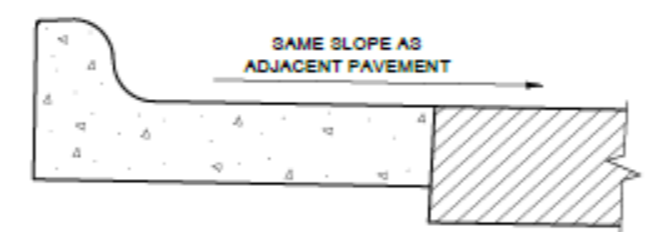


4" SLOPED CURB TYPES R¹ & T⁵
CONCRETE CURB AND GUTTER 36"



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁵
(TYPICAL FOR ALL CURB & GUTTER TYPES)

CONCRETE CURB AND GUTTER
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 08D01 - 21a

SDD 08D01 - 21a

ENTRUST COMMS

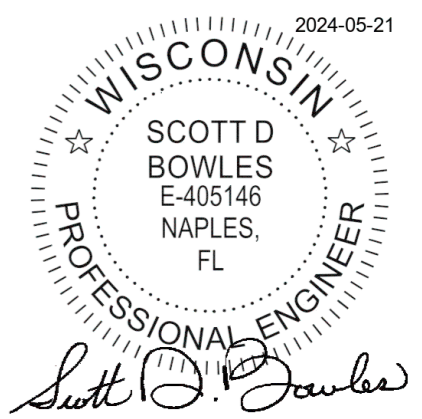
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-04

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

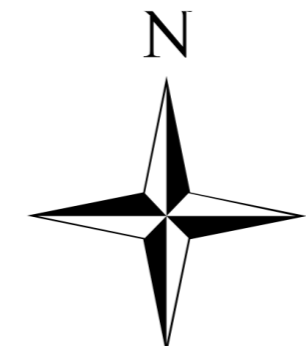


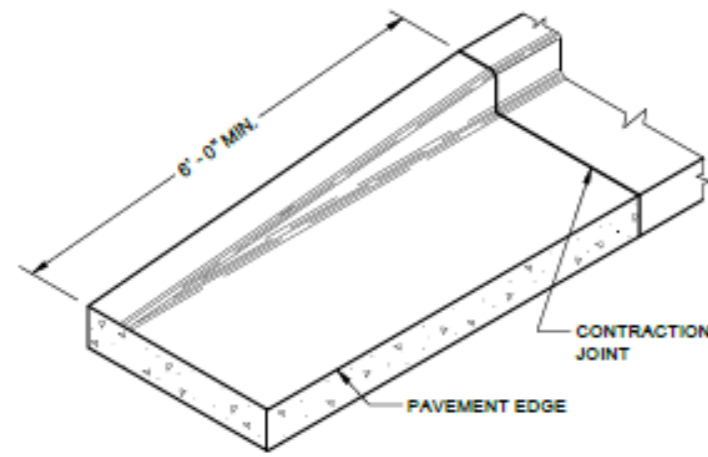
Scott D. Bowles

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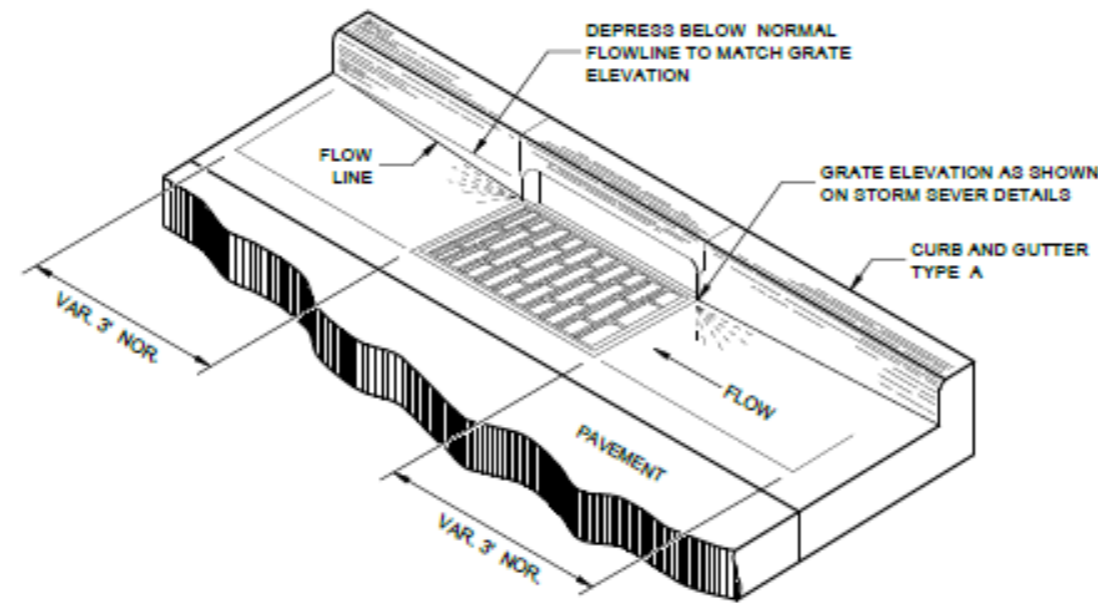
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END SECTION CURB AND GUTTER

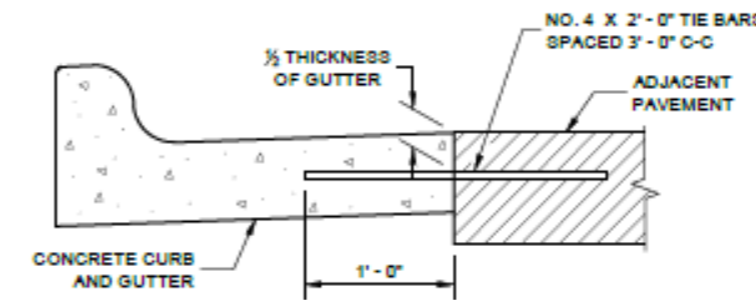


DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

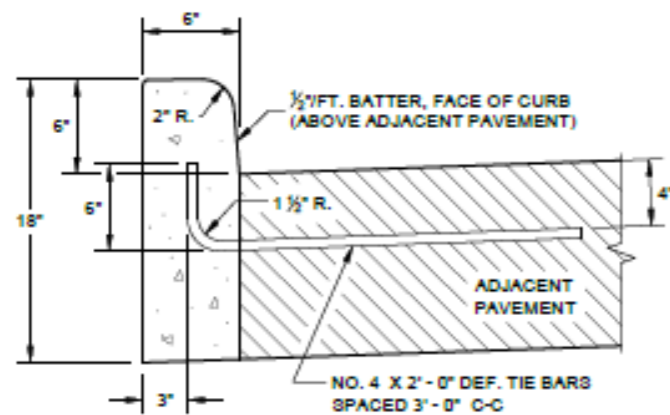
GENERAL NOTES

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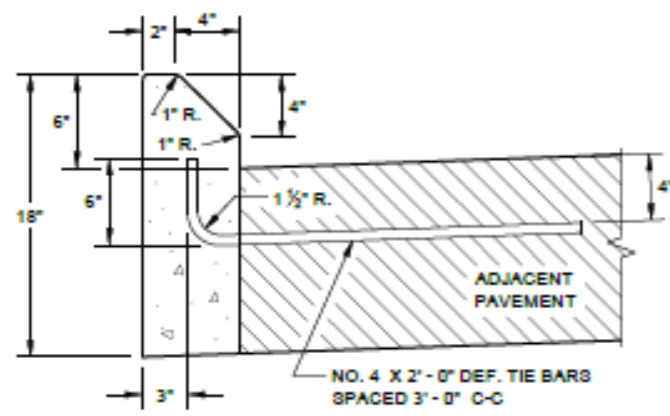
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND T8TT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

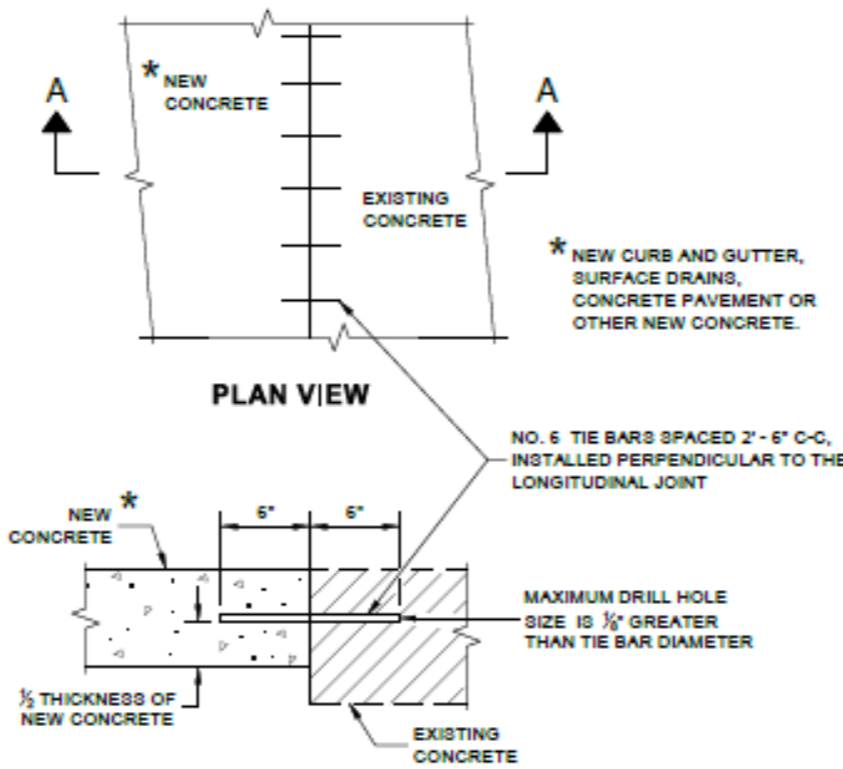


TYPES A ① & D



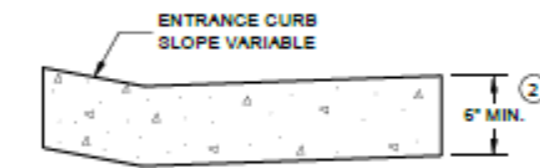
TYPES G ① & J

CONCRETE CURB



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB ③
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 APPROVED
 February 2020 DATE /S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER

6

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SDD 08D01 - 21b

SDD 08D01 - 21b

ENTRUST COMMS

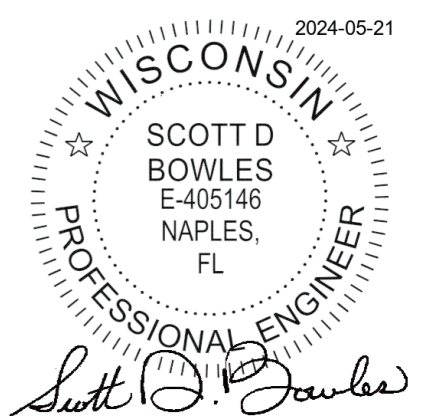
CITY OF SUPERIOR, WI
 FIBER OPTIC NETWORK
 TYPICAL PACKAGE
 PRELIMINARY - FOR REVIEW

SHEET-COSS-05

ENGINEER: SM
 DRAWN BY: JW
 CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

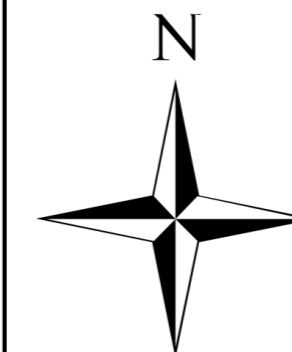


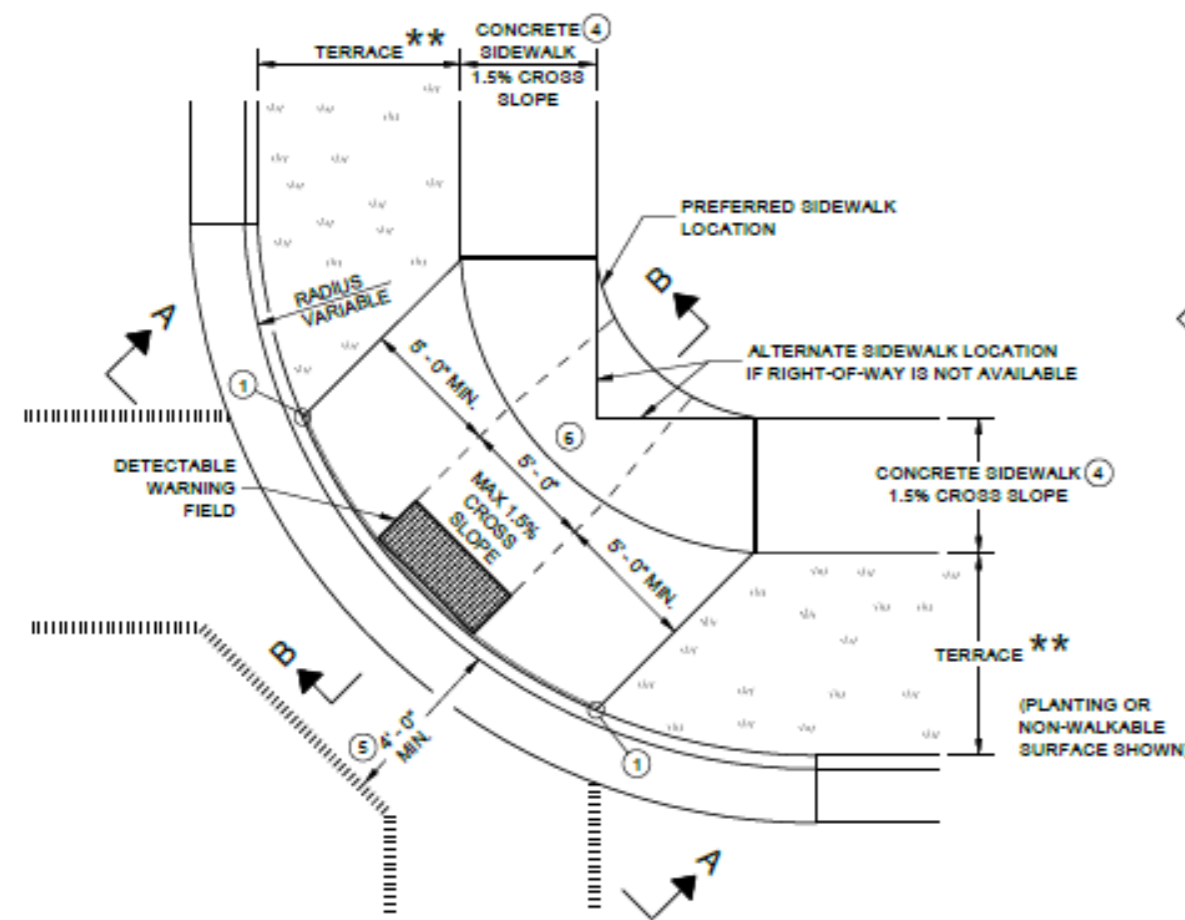
DATE: 5/21/2024
 SHEET: 58 OF 69
 FILE: City of Superior, WI Construction Standards v4_05202024.pdf

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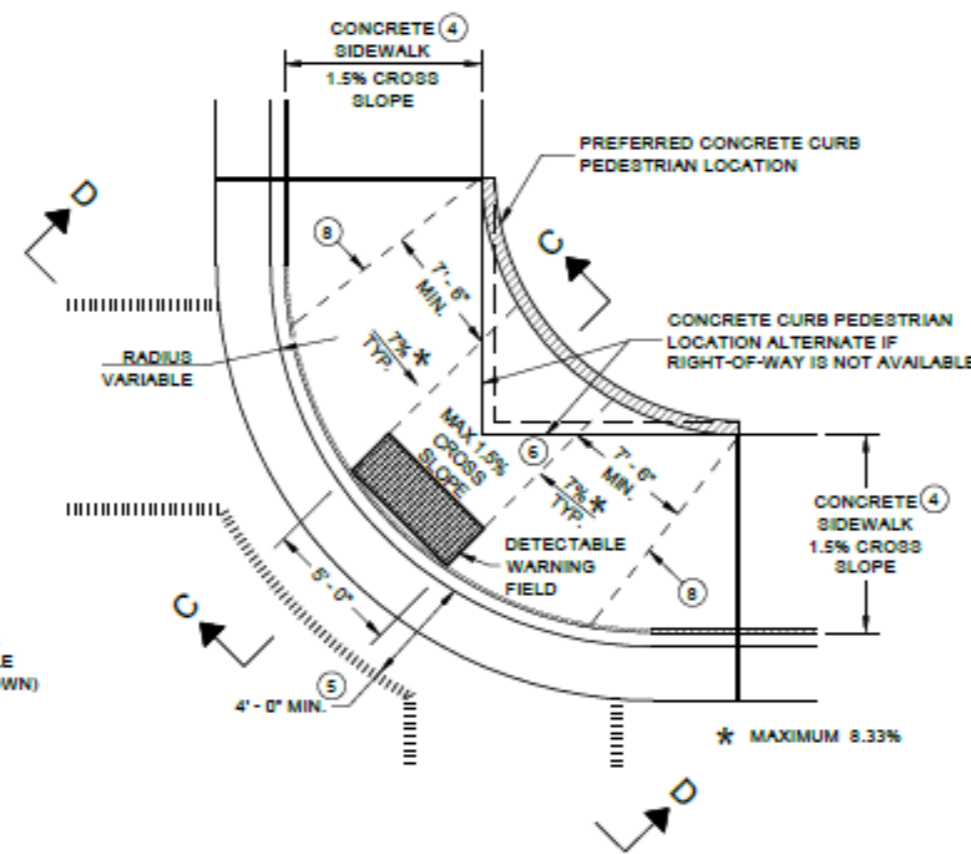
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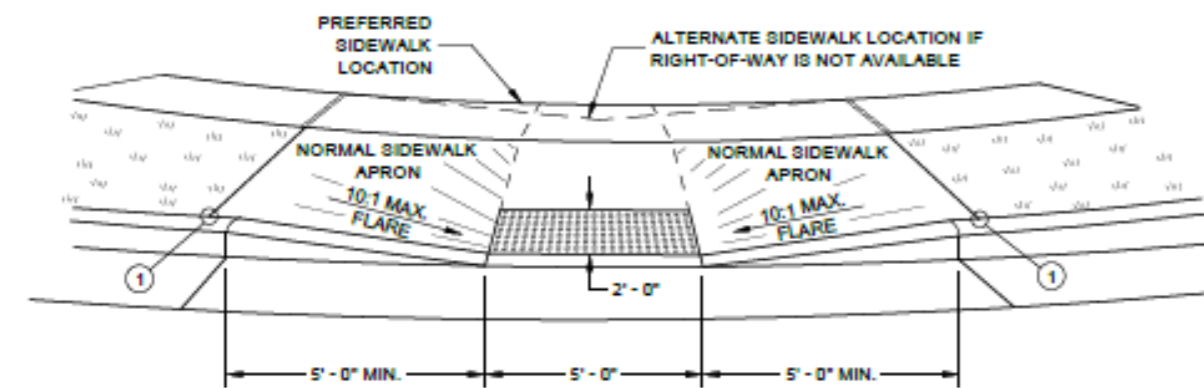
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**

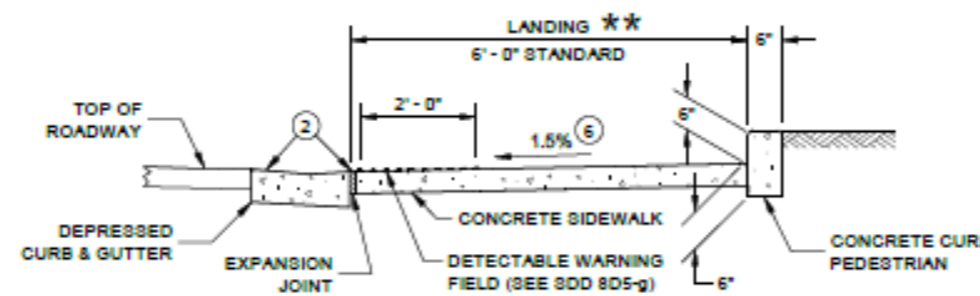
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMP SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- 1 THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 5 PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.



VIEW A - A FOR TYPE 1

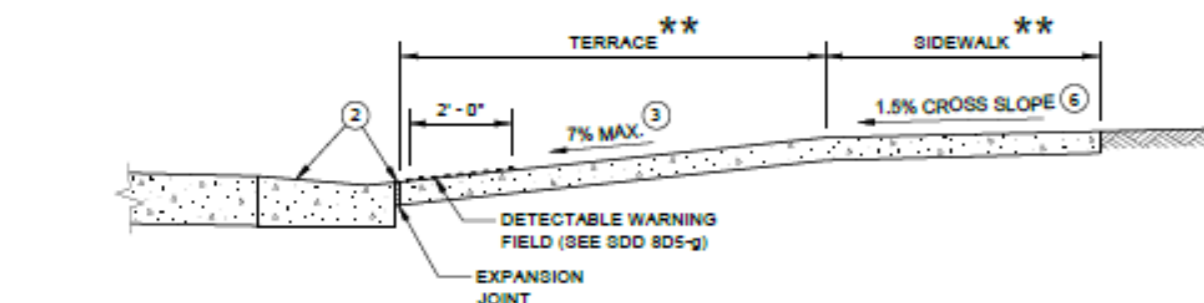
★ WIDTH SHOWN ELSEWHERE IN THE PLANS



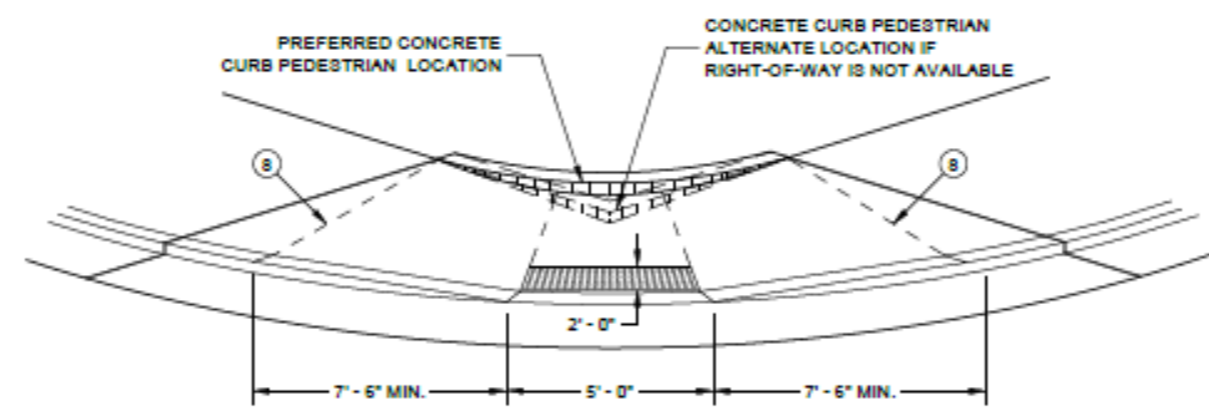
SECTION C - C FOR TYPE 1 - A

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

**CURB RAMP
TYPE 1 AND 1-A**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

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SDD 08D05 - 20a

SDD 08D05 - 20a

ENTRUST COMMS

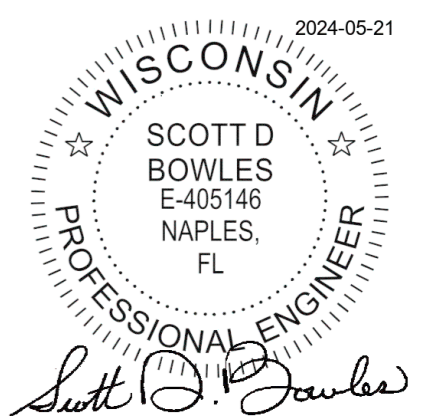
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-06

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

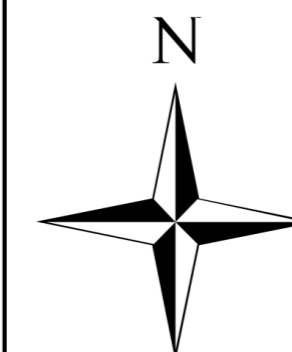
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



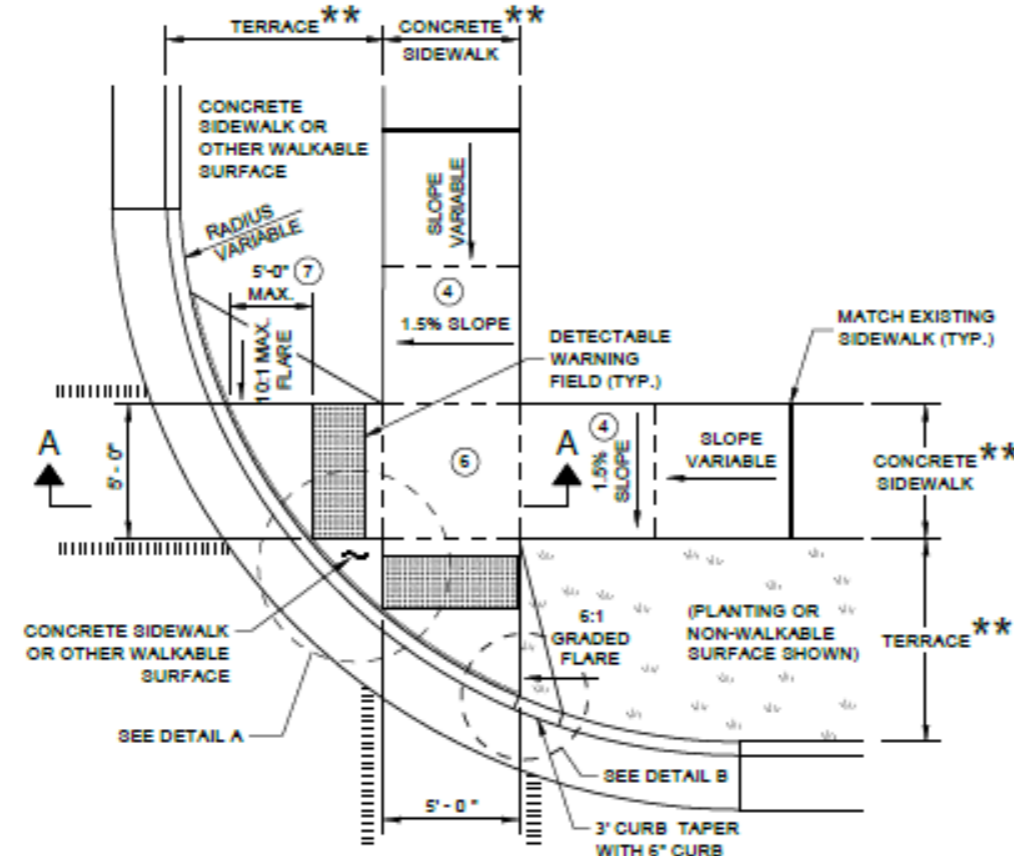
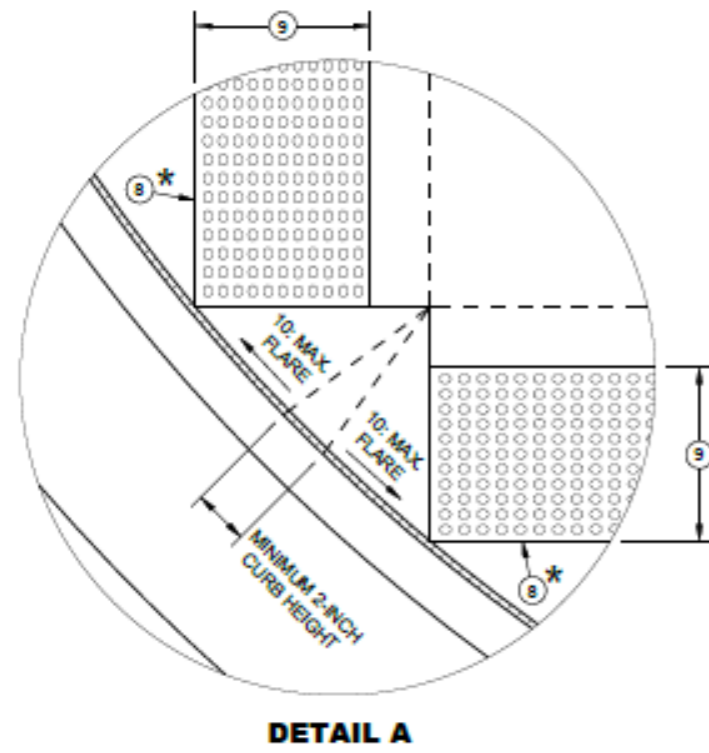
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PRIOR TO CONSTRUCTION CALL call811.org (TOLL FREE) AT 1-800-242-8511 OR 811 FOR LOCATION OF UNDERGROUND UTILITIES

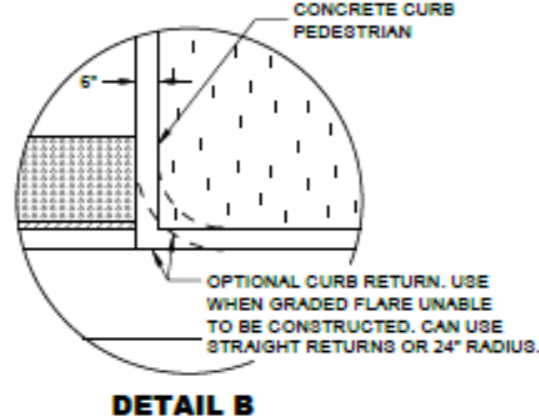
NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED



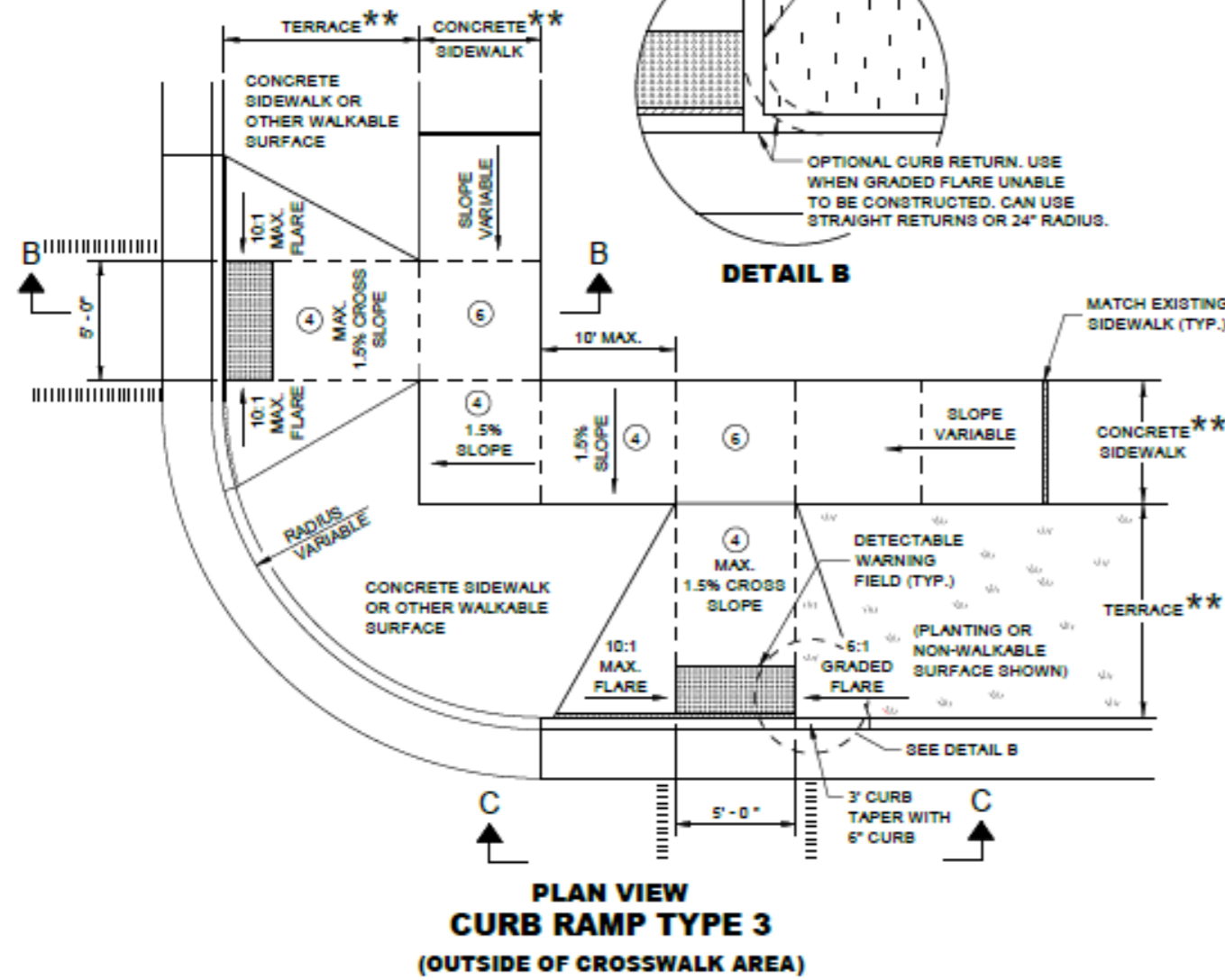
DATE: 5/21/2024
SHEET: 59 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



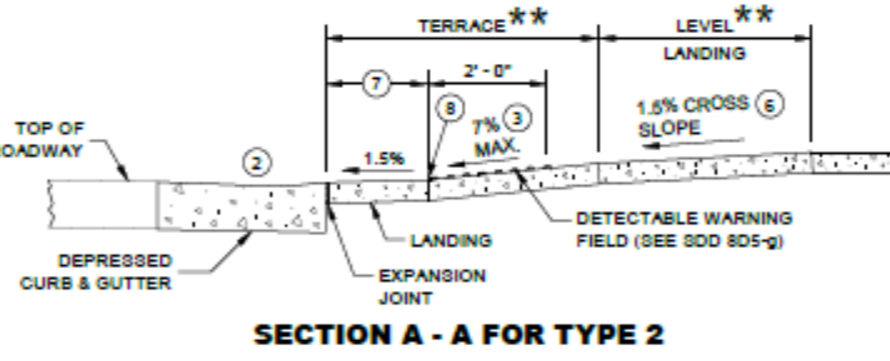
DETAIL B



PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-1.
- PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- WHEN DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

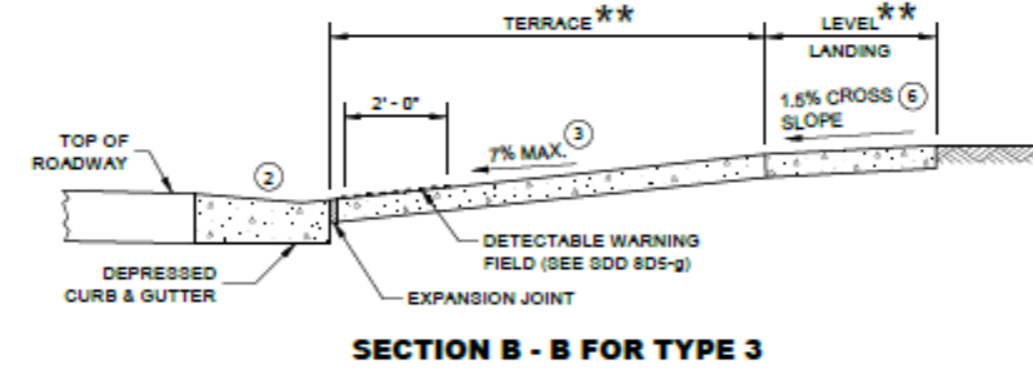


SECTION A - A FOR TYPE 2

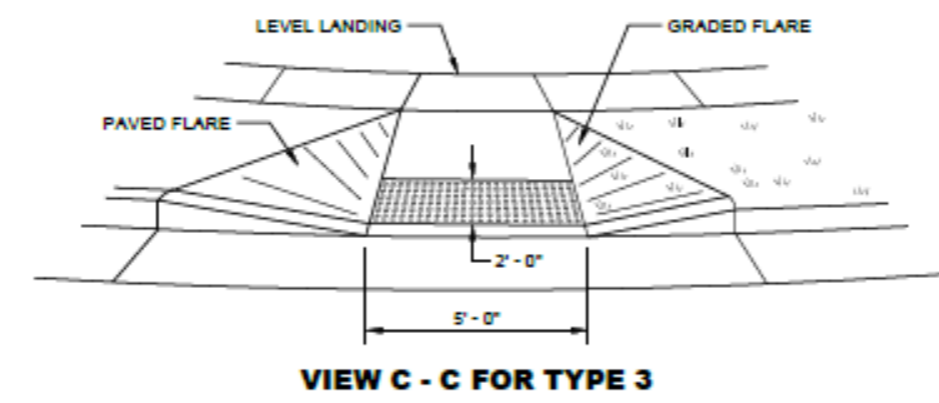
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
 ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

CURB RAMPS TYPE 2 AND 3
 STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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SDD 08D05 - 20b

SDD 08D05 - 20b

ENTRUST COMMS

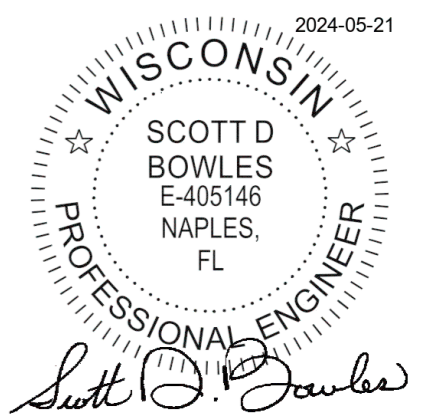
CITY OF SUPERIOR, WI
 FIBER OPTIC NETWORK
 TYPICAL PACKAGE
 PRELIMINARY - FOR REVIEW

SHEET-COSS-07

ENGINEER: SM
 DRAWN BY: JW
 CHECKED BY: JW

REVISIONS

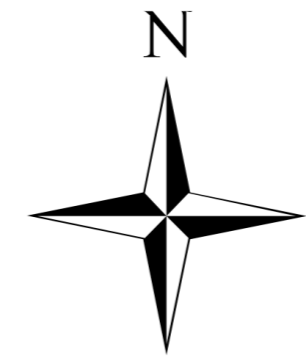
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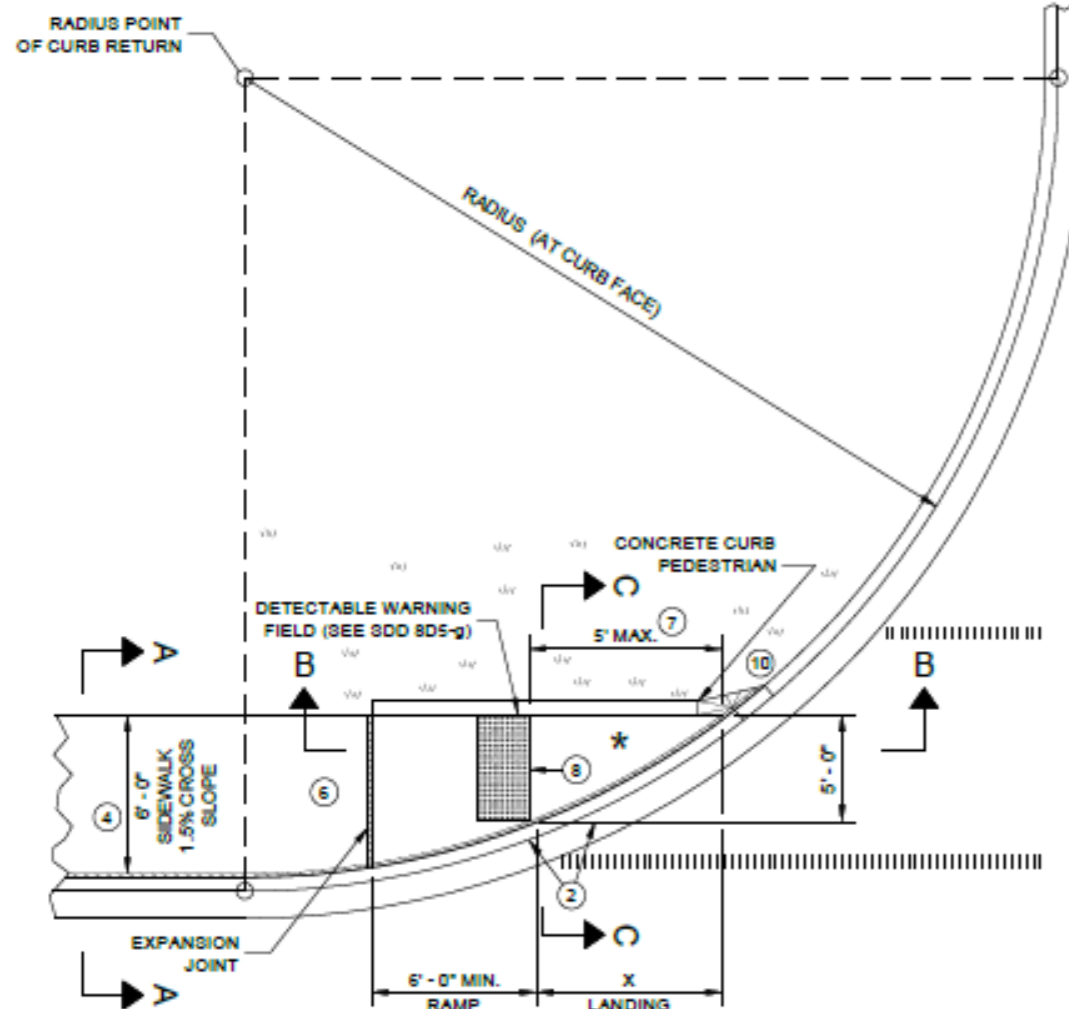


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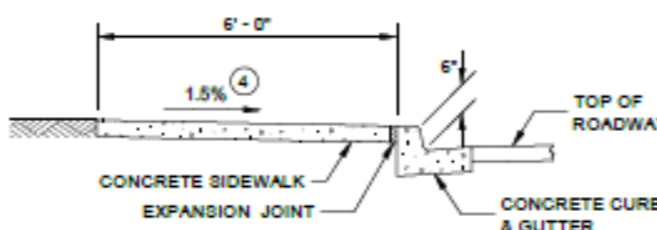
NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED





RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED

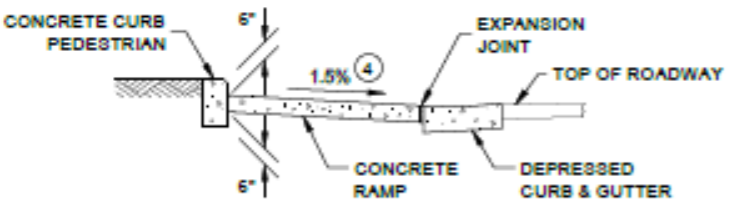
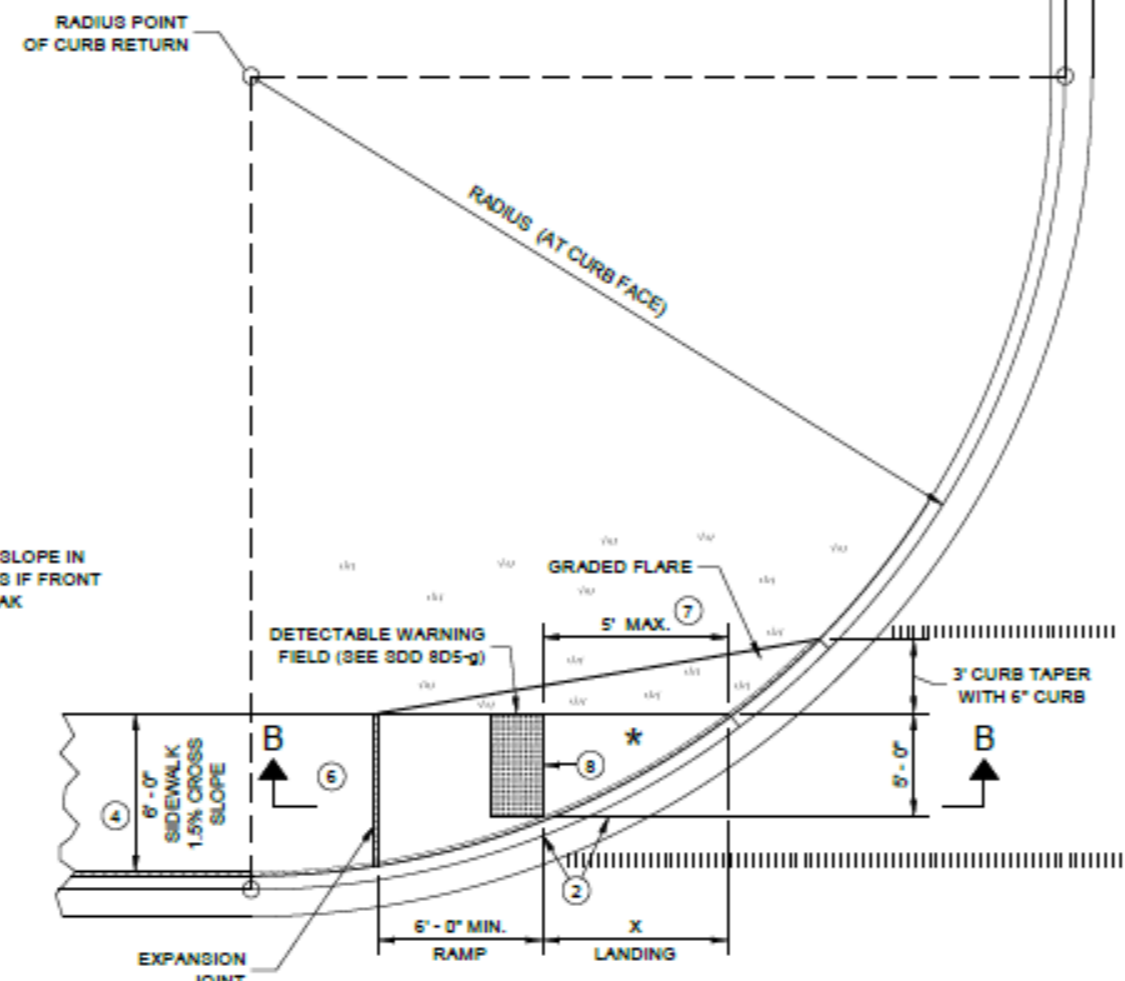


GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDINGS, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 5 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 6 WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-1.
- 7 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 8 INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

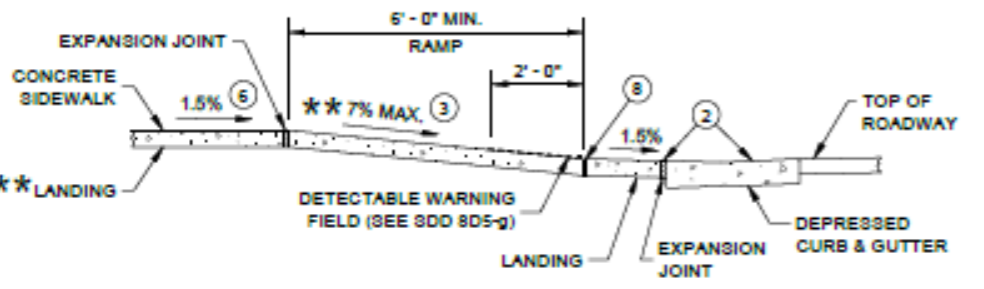
LEGEND

- EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



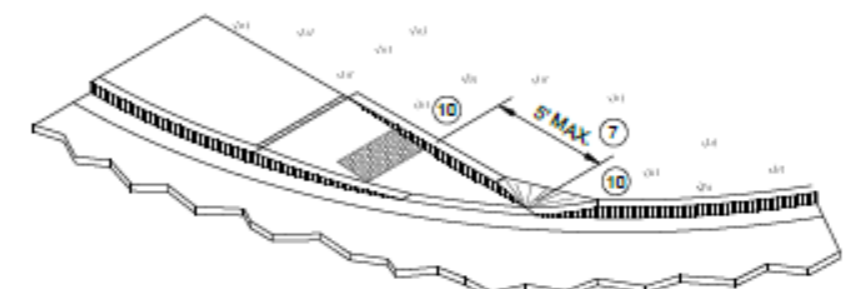
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

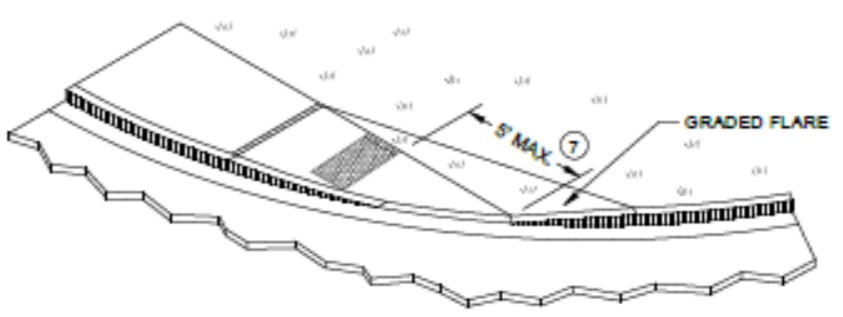


SECTION B - B FOR TYPE 4A AND TYPE 4A1

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMPS TYPE 4A AND 4A1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 08D05 - 20c

SDD 08D05 - 20c

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-08

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
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2024-05-21

WISCONSIN PROFESSIONAL ENGINEER

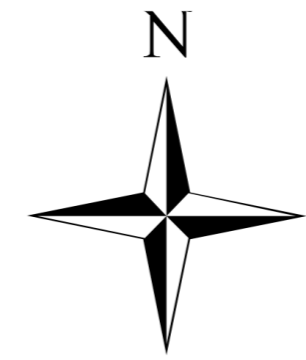
SCOTT D BOWLES
E-405146
NAPLES, FL

Scott D. Bowles

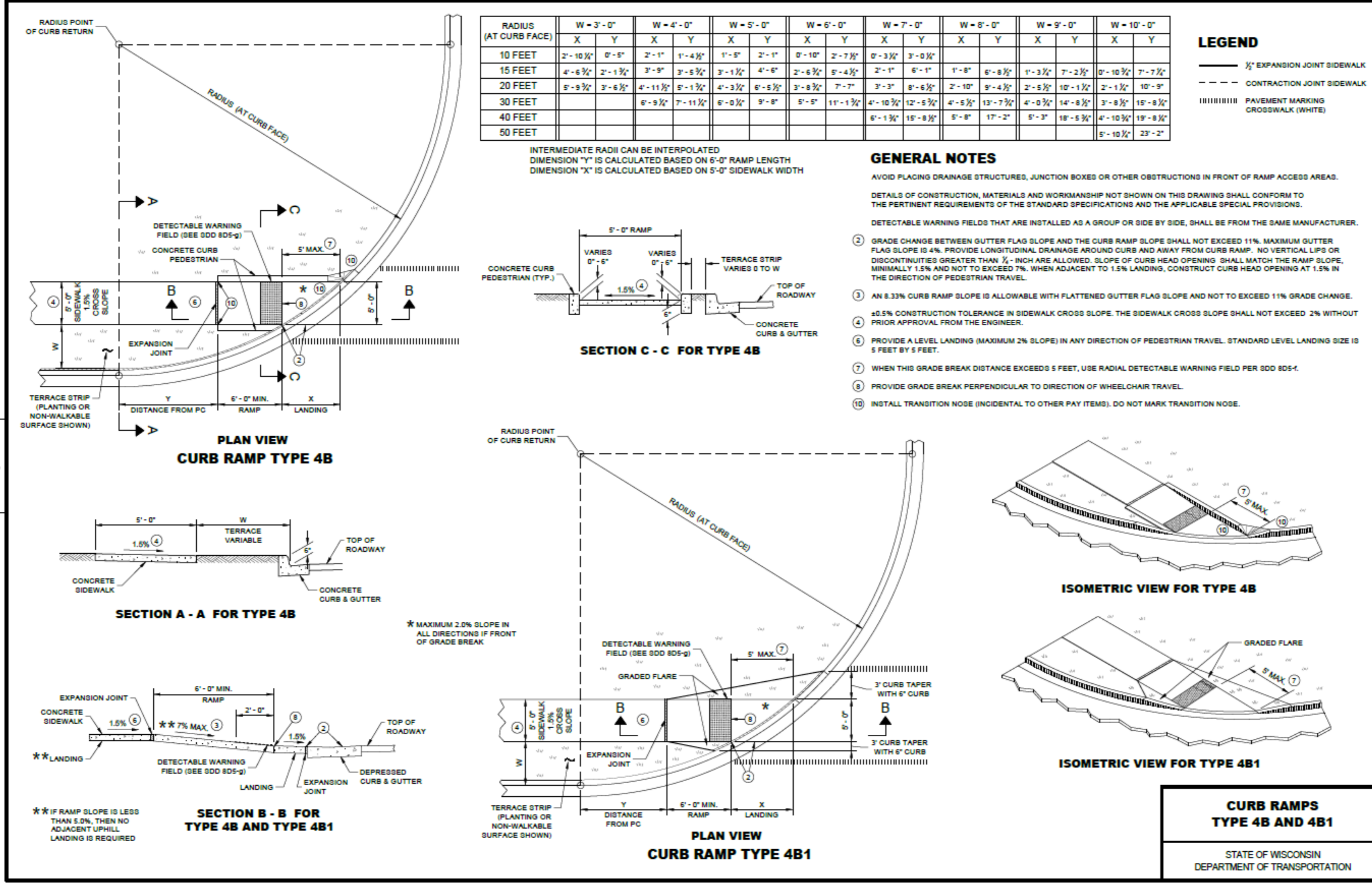
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DATE: 5/21/2024
SHEET: 61 OF 69
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf



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SDD 08D05 - 20d

SDD 08D05 - 20d

ENTRUST COMMS

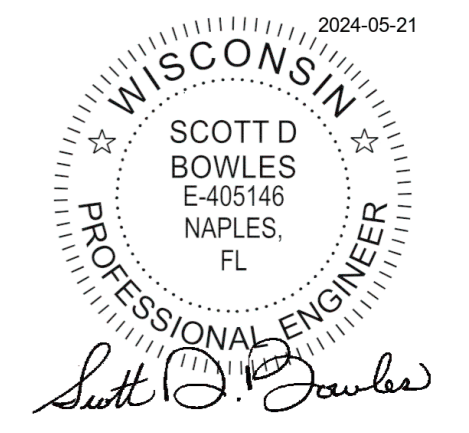
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-09

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

REVISIONS

DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	



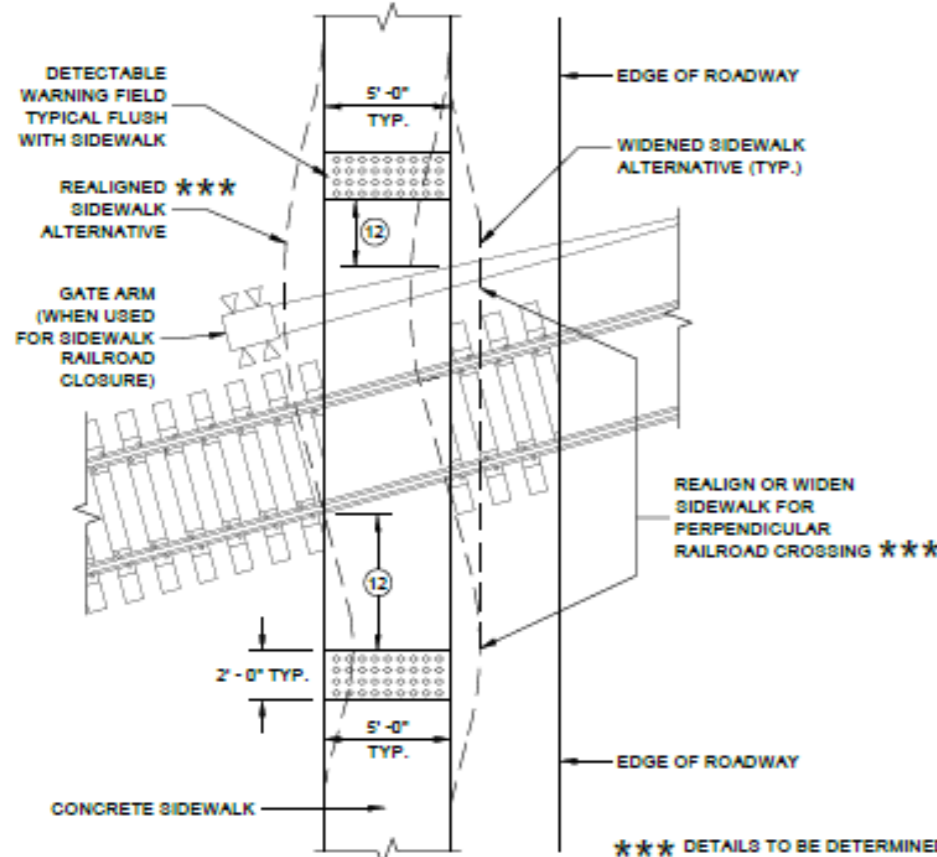
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SHEET: 62 OF 69
FILE: City of Superior, WI Construction Standards v4_05202024.pdf

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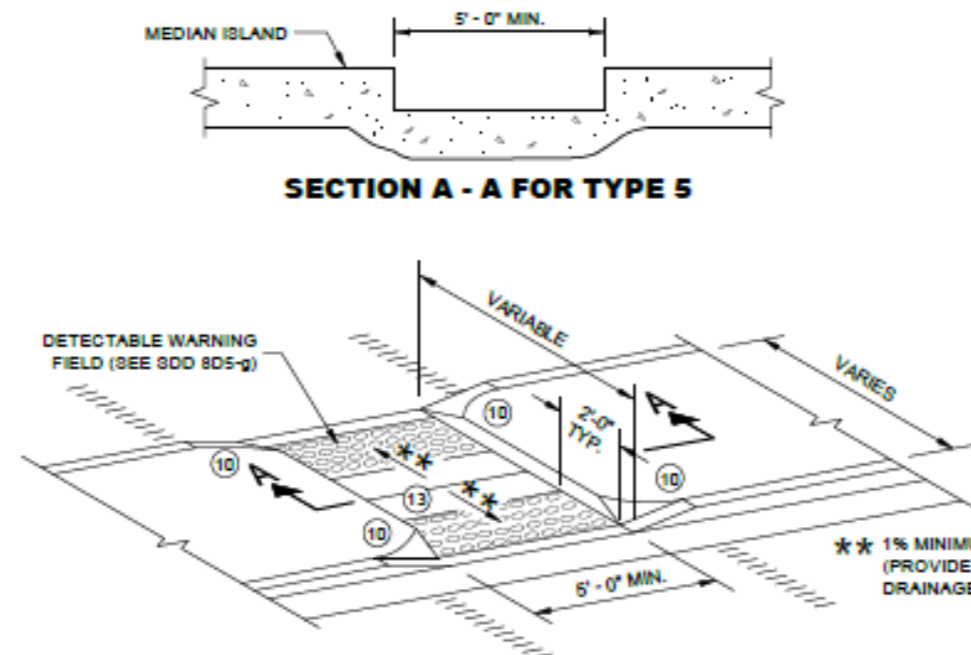
NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED





**CURB RAMP TYPE 8
DETECTABLE WARNINGS
AT RAILROAD CROSSING**

*** DETAILS TO BE DETERMINED BY ENGINEER



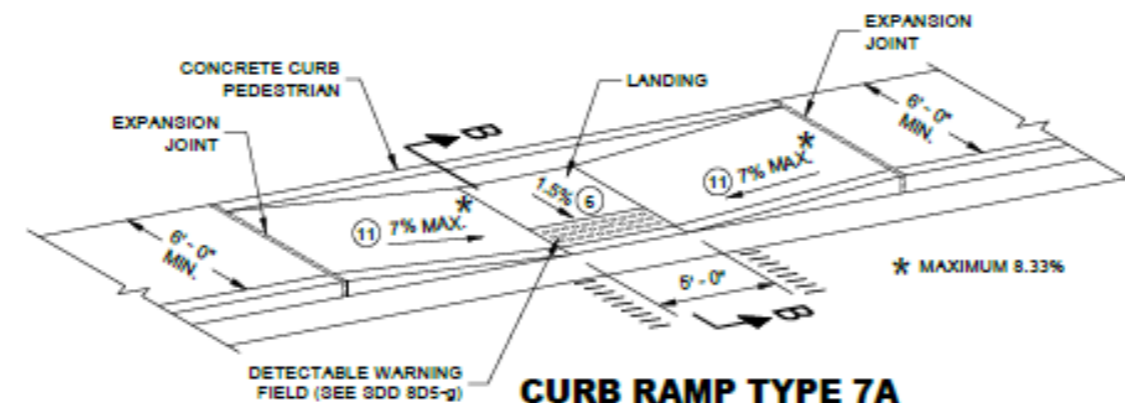
**CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**

GENERAL NOTES

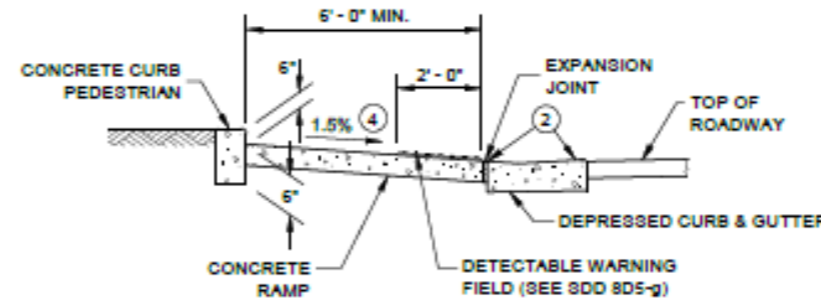
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- 2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- 11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- 12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- 13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

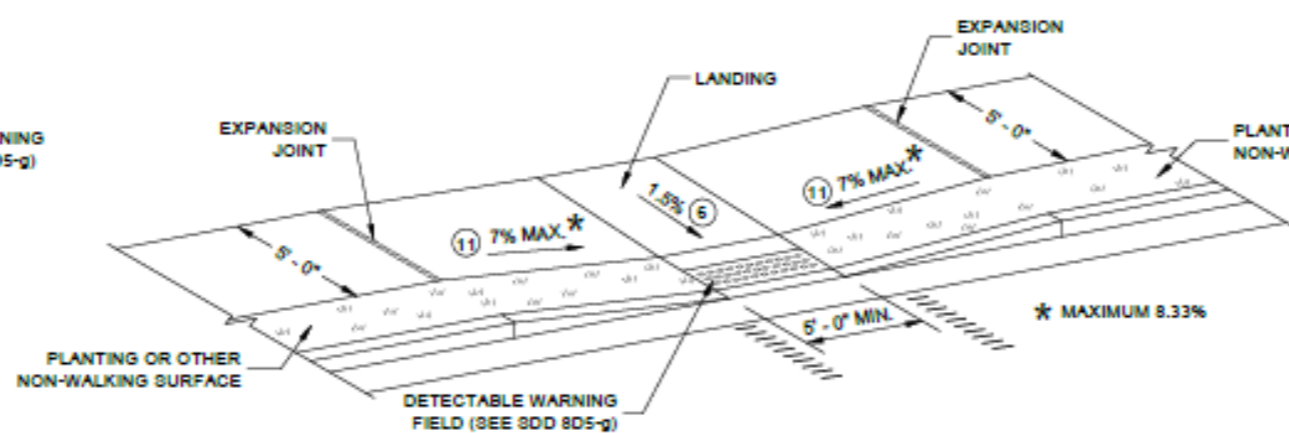
- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



**CURB RAMP TYPE 7A
MID BLOCK CROSSING**



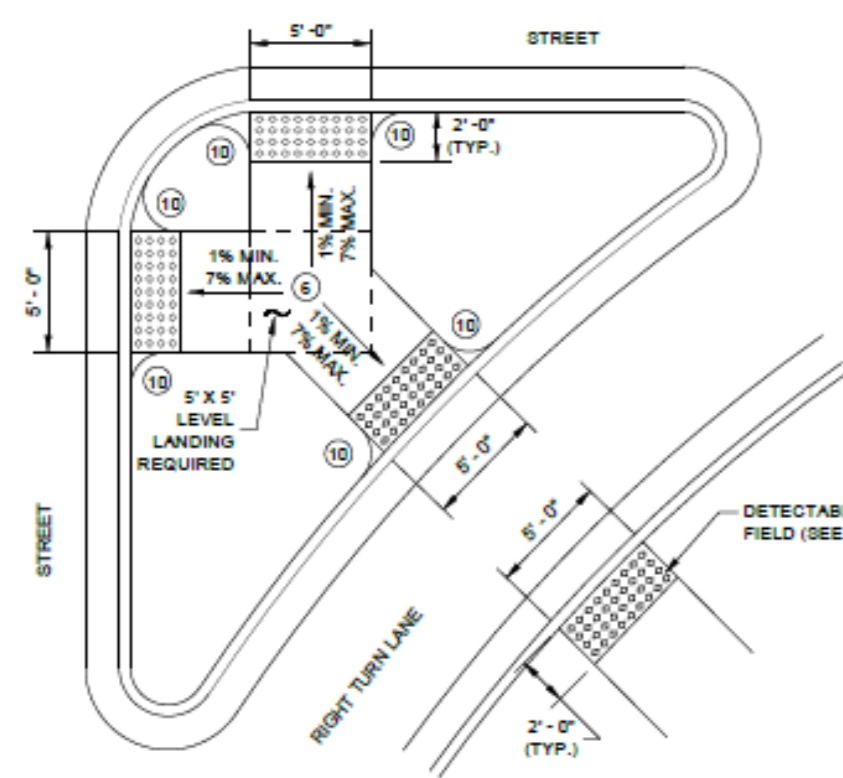
SECTION B - B FOR TYPE 7A



**CURB RAMP TYPE 7B
MID BLOCK CROSSING**

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS**

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS

ENTRUST COMMS

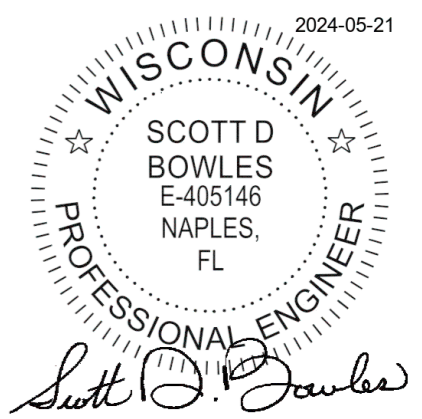
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-10

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

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DATE: 5/21/2024
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SDD 08D05 - 20e

SDD 08D05 - 20e

SDD 08D05-f Curb Ramps Radial Detectable Warning Field Applications

ENTRUST COMMS

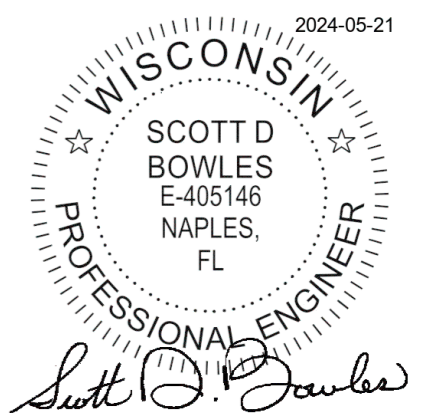
CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-11

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

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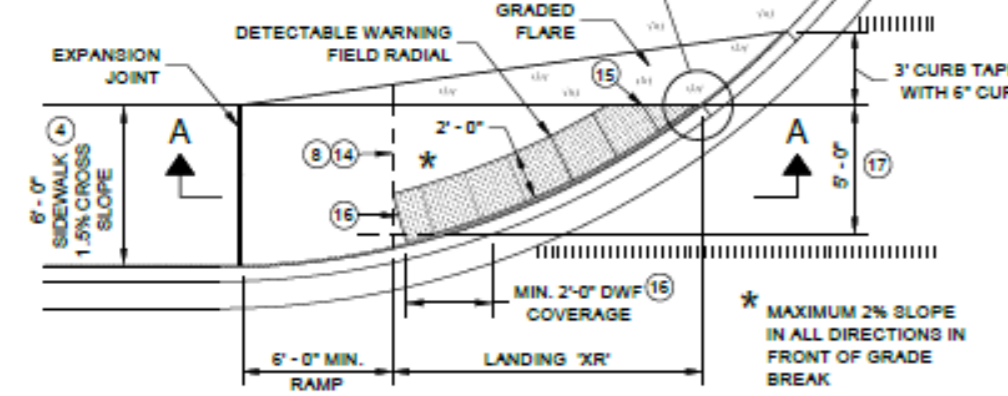
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

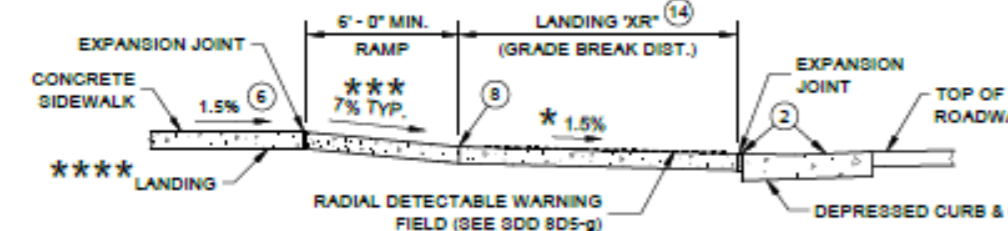
GENERAL NOTES

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- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMP AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMP. TYPE 4A AND 4B CURB RAMP ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDINGS, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2' RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED

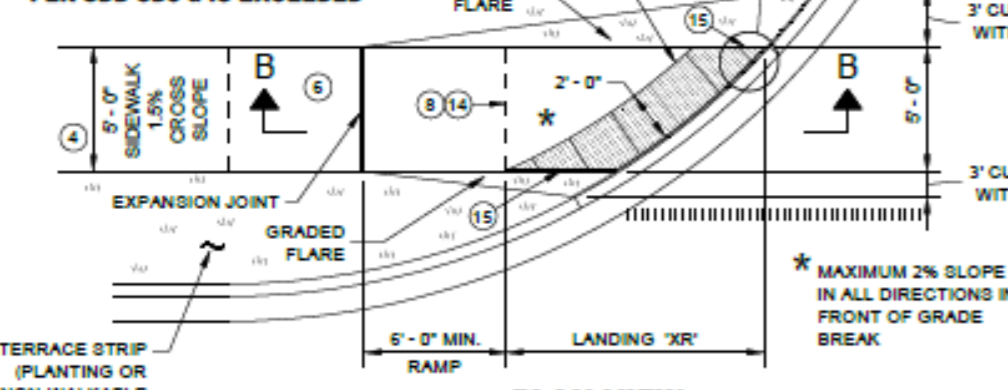


PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

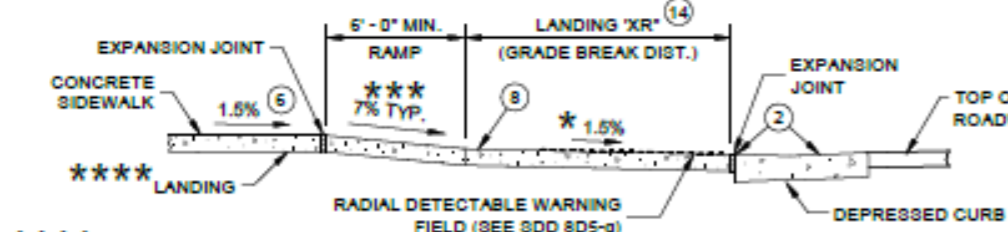


SECTION A - A FOR TYPE 4A1

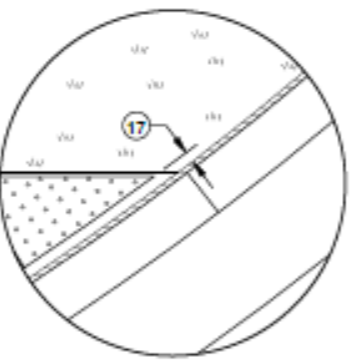
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



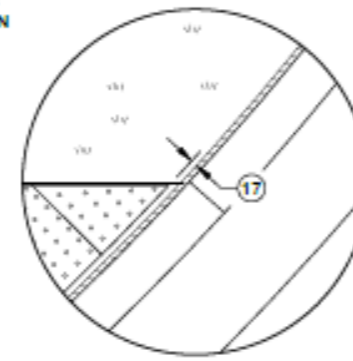
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

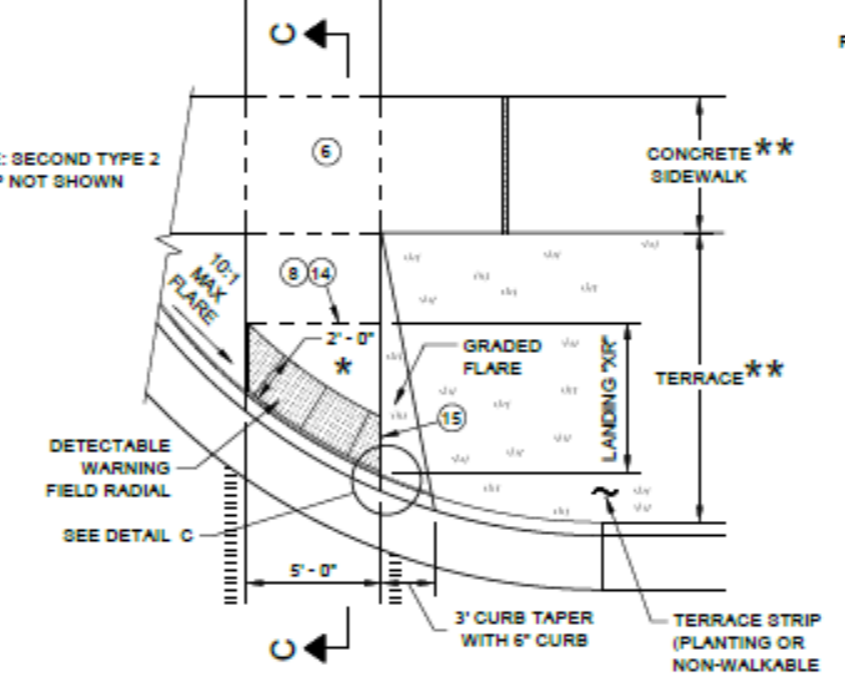


DETAIL A

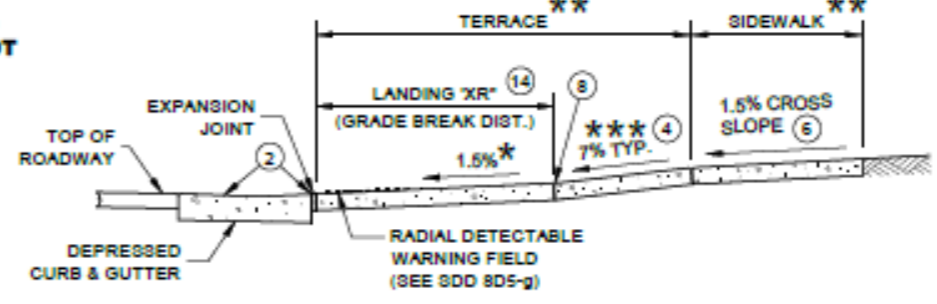


DETAIL B

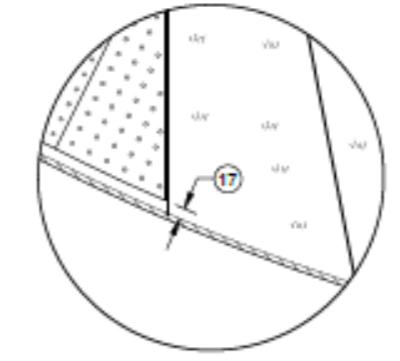
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (ON LINE WITH SIDEWALK) (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION C - C FOR TYPE 2



DETAIL C

CURB RAMP RADIAL DETECTABLE WARNING FIELD APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 08D05 - 20f

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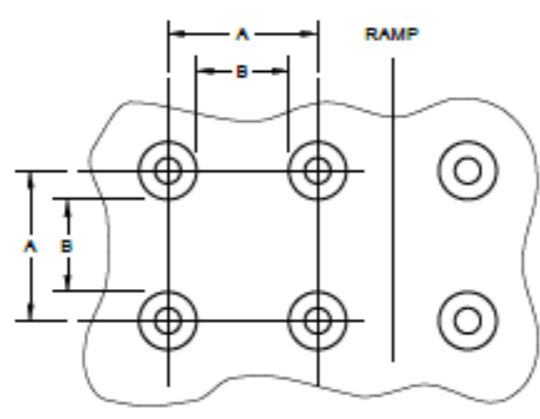
PRIOR TO CONSTRUCTION CALL call811.org (TOLL FREE) AT 1-800-242-8511 OR 811 FOR LOCATION OF UNDERGROUND UTILITIES

NOTE: ALL CONDUIT TO BE PLACED AT A MINIMUM DEPTH OF 36" UNLESS OTHERWISE NOTED

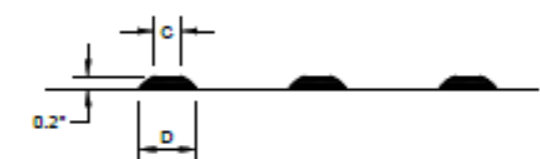


	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.5"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

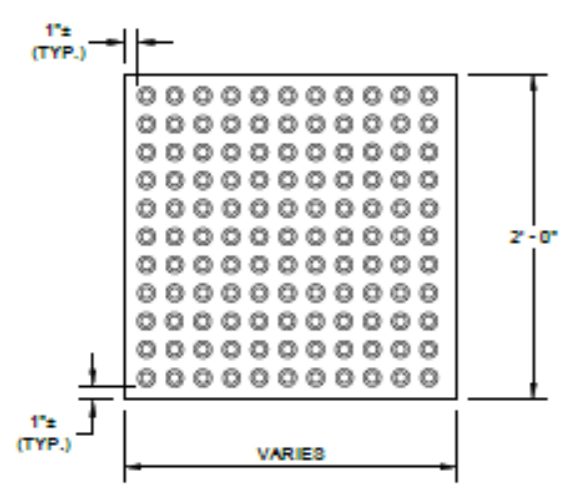


PLAN VIEW

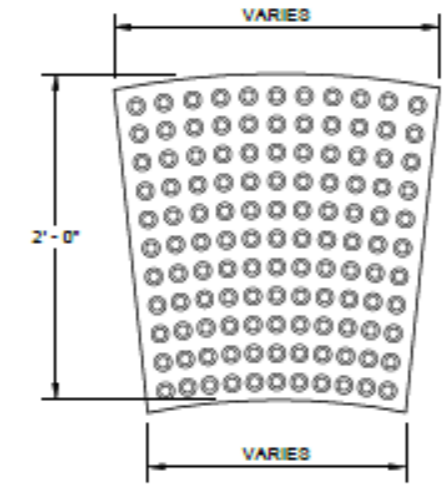


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**



RECTANGULAR
PLATES



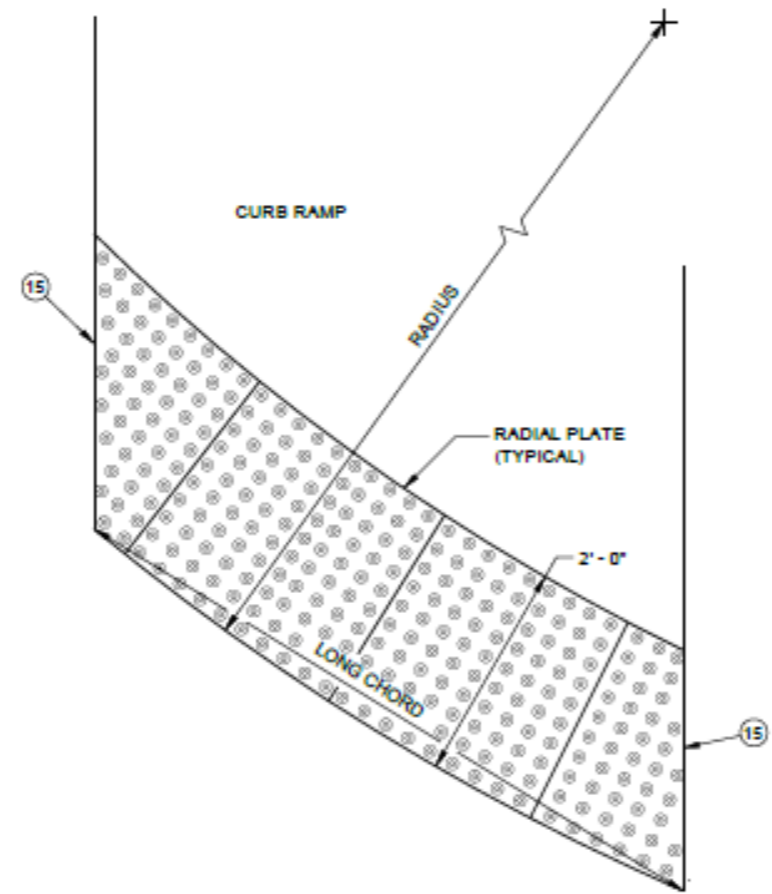
RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)

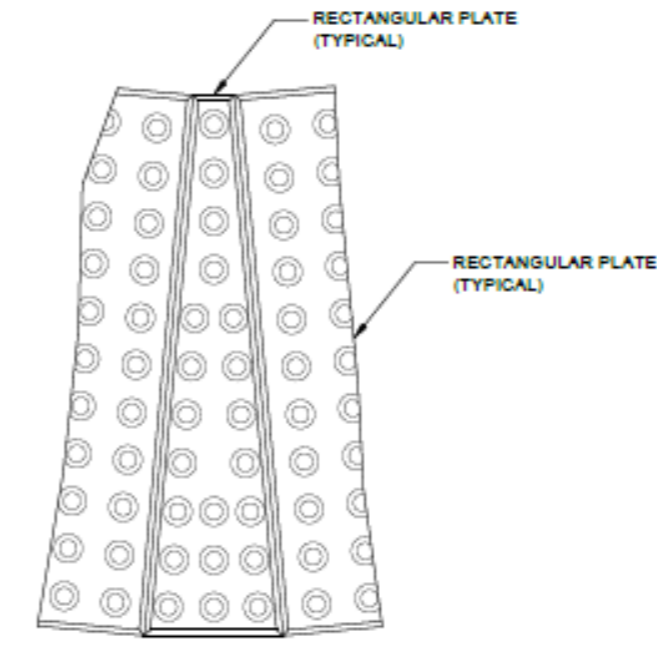
GENERAL NOTES

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

**CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /s/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

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SDD 08D05 - 20g

SDD 08D05 - 20g

ENTRUST COMMS

CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-12

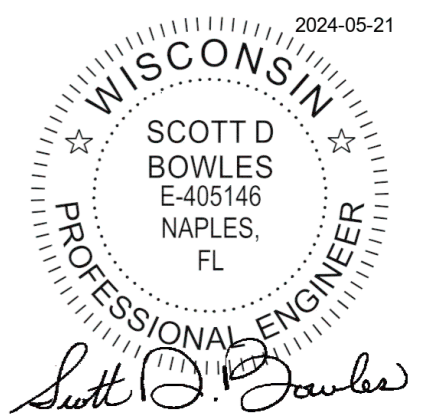
ENGINEER: SM

DRAWN BY: JW

CHECKED BY: JW

REVISIONS

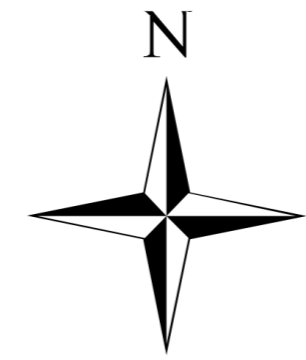
DATE	DESCRIPTION	BY
04/30/2024	VERSION 1	

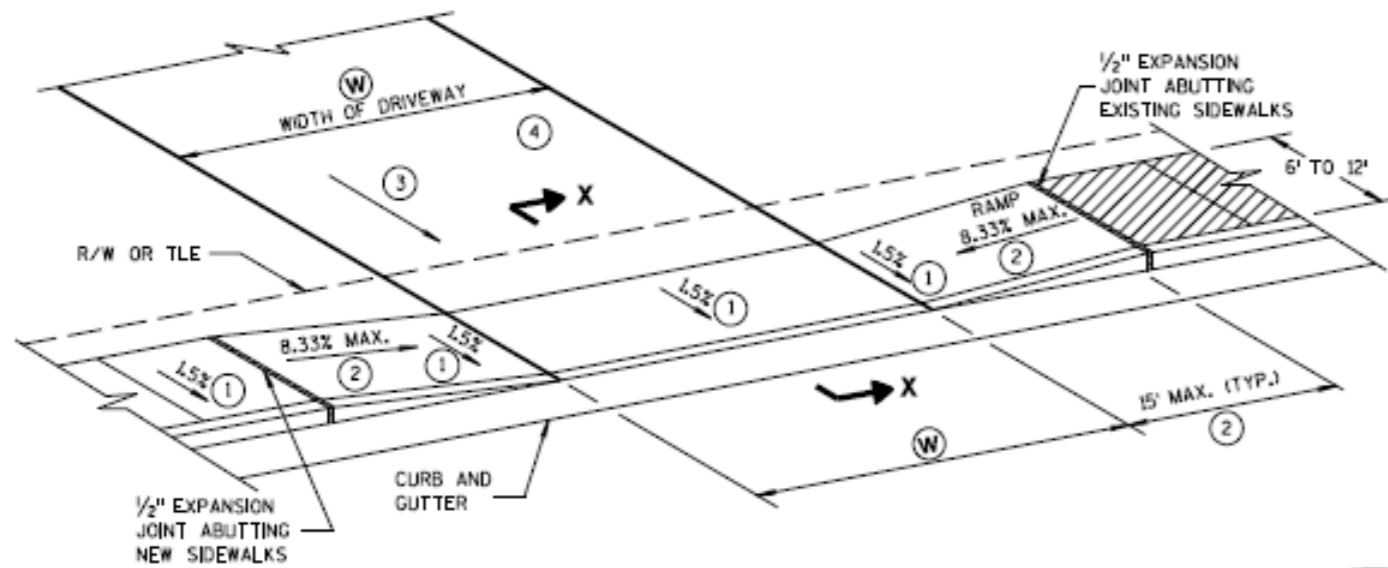


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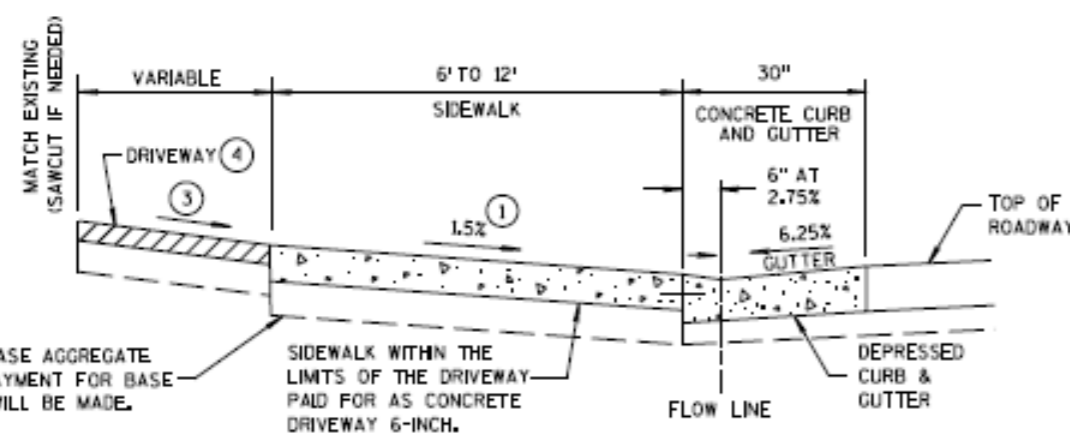
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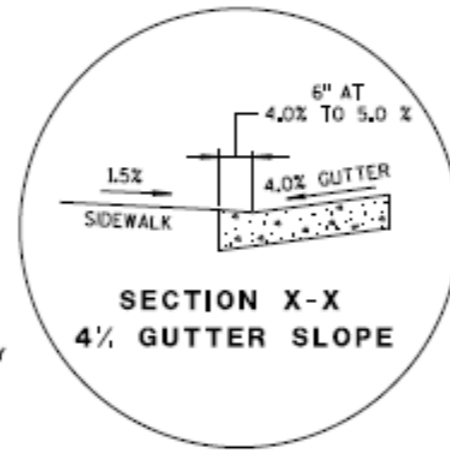




TYPE X
SIDEWALK ABUTS CURB & GUTTER
TERRACE VARIES 0 TO 3 FEET



SECTION X-X



SECTION X-X
4% GUTTER SLOPE

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)

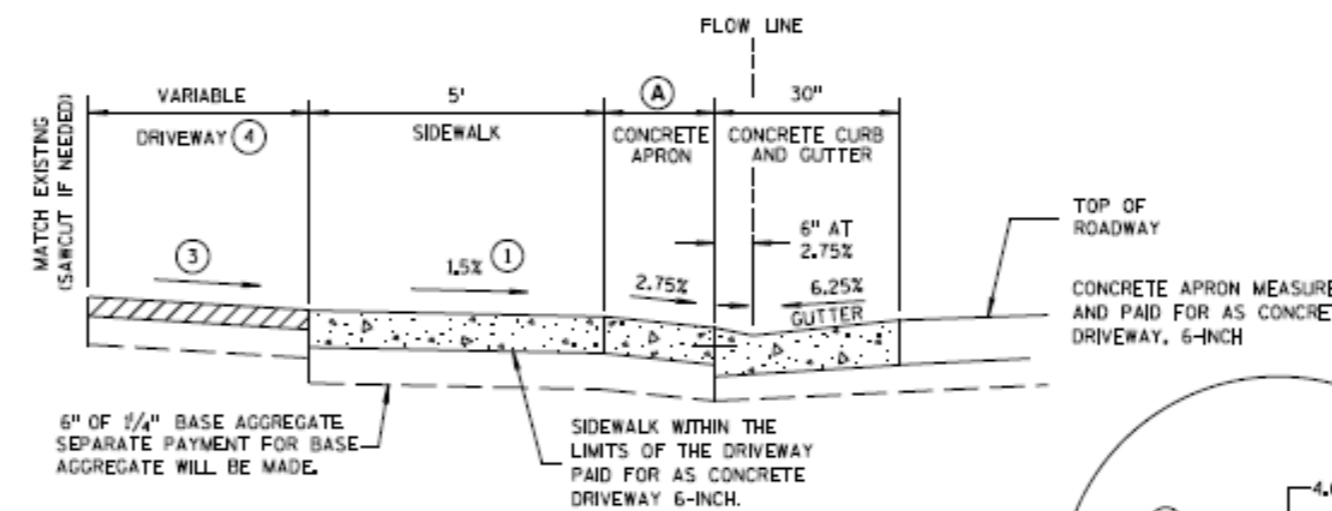
GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- CONSTRUCTION TOLERANCE OF 0.5% ± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- DRIVEWAY TYPES**
• 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
• 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
• 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES)



SECTION Y-Y
DRIVEWAY DETAIL WITH CONCRETE CURB & GUTTER
(URBAN AND SUBURBAN)

TABLE Y

(A) FEET	(C) FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'

NOT TO SCALE

DRIVEWAY AND SIDEWALK RAMP TYPES X & Y

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /s/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

S.D.D. 8 D 18-2

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S.D.D. 8 D 18-2

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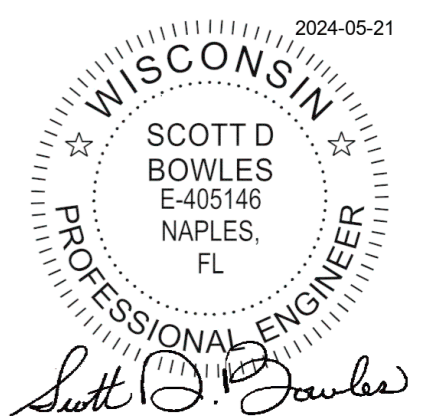
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FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

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ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

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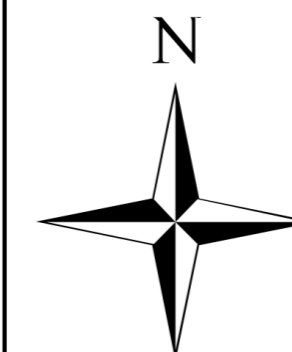
Scott D. Bowles

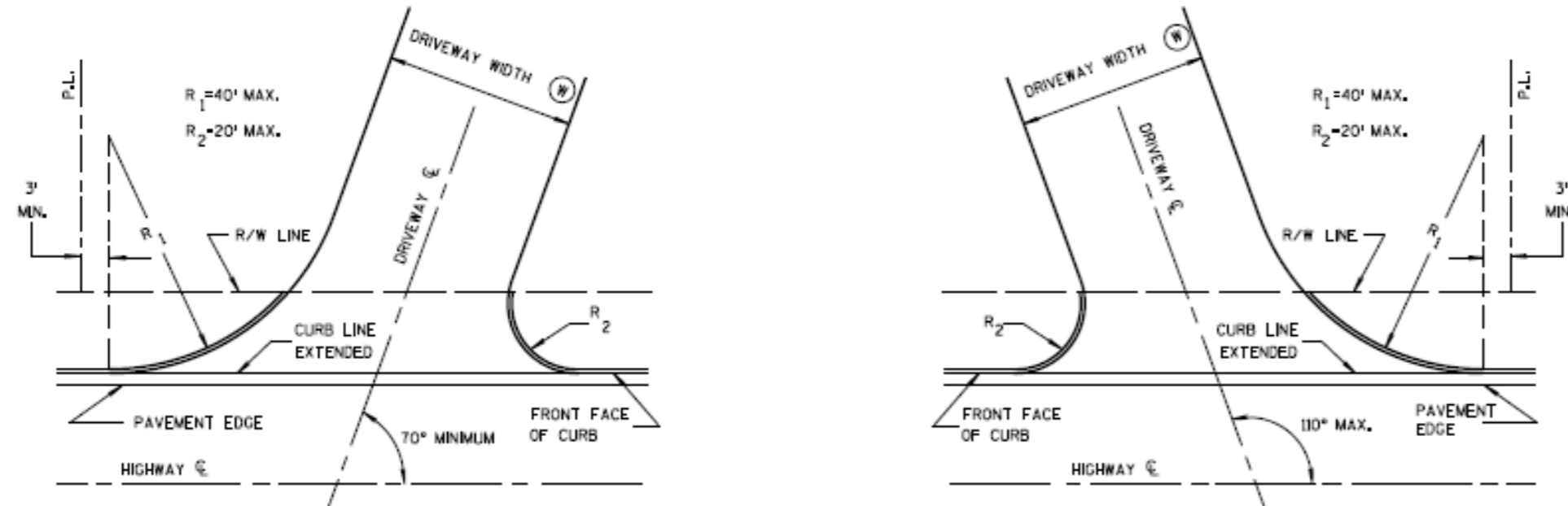
DATE: 5/21/2024
SHEET: 66 OF 69
FILE: City of Superior, WI Construction Standards_v4_05202024.pdf

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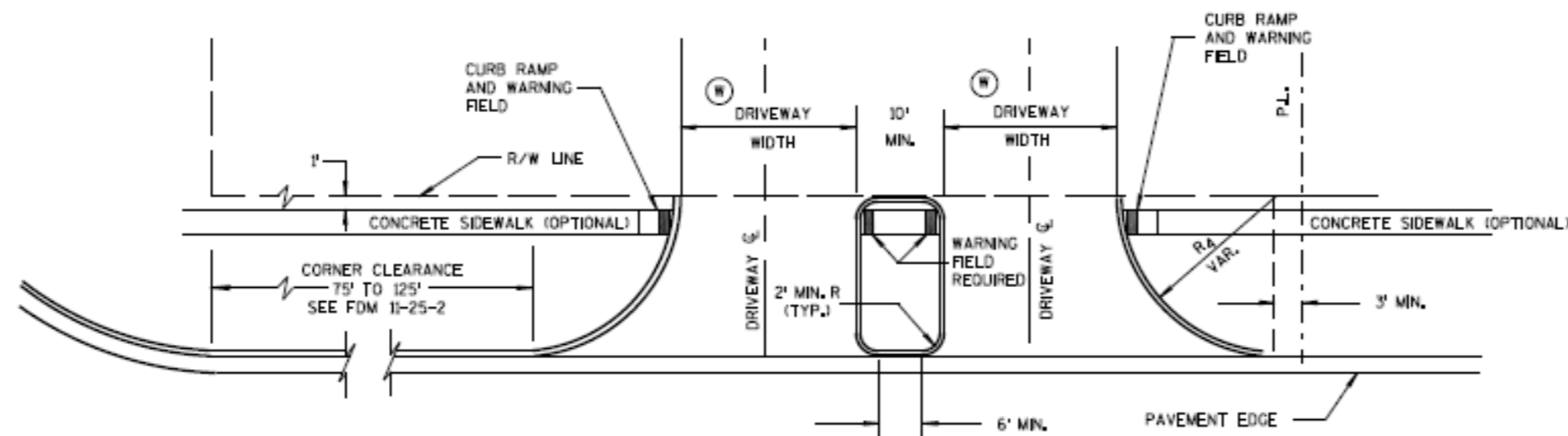
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**SKewed DRIVEWAY DETAILS
(COMMERCIAL AND NON-COMMERCIAL)**

SIDEWALK NOT SHOWN



DRIVEWAY LOCATION AND SPACING DETAILS

SIDEWALK SHOWN

NOTES

- A MAXIMUM RADIUS OF 10 FEET SHALL BE USED FOR NON-COMMERCIAL PRIVATE ENTRANCES. RADII FOR COMMERCIAL DRIVEWAYS SHALL BE DETERMINED BY THE ENGINEER BASED ON TRAFFIC AND DRIVEWAY PERMIT RESTRICTIONS.
- THE MINIMUM ANGLE OF INTERSECTION BETWEEN THE DRIVEWAY AND HIGHWAY CENTERLINES SHALL BE 70°.
- ALL CURVILINEAR PRIVATE ENTRANCE OUTLINES SHALL BE CONTAINED WITHIN THE HIGHWAY R/W.
- NO DRIVEWAY SHALL BE BUILT WITHIN 3 FEET OF THE PROPERTY LINE EXCEPT FOR EXISTING JOINT DRIVEWAY SHARED BY TWO OWNERS.

- Ⓜ DRIVEWAY WIDTHS:
COMMERCIAL - 35' MAX., 16' MIN.
RESIDENTIAL AND NON-COMMERCIAL - 24' MAX., 12' MIN.

DRIVEWAYS WITH CURB & GUTTER RETURNS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December, 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
PHWA

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S.D.D. 8 D 20-1

S.D.D. 8 D 20-1

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CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COSS-14

ENGINEER: SM
DRAWN BY: JW
CHECKED BY: JW

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2024-05-21

WISCONSIN PROFESSIONAL ENGINEER

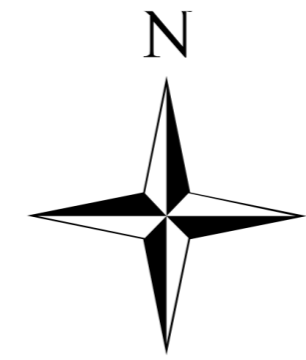
SCOTT D BOWLES
E-405146
NAPLES, FL

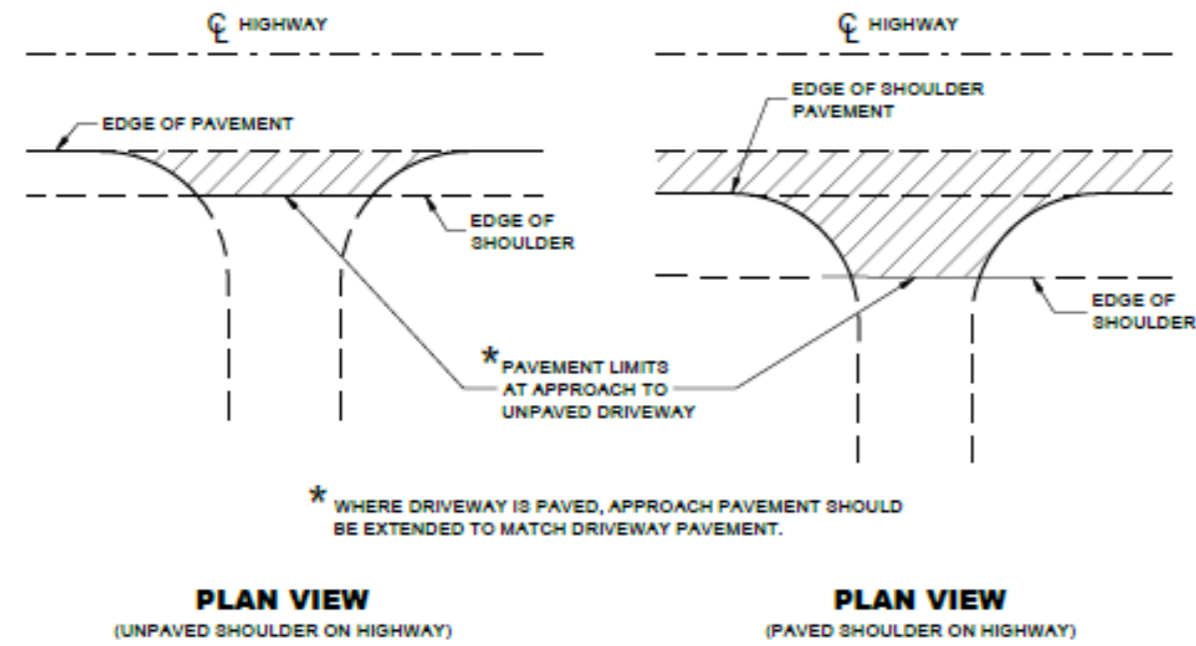
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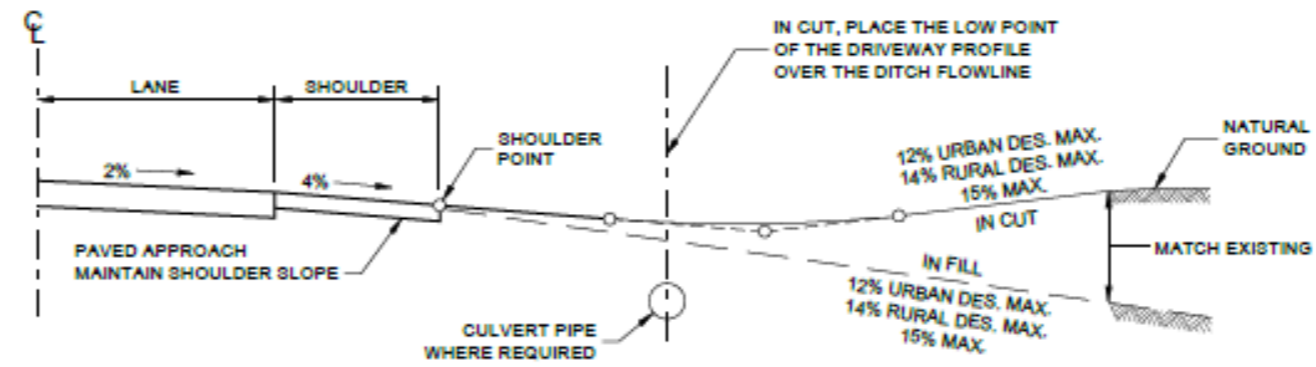
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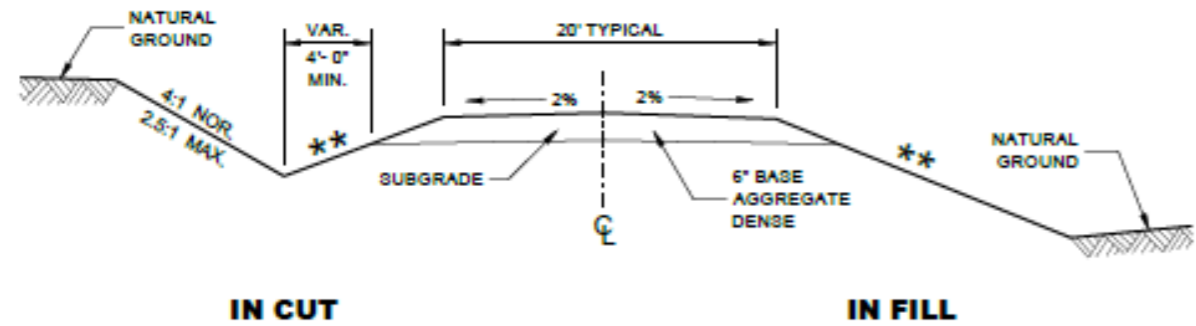


PLAN VIEW
(UNPAVED SHOULDER ON HIGHWAY) **PLAN VIEW**
(PAVED SHOULDER ON HIGHWAY)

**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



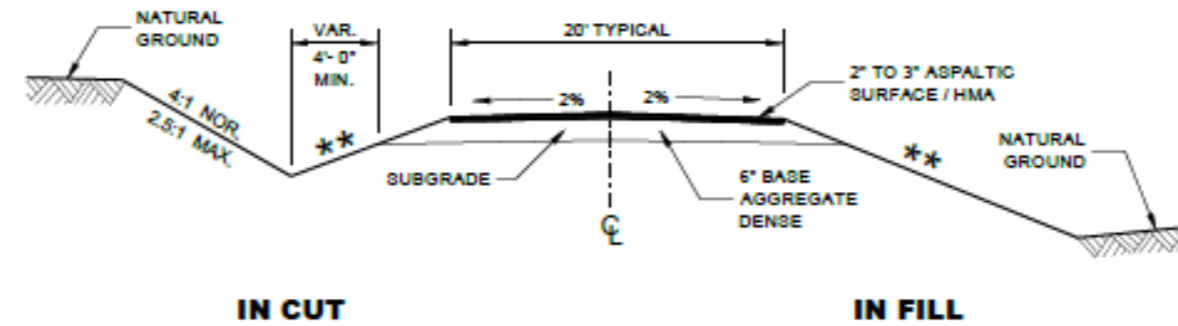
TYPICAL DRIVEWAY PROFILES



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥ 60	10:1



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

DRIVEWAYS WITHOUT CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2017 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

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CITY OF SUPERIOR, WI
FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

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ENGINEER: SM
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CHECKED BY: JW

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WISCONSIN
SCOTT D BOWLES
E-405146
NAPLES, FL
PROFESSIONAL ENGINEER

Scott D. Bowles

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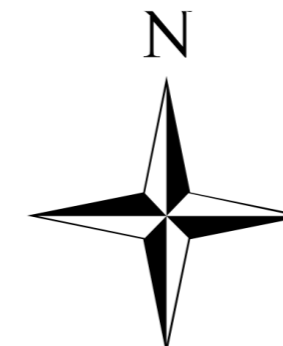
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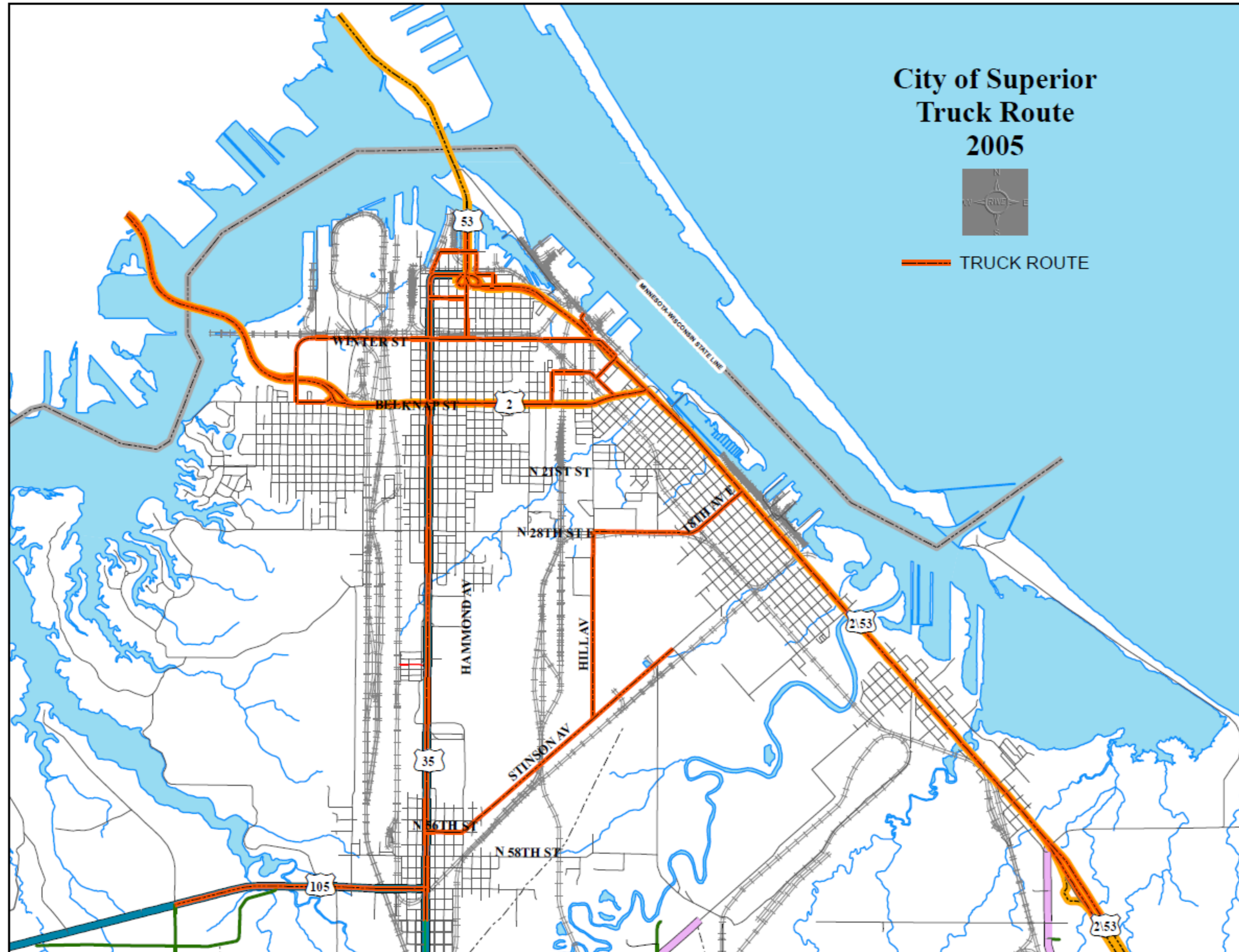
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SHEET: 68 OF 69
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**City of Superior
Truck Route
2005**



— TRUCK ROUTE

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FIBER OPTIC NETWORK
TYPICAL PACKAGE
PRELIMINARY - FOR REVIEW

SHEET-COS-TR-01

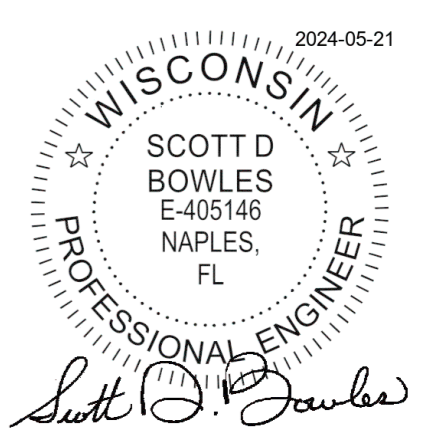
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DRAWN BY: JW

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04/30/2024	VERSION 1	



DATE: 5/21/2024

SHEET: 69 OF 69

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